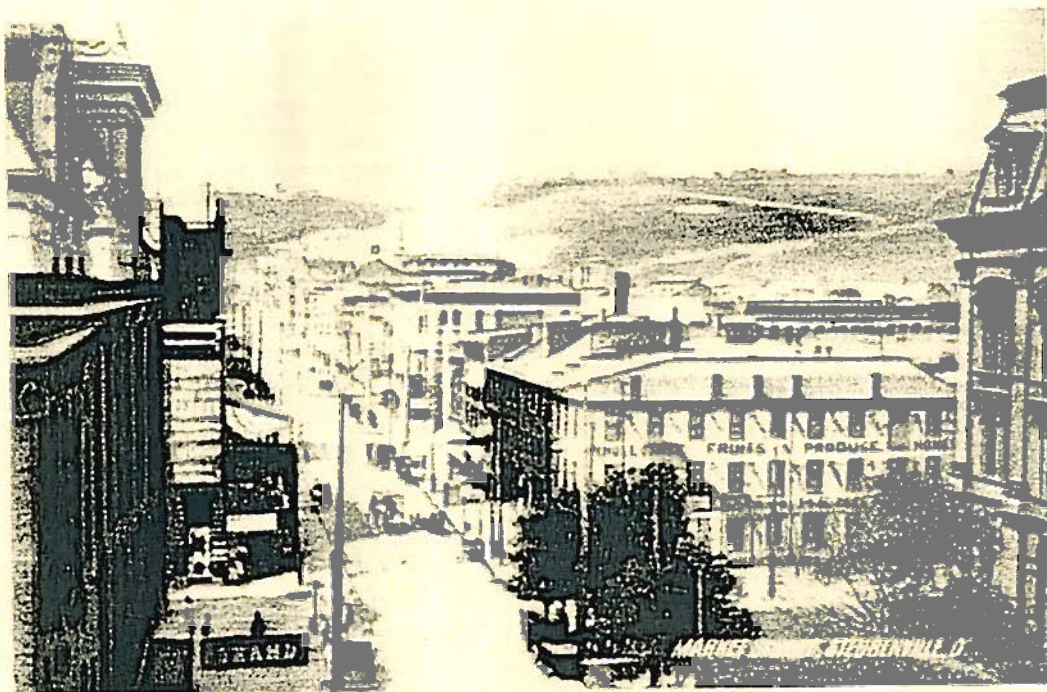


# STEUBENVILLE HISTORIC CONTEXTS AND RESOURCE EVALUATION



Prepared for the  
Steubenville Historic Landmarks  
Commission

by

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## About This Publication

This publication is the result of several phased projects undertaken by the City of Steubenville and the Steubenville Historic Landmarks Commission. Under the Certified Local Government (CLG) program administered by the Ohio Historic Preservation Office of the Ohio Historical Society, Steubenville has used federal matching historic preservation funds to complete several projects focused on the city's historic buildings, structures, and districts. This work has included surveys and inventories to identify, research, and document potentially significant properties; nomination of the North End Neighborhood Historic District and the Steubenville Commercial Historic District to the National Register of Historic Places; passage of city legislation to designate and protect significant historic properties; and preparation of historic contexts, or thematic historical narratives, to help evaluate future local and National Register listings.

The City of Steubenville achieved CLG status in 1988 because the City recognized that preservation, protection, and enhancement of a community's historic properties and districts can be a significant element of economic revitalization. Since achieving CLG status, in addition to the preservation and planning efforts noted above, the City has initiated a successful facade renovation program in the downtown area and created the Steubenville Historic Landmarks Commission to provide design review and guidance in appropriate rehabilitation work in the downtown. Private efforts such as the murals program, the Federal Land Office Park, and reconstruction of Fort Steuben, as well as extensive private investment in building rehabilitation, have dovetailed with the City's efforts to the point that real revitalization progress is now readily visible.

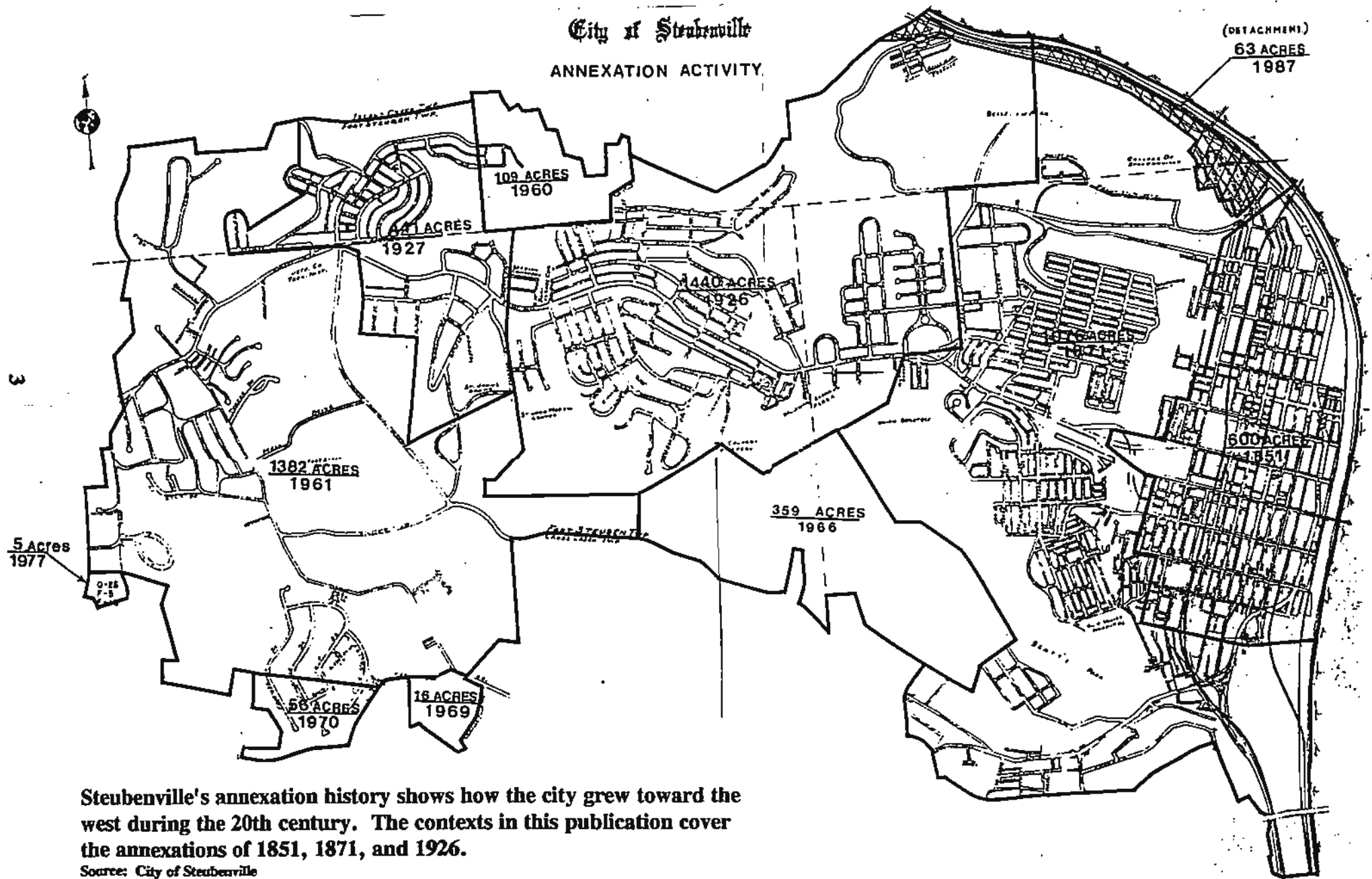
CLG status requires that a city have an ongoing program for evaluating and recognizing significant historic buildings and districts. Part of doing so is establishing a means of evaluating properties, of placing them in the context of Steubenville's history so their significance can be more accurately gauged. A logical approach, and the one followed in Steubenville, was to divide the city's history into broad themes which, taken together, covered all the major aspects of the city's development. These thematic narratives, known as historic contexts, include a discussion of the various types of properties associated with each theme in Steubenville, and a statement of what qualities a property or district should have in order to be considered significant and therefore qualified for registration.

This publication contains the seven historic contexts that describe the major themes in Steubenville's history. Four have been previously published in preliminary reports: "Industrial Development in Steubenville, 1847-1959" and "Transportation and Distribution in Steubenville, 1853-1959" were published in 1990; "Development of Downtown Neighborhoods in Steubenville, 1870-1910" and "Downtown as Steubenville's Governmental, Commercial, and Social Center, 1870-1950" were published in 1991. The previously-published contexts are re-published here with revisions. The final three contexts, "Ethnic Diversity in Steubenville, 1840-1940," "Hilltop Neighborhoods in Steubenville, 1890-1950," and "Development of Open Space and Recreational Facilities in Steubenville, 1854-1945." are published here for the first time. The City of Steubenville decided to publish all the contexts in a single new publication to make them more convenient to use.

Two maps follow this introductory section. One, based on the U.S. Geological Survey topographic map of 1958, depicts Steubenville as it was at about the ending date of most of the contexts. The other, provided by the City of Steubenville, is an annexation map showing the growth of the city during the 19th and 20th centuries.



**Steubenville in the late 1950s. North is toward the top of the map.**  
 Source: U.S. Geological Survey 7.5-minute series topographic maps



Steubenville's annexation history shows how the city grew toward the west during the 20th century. The contexts in this publication cover the annexations of 1851, 1871, and 1926.

Source: City of Steubenville

## Associated Contexts

Each context that follows begins with a historical narrative, based on primary and secondary sources listed in the Bibliography, that recounts the evolution of a major theme in Steubenville's historical development. Following each narrative is a discussion of the types of properties -- buildings or structures -- that were associated with that particular theme. Included in the discussion of property types is a statement of why a property type is considered significant; what qualities a property must have in order to be considered eligible for registration; and an evaluation of extant Steubenville properties associated with that theme which may be qualified for registration.

The first context covers the industrial history of Steubenville, and the second covers transportation and distribution. Because so few properties associated with these contexts have survived, following the transportation/distribution context is an evaluation of archaeological resources associated with both themes that may be of significance and that may warrant preservation efforts.

In some cases knowledge about property types is incomplete, and a more complete treatment must await further research. Such cases are noted in the text.

The contexts have thematic bounds, but also geographic and temporal limits as well. For each context, the historical narrative and property types are defined by time periods within which that particular theme was of the greatest significance in the city's history; these are clearly stated in the context titles. The geographic limits were established by a study of where in the city the events associated with the theme primarily took place. The geographic boundaries for the contexts varied with time period, but all occurred within the corporation limits of the City of Steubenville. Early events generally occurred within the 1851 city limits (refer to the annexation map for details), which encompassed primarily the flat land east of the hilltops and north of the steel mill. Later events included the much larger 1871 boundaries, which encompassed all of the hilltops east of Brady Estates, as well as areas to the north and south, including Beatty Park, Union Cemetery, and most of the steel mill land. Later events included these earlier areas and also the large tract annexed in 1926, which covered Brady Estates and extended well to the west.

## Evaluation Criteria

In the "Significance" and "Registration Requirements" sections that follow each property type below, the criteria used are those for the National Register of Historic Places. These criteria have been developed by the National Park Service as a succinct means of describing the importance of historic resources and to set a measurable standard by which these resources can be evaluated for National Register listing. A district, site, building, structure or object must meet at least one of the criteria in order to qualify for listing.

The following Criteria for Evaluation have been taken from National Register Bulletin 15, published by the National Park Service, U.S. Department of the Interior:

The quality of significance in American history, architecture, archaeology, engineering and culture is present in districts, sites, buildings, structures and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association, and:

- A. That are associated with events that have made a significant contribution to the broad patterns of our history (association with important events); or
- B. That are associated with the lives of persons significant in our past (association with important person[s]); or
- C. That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction (representative of important design/construction); or
- D. That have yielded, or may be likely to yield, information important in prehistory or history (potential to provide important information).

### Integrity

In addition to meeting at least one of the Criteria for Evaluation, properties must also possess integrity, which is defined as the ability of a property to convey its significance. There are seven aspects of integrity, and in order to be considered significant a property must possess some combination of these aspects.

### Assessing Integrity in Historic Properties (taken from National Register Bulletin 15)

Integrity is based on significance: why, where, and when a property is important. Only after significance is fully established can you proceed to the issues of integrity.

The steps in assessing integrity are:

\*Define the essential physical features that must be present for a property to represent its significance.

\*Determine whether the essential physical features are visible enough to convey their significance.

\*Determine whether the property needs to be compared with similar properties. And,

\*Determine, based on the significance and essential physical features, which aspects of integrity are particularly vital to the property being nominated and if they are present.

Ultimately, the question of integrity is answered by whether or not the property retains the identity for which it is significant.

#### Aspects of Integrity

The following explanation of the seven aspects of integrity also is taken from the National Park Service's National Register Bulletin 15:

1. Location. Location is the place where the historic property was constructed or the place where the historic event occurred.

The relationship between the property and its location is often important to understanding why the property was created or why something happened. The actual location of a historic property, complemented by its setting, is particularly important in recapturing the sense of historic events and persons. Except in rare cases, the relationship between a property and its historic associations is destroyed if the property is moved.

2. Design. Design is the combination of elements that create the form, plan, space, structure, and style of a property.

It results from conscious decisions made during the original conception and planning of a property (or its significant alteration) and applies to activities as diverse as community planning, engineering, architecture, and landscape architecture. Design includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials.

A property's design reflects historic functions and technologies as well as aesthetics. It includes such considerations as the structural system; massing; arrangement of spaces; pattern of fenestration; textures and colors of surface materials; type, amount, and style of ornamental detailing; and arrangement and type of plantings in a designed landscape.

Design can also apply to districts, whether they are important primarily for historic association, architectural value, information potential, or a combination thereof. For districts significant primarily for historic association or architectural value, design concerns more than just the individual buildings or structures located within the boundaries. It also applies to the way in which buildings, sites, or structures are related: for example, spatial relationships between major features; visual rhythms

in a streetscape or landscape plantings; the layout and materials of walkways and roads; and the relationship of other features, such as statues, water fountains, and archaeological sites.

3. Setting. Setting is the physical environment of a historic property.

Whereas location refers to the specific place where a property was built or an event occurred, setting refers to the character of the place in which the property played its historical role. It involves how, not just where, the property is situated and its relationship to surrounding features and open space.

Setting often reflects the basic physical conditions under which a property was built and the functions it was intended to serve. In addition, the way in which a property is positioned in its environment can reflect the designer's concept of nature and aesthetic preferences.

The physical features that constitute the setting of a historic property can be either natural or manmade, including such elements as:

\*Topographic features (a gorge or the crest of a hill);

\*Vegetation;

\*Simple manmade features (paths or fences); and

\*Relationships between buildings and other features or open space.

These features and their relationships should be examined not only within the exact boundaries of the property, but also between the property and its surroundings. This is particularly important for districts.

4. Materials. Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property.

The choice and combination of materials reveals the preferences of those who created the property and indicate the availability of particular types of materials and technologies. Indigenous materials are often the focus of regional building traditions and thereby help define an area's sense of time and place.

A property must retain the key exterior materials dating from the period of its historic significance. If the property has been rehabilitated, the historic materials and significant features must have been preserved. The property must also be an actual historic resource, not a recreation; a recent structure fabricated to look historic is not eligible. Likewise, a property whose historic features and materials have been lost and then reconstructed is usually not eligible.

5. Workmanship. Workmanship is the physical evidence of the crafts of a

particular culture or people during any given period in history or prehistory.

It is the evidence of artisans' labor and skill in constructing or altering a building, structure, object, or site. Workmanship can apply to the property as a whole or to its individual components. It can be expressed in vernacular methods of construction and plain finishes or in highly sophisticated configurations and ornamental detailing. It can be based on common traditions or innovative period techniques.

Workmanship is important because it can furnish evidence of the technology of a craft, illustrate the aesthetic principles of a historic or prehistoric period, and reveal individual, local, regional, or national applications of both technological practices and aesthetic principles. Examples of workmanship in historic buildings include tooling, carving, painting, graining, turning, and joinery. Examples of workmanship in prehistoric contexts include Paleo-Indian clovis projectile points, Archaic period beveled adzes, Hopewellian birdstone pipes, copper earspools and worked bone pendants, and Iroquoian effigy pipes.

6. Feeling. Feeling is a property's expression of the aesthetic or historic sense of a particular period of time.

It results from the presence of physical features that, taken together, convey the property's historic character. For example, a rural historic district retaining original design, materials, workmanship, and setting will relate the feeling of agricultural life in the 19th century. A grouping of prehistoric petroglyphs, unmarred by graffiti and intrusions and located on its original isolated bluff, can evoke a sense of tribal spiritual life.

7. Association. Association is the direct link between an important historic event or person and a historic property.

A property retains association if it is the place where the event or activity occurred and is sufficiently intact to convey that relationship to an observer. Like feeling, association requires the presence of physical features that convey a property's historic character. For example, a Revolutionary War battlefield whose natural and manmade elements have remained intact since the 18th century will retain its quality of association with the battle.

Because feeling and association depend on individual perceptions, their retention alone is never sufficient to support eligibility of a property for the National Register.

#### Applying the Aspects of Integrity to the Criteria for Evaluation

The specific aspects of integrity which must be retained will vary with each criterion for evaluation. For each of the criteria, certain aspects of integrity are, as a rule, more important than others. These are discussed in general terms below and will be discussed in greater detail under the registration requirements for each property type.

Criterion A (association with important events)  
and

Criterion B (association with important person[s])

LOCATION - The property should be on its original site or, if it has been moved, its site should be the place where the property made its contribution to the history of Steubenville.

DESIGN - The property's original form, plan, site placement, structure, and style should be intact. Existence of additions will not necessarily keep a property from qualifying, but the additions must not obscure the basic original design elements.

ASSOCIATION - The property should be directly linked to the event(s) or person(s) which contributed to the history of Steubenville, during the time period in which that contribution was made.

Criterion C (representative of important design/construction)

DESIGN - The property's original form, plan, site placement, structure, and style should be intact. Existence of additions will not necessarily keep a property from qualifying, but the additions must not obscure the basic original design elements.

MATERIALS - The property should retain key materials put in place during its construction period. This includes wall materials/surfaces, trim at window and door openings, roof structure and shape, and major ornamental elements. Ideally, elements such as doors, windows, and original roofing materials should survive. Lack of one or more of these will not keep a property from qualifying, however, if the key exterior materials are intact.

WORKMANSHIP - Important examples of workmanship, whether original or added at some time during the period of significance, should survive intact. Such features, in buildings built in architectural styles, might include elements such as brackets, panels, balustrades, or leaded glass windows. For many buildings, porches are important components of their overall workmanship. In vernacular buildings, stylistic treatments and decorative elements may be minimal or non-existent. In such buildings, simple finishes and architectural elements usually are the important examples of workmanship. These might include plain window and door trim, simple paneled doors, or plain, unornamented porches.

FEELING - The property should retain sufficient historic physical features so that, viewed as a whole, the property represents a specific period of time. This aspect of integrity is important for Criterion C but applies equally to Criteria A and B as well. As a general rule, if the property would still be recognizable to a person who lived during the period of significance, then it would be considered as evoking the feeling of the period. As under the aspect of Design, the existence of later additions or alterations will not necessarily keep a property from qualifying. Indeed, these can serve to illustrate how buildings change over time with changes in economic conditions and aesthetic attitudes. Thus a building with

additions or alterations may represent a span of time rather than a point in time. In any case, the feeling of that time period should be intact in the form of physical building features.

SETTING - This aspect of integrity comes into play in evaluating historic districts. It should be noted that most residential and commercial structures in Steubenville will qualify as components of historic districts rather than as individual entities. It is a fact of Steubenville architecture, as in many communities, that most buildings do not have enough individual merit to warrant individual National Register or local registration. It is, instead, their importance as part of a cohesive district that matters. In historic districts, the aspect of setting includes not just the buildings but also the spaces between them, the spatial relationships among them, and what actually fills those spaces. In the downtown commercial district, for example, continuous street facades of uniform setback, with fairly wide continuous sidewalks are important components of the area's setting. Significant loss of buildings by demolition, or a change in setback in a new building could seriously affect the area's setting. The same is true in residential areas such as North Fourth Street, where some important components of the setting include the building setback, sidewalks, large trees and grassy lawns, and spacing between houses.

Criterion D is not discussed here because it typically deals with archaeological resources. Archaeological potential may exist throughout downtown Steubenville but has not been extensively researched and cannot be assessed here. Since Steubenville has been the focus of fairly intensive development since the early 19th century, successive stages of construction, demolition and rebuilding in the downtown probably have left numerous below-ground deposits such as trash heaps, trash-filled wells and cisterns, cellars and basements, and other features that may contain information about the development of Steubenville.

## Historical Background of Steubenville

Located some 65 miles down the Ohio River from that important artery's source in downtown Pittsburgh, Pennsylvania, Steubenville lies on the river's west bank at a point where the Ohio's twists and turns cause it to run in a nearly straight north-south direction. To the east, the hills of the West Virginia Panhandle rise abruptly from the river's bank, with no flat bottom or terrace land to encourage development. To the west, the hills are just as high, rising from an elevation of 640 feet at river level to over 1,000 feet along the nearby hilltops, but the river's terrace here is about a half-mile wide and is well above river flood levels. Broad and sloping, the terrace provides an ideal site for settlement and development.

From the middle of the 18th century until final subjugation of Ohio's Indians in 1795, the Ohio Valley and the area that would become Jefferson County were the locus of considerable White/Indian interaction and conflict. This was the era when eastern Whites began exploration of the lands beyond the Ohio with an eye to eventual acquisition and settlement. Among the explorers was George Washington, who is said to have made his only visit to what would become Ohio when he stopped at Mingo Town in today's Jefferson County. This was at what is now Mingo Junction, a little south of Steubenville.

Government policy was to discourage settlement on Indian lands beyond the Ohio, to the extent that squatters' cabins were regularly burned by government troops. The squatters persisted anyway, leading to repeated instances of Indian attacks to dislodge the Whites and White attacks in retaliation. Yielding to pressures for settlement of the Northwest, in 1785 (after Virginia ceded its Colonial claim to the entire Northwest) the government passed the pivotal ordinance creating the U.S. land system and establishing the Seven Ranges of public lands. From a beginning point at the Ohio/Pennsylvania border upriver from Steubenville, the Seven Ranges ran 42 miles to the west, forming seven vertical ranks of townships, with each township six miles square. The system of surveying townships into 36 one-square-mile sections of 640 acres each, and then further subdividing these into quarter- and even eighth-section parcels, began here and set the pattern for all future subdivision and sale of U.S. public lands.

To protect the surveyors from the still-hostile Indians during the survey of the Seven Ranges, in 1786 the government selected the site of Steubenville as an ideal location for a fort. Completed early in 1787, the fort was named for the Prussian nobleman who had provided vital assistance to the American colonies during the Revolutionary War. Though abandoned after only three months when its troops were moved downriver to Fort Harmar, Fort Steuben showed the benefits of its commanding site. Well above the river but with good views to north and south, the site was well drained and had good soil, with abundant fresh water and good timber. The fort itself lasted until about 1790, when it was burned.

Bezaleel Wells, a government surveyor in the Seven Ranges, was offered a choice of cash or land for his work, and he chose the latter. In 1796 he took title to 1,100 acres on the west side of the Ohio, at the site of Fort Steuben. Today's North Street marks what was the northern boundary of

his land. James Ross held the land just to the north, and in 1797 he and Wells laid out a town, giving it the name Steubenville. Composed of some 250 inlots and outlots, the town was made up of a pattern of streets and alleys nearly identical to today's downtown street pattern. Based on a simple grid pattern, the plat extended from North Street to South Street; Washington, Market and Adams were the other east-west streets, with Market being the primary commercial street as well as the street extending westward up the hill into the county's hinterlands. Water, High, Third and Fourth were the north-south streets, with Third being the route out of town along the river to the north and south. In 1800 the federal government established a land office in Steubenville to facilitate sale of public lands.

The earliest Steubenville homes and businesses clustered close to the river, where commercial, industrial and transportation activity was the greatest. The earliest development occurred parallel to the river along Water Street, into which roads from the north and south fed their traffic. Water Street ran along the top of the steep river bank, while sloping cross streets connected with High Street and other streets farther west. The earliest homes and businesses were built along these north-south axes close to the river. Residences were mixed in with commercial and industrial buildings, which housed enterprises typical of a town's early settlement era, such as grist and flour mills, tanneries, distilleries, and paper and saw mills. Today there are no above-ground remains of these enterprises, and ground-disturbing activities such as railroad and highway development, as well as construction of later buildings, probably have seriously disrupted archaeological remains. Nonetheless, this area may have the potential for below-ground archaeological remains from the city's earliest years.

The general desirability of Steubenville's site caused people to build farther west fairly early in the town's history. Though Washington Street (today the main artery to the western part of the city) was still only a muddy path as late as the 1820s (the population then was about 1,000), by the 1850s Steubenville's orientation had shifted away from the river. The population by this time had risen to over 3,000.

Steubenville's early development naturally occurred along and close to the river because this was the principal transportation artery into and out of town. This remained true until the 1850s, when two railroads were constructed into Steubenville: the Steubenville & Indiana and the Cleveland & Pittsburgh entered Steubenville in 1853 and 1855, respectively, running on parallel north-south routes several blocks apart. The C&P ran along the east side of Water Street along the river bank, while the S&I ran between Sixth and Seventh Streets. These rail routes created distinct eastern and western boundaries for the downtown area, and natural conditions set northern and southern boundaries.

To the north, steep hills dropping right to the river's west bank cut off the terrace on which Steubenville sat. This sense of closure was further accentuated in the mid 1860s when the S&I built a high embankment as an approach to its Ohio River bridge. To the south, the terrace dropped off quickly into the valley of Wells Run, effectively sealing off development to the south, and just below Wells Run the hills closed in again, as they

did to the north. In 1856 the city was blocked from further development in its south end by the establishment of the Jefferson Iron Works along Wells Run.

Thus, natural conditions, together with early transportation and industrial developments, created a well-defined area about six blocks by ten in which the heart of downtown Steubenville grew from the 1850s on. As early as the mid-1850s, the grid pattern of streets in this area was rapidly filling in with residential, commercial and institutional construction. Between the mid-1850s and 1870, development patterns became firmly established: the northern and southern ends of the downtown area became primarily residential, with the northern end considered the more desirable and fashionable; these were separated by the downtown commercial core, which was centered on Market and Fourth Streets; the downtown core was listed in the National Register of Historic Places in 1986 as the Steubenville Commercial Historic District. The residential area centered on North Fourth Street was listed in 1991 as the North End Neighborhood Historic District.

The seven contexts that follow build upon this account of early Steubenville development, showing how the major themes of the city's history developed and in the process shaped the community's physical environment.

## INDUSTRIAL DEVELOPMENT IN STEUBENVILLE, 1847 - 1959

Steubenville's industrial development during the last half of the 19th century and the first half of the 20th century was the most important factor which influenced the city's growth and the development of its built environment. While Steubenville is probably best known as a steel manufacturing center (steel is still the major industry today), there were other important industries -- including production of pottery and clay products, glass manufacturing and coal mining -- which were active during this period. An abundance of raw materials and an excellent transportation network led to extensive industrial development between 1880 and 1929. This led to rapid growth of the city's population, including a large influx of immigrant groups; development of new residential areas to house incoming residents; and expansion of the local transportation system and commercial district to serve the expanding population. From the late 1920s through the Second World War, the city's industrial prosperity continued (even in the 1930s, when the deflation of the Depression facilitated new investment by the steel companies), and it is only since the late 1950s that significant reverses have been experienced in the city's industrial base.

Steubenville's location in the Ohio Valley, together with the surrounding topography and geological features, had a direct influence on the city's industrial development. Even a casual observer would note the dramatic difference in elevation between downtown Steubenville and the upper areas of the city to the west; the difference is somewhat over four hundred feet. The Ohio River shoreline at Steubenville normally is at 640 feet above sea level (asl), and the top of the bank is 670' asl. The ground on which the city is built rises gradually from the top of the bank, reaching 720' at the corner of 4th and Market streets and 780' at the foot of the Market Street Hill, a little over a half-mile west of the river bank. On the heights, Belleview Boulevard is at 1050', Pleasant Heights is at 970', Ross Park Boulevard is at 980' and Brady Circle is at 1059' asl.

The differences in elevation are due, of course, to the action of the Ohio River, which formed in the post-glacial period. While the glaciers never extended as far as Steubenville, this area was affected by them nonetheless. Fed by the melting glacier to the north, the river eroded down through four hundred feet of strata to its current level. These strata are part of the western foothills of the Allegheny Mountains, which begin in Jefferson County. Laid over one another in these hills are strata of fireclay, sandstone, limestone, iron ore and coal, representing all the ingredients necessary for the establishment of an iron and steel industry, as well as other kinds of industry.

As the Ohio River gradually cut down into its gorge, deeper and deeper cross gorges formed from the action of streams feeding into the valley from the hills west of the river. These streams cut through and exposed the various strata of useful minerals, making them readily accessible for exploitation.

Another effect of tributary creek erosion was the disintegration of exposed hilltop limestone deposits, and the deposition of eroded materials both on the hilltops and in the terraces and bottom lands of the Ohio River. In the hilltop areas of Jefferson County, where the elevations average 1300'

and rise to a maximum of 1434', the decomposed limestone strata formed a soil which was, according to Doyle's 1910 history, ". . . better adapted to the raising of fine woolled Merino sheep than any other section in the United States."

In the terraces and bottom lands along the river, large amounts of sand and gravel, which originated in the glacier to the north as it melted, were deposited by river action and then were overlaid both by rich soils from the hills and by silts carried and dropped by the river. In its naturally meandering course, the river left a large, relatively flat terrace at the site of Steubenville; other similar terraces nearby were at Toronto (above Steubenville) and Brilliant (below Steubenville). These flat lands were ideal for farming and, later, for industrial and urban development.

### **Industry Prior to 1847**

With one exception -- the production of wool -- the earliest industries in Steubenville were not unlike those in dozens of newly founded communities scattered throughout Ohio. They mainly served the needs of the local population -- grist and flour mills, tanneries, distilleries, and paper and saw mills. Boatbuilding, which naturally grew out of the city's riverside location, was another early industry. Unfortunately, other than passing mention in secondary sources, available research materials provide no detailed information about this industry, the people associated with it, or the time period in which it occurred.

By 1820 Steubenville was establishing itself as a center for wool production, the result of the city's founder, Bezaleel Wells, having brought a single pair of sheep from the east about 1814. Steubenville reportedly had one of the first woolen mills in the U.S. (this is mentioned in a secondary source, but no information confirming this claim has come to light), and during the first half of the 19th century (until the end of the Civil War) wool was Steubenville's largest industry. During the peak years there were twelve woolen mills operating in the city. Unfortunately, available sources provide no information as to beginning and ending dates for these enterprises, nor can their locations or appearance be determined. Further research in this area, especially regarding the "earliest" mill, should be conducted.

As noted above, hilltop farmlands west of Steubenville, as well as in the West Virginia panhandle and in western Pennsylvania, were found to have a limestone-based soil particularly suited to the raising of Merino sheep with high-quality wool. Whether grown here or elsewhere, the wool of the sheep bred in Jefferson County was known as Ohio wool. The proximity of Steubenville to sheep-raising lands, together with the Ohio River and local creeks as a water source for power, processing and transportation, enabled the woolen industry to take root and prosper in the pre-Civil War era. After that time, a combination of limited capital, indifferent management and competition from other parts of the country now united by railroads led to the decline and disappearance of Steubenville's wool industry.

### **The Steel Industry**

By accident of geological history, the hills around Steubenville consisted of strata of useful minerals, including coal and limestone, two basic

ingredients of the iron and steel industry. The third ingredient, iron ore, was available at numerous nearby Ohio Valley locations, including Steubenville itself. After the Civil War period, Lake Superior iron ores became readily available by means of rail and water shipment via Cleveland, so the stage was set for development of Steubenville's iron and steel industry. This was enhanced by the ready availability, locally, of natural gas, which was useful in various iron and steel finishing processes.

The Ohio Foundry, which is recognized as the precursor of the steel industry in Steubenville, was established in 1847 by William Sharp. The first works were located near the Steubenville Coal and Mining Company's "High Shaft" on Liberty Avenue, below the Market Street hill. The Ohio Foundry made plows and stoves and the company from its early years sold its products as far away as New Orleans by means of flatboats. The firm was a pioneer in selling its wares by wagon-peddler from farm to farm and town to town, and its astute marketing and selling methods led to increasing demand. As an example of its methods, the company accepted payment in horses rather than cash, and these were driven east and sold either for cash or for iron which was then transported back to Steubenville for processing. In 1860, the foundry moved to 118-124 North Fifth Street, a building known as the "Gymnasium," which was converted into a foundry. Mr. Sharp, who was opposed to manufacturing anything that could be used for killing, refused to make weapons during the Civil War. When Sharp's son entered the business in 1865, the name was changed to W.L. Sharp and Son. The foundry expanded to the adjoining property on North Fifth St. (later the Paramount Theater site) in 1885, where business was conducted until the entire foundry was destroyed by a fire in 1891. It was rebuilt along the Pittsburgh, Cincinnati, Chicago and St. Louis Railroad (later the Pennsylvania Railroad) at Slack Street. In 1893, A.B. Sharp, the grandson of the founder and the third generation of the family, entered the business and the name was changed to Ohio Foundry Company. At that time it manufactured coal and gas grates and stove and fireplace linings. The Steubenville Centennial publication (1897) stated that the Ohio Foundry was "symbolic of the city's industrial evolution. . . . Established half a century ago, it is still a leading industry of the city and its wares are shipped to all parts of the continent." The foundry employed 150 people at the turn of the century and its trade extended from Florida to California and throughout Canada.

Another grandson joined the firm in 1906 and was responsible for the design of many of the new products introduced by the Ohio Foundry through the years. The fourth generation entered the business in 1924. When Steubenville celebrated its sesquicentennial in 1947, the Ohio Foundry & Manufacturing Co. continued to operate under the direction of the fourth generation of the Sharp family. Throughout its long history, the company manufactured heating and cooking appliances to accommodate nearly every type of fuel -- wood, coal, coke, oil, electricity, natural gas and liquefied petroleum gas. They also made fancy coal and gas fireplace grates plated in brass, copper, bronze and oxidized colors. The firm ceased business in 1967 after 120 years of activity.

In 1856 Frazier, Kilgore & Co. erected a rolling mill known as the Jefferson Iron Works. A rolling mill used large shaped metal rollers to form hot iron or steel into useful shapes such as I-beams or railroad rails. The Kilgore mill was located on the site of the present

Wheeling-Pittsburgh Steel Plant. The company was purchased by Spaulding, Woodward & Co. in 1859 and became known as "The Jefferson" in the mid-1880s. A steel plant was erected in 1890. These works consisted of one blast furnace with a capacity of 200 tons of pig iron per day and a cut nail factory with a capacity of 8,000 kegs of nails a week.

In 1900, the Jefferson Iron Works was purchased by the LaBelle Iron Works of Wheeling. LaBelle also purchased a 25-acre tract on the south side of the city and constructed one of the largest iron and steel plants in the U.S. In 1901, LaBelle started construction on an open hearth and six 50-ton furnaces. A sheared plate mill unit (which cut large flat pieces of metal into smaller pieces for sale or for further processing) was also constructed the same year. LaBelle then had a production capacity of 400 tons/day per furnace. In 1915 LaBelle built a Koppers by-product plant in West Virginia directly opposite the Steubenville Works and constructed its own steel railroad bridge connecting them in 1915. The by-product plant recovered the useful gases, tars and liquids produced in the coking process (the heating of coal in the absence of air to produce high-carbon coke as blast furnace fuel). The LaBelle mill was by this period one of the nation's important independent steel producers. The first stage of the war boom began in 1916 when foreign governments began purchasing millions of dollars' worth of LaBelle products. In 1919, following the war, LaBelle Iron Works reported income of over \$3.85 million.

1919 was also the year of a nationwide steel strike. The root issue was United States Steel Co.'s refusal to acknowledge the right of workers to organize; it was opposed by the American Federation of Labor, which called a strike on September 22, 1919. This strike, like the equally unsuccessful Homestead Strike in Pittsburgh in 1892, achieved nothing for the workers (except an increased fear among Americans of foreign immigrants) and in fact weakened the effort to organize the steel industry. Though some 365,000 workers struck in 1919, USS and other companies were able to hire new immigrant employees, and the strike was called off early in January, 1920. The effect, in Steubenville as elsewhere, was no more than a short-term reduction in steel production and no appreciable improvement in the lot of mill workers.

Dohrman Sinclair, a banker, was also one of Steubenville's leading promoters. In 1902, Mr. Sinclair induced the Follansbee Brothers to erect a large tin mill opposite the lower end of the city on the West Virginia side. In return, he promised to provide a means of transportation for workers from Steubenville to get to the mill. Sinclair formed the Steubenville Bridge Company to construct the Market Street Bridge and the Tri-State Traction Company to provide streetcar service across the bridge to the mill. Sinclair was also responsible for encouraging Phillips Sheet and Tin Plate Co. to erect a plant on the land adjoining Holliday's Cove, West Virginia, a little north and east of Steubenville.

Growth of the automobile industry after about 1915 meant an increasing demand for sheet steel, which was used in making auto bodies and parts such as hoods and doors. Through its excellent rail connections, Steubenville had easy access to the automobile plants of Detroit, and elsewhere in the midwest as the auto industry grew, and sheet steel became a principal product of LaBelle Iron Works and its successors; it remains an important product of Wheeling-Pittsburgh Steel Co. today.

1920 was a significant year for the steel industry in Steubenville. Three independent steel companies -- LaBelle Iron Works, Whittaker-Glessner Company and the Wheeling Steel and Iron Company were merged and became the Wheeling Steel Corporation. In 1923, a new blooming mill and continuous sheet bar mill (intermediate steel products which are then further processed) opened. The Steubenville finishing mills then became independent of purchased raw material. In 1926, the #2 blast furnace was blown out (shut down) and completely rebuilt, doubling its capacity to 800 tons/day. The #1 furnace was also rebuilt and enlarged. In 1929, the continuous hot strip mill was put into operation. For a short time, the Steubenville Works held the world's record for speed and size in producing hot-rolled sheet steel. Production slowed during the 1930s but with the advent of World War II in Europe, the plant was put back into full operation and produced pig iron, scrap and armor plate. In 1946, the Wheeling Steel Corporation purchased the Mingo Works (which had ceased operation in 1945) of Carnegie-Illinois Steel Corporation and changed the name of the plant to Steubenville-South and the name of the company to Wheeling Steel. The company sold \$110,000,000 worth of products and employed 14,928 people in 1946. In 1947, Steubenville-South was the largest Wheeling Steel plant and the company was the city's largest employer and taxpayer. In the 1960s the company became Wheeling-Pittsburgh Steel, under which name it operates today. Though it has seen financial troubles and employs far fewer people than in the past, the company remains critical to Steubenville's economy.

Although most of the facilities are located in West Virginia, the Weirton Steel Co. also had an impact on the industrial development of Steubenville. Ernest T. Weir purchased the Jackson Sheet & Tin Plate Co. of Clarksburg, West Virginia, in 1905. In 1909, Weir purchased land across from Steubenville and erected a 10-mill plant. The company underwent rapid expansion during the 1910s. In 1910, ten more mills were constructed; 12 additional mills were added in 1911 with the purchase of the Pope Tin Plate Co. (formed in 1901), which became the Steubenville Plant. In 1914, 4 hot mills were constructed with another 2 added in 1915 to supply the raw material to the cold rolled strip steel plant which was completed in 1913. By 1920, the plant had 7 open hearth furnaces, a 40" blooming mill, and 18" and 21" continuous rolling mills. The company had also purchased ore properties in the Lake Superior region, and coal lands in Pennsylvania to provide the raw materials for the plant. In 1925, the first 48" continuous hot strip mill in the country was built. Between 1926-28 the plant added more than 3,000 employees and increased the annual payroll by several million dollars. Weirton Steel was considered an innovator in the cold reduction process, developed during the late 1920s, in which steel shapes were made thinner and flatter without having to heat the metal. Although 1930 was the start of the Depression, Weirton Steel believed that a superior product could be produced by the cold reduction process and installed new equipment in 1932. It was ahead of the other independent mills by several years. In 1938, a corner of the old hot mill was converted to an experimental 10" wide electrolytic tinning line (to produce tin-coated steel for such uses as tin cans), and by 1941 a decision was made to build a 38" electro-tinning line (whether it actually went into production is not known). Through cooperation between Weirton Steel and DuPont Electroplating Division, a Halogen Tin Bath, yet another way of

tin-plating steel, was successfully developed by 1942. During World War II, Steubenville's industries turned out steel for tanks, guns, and shells. After the close of the War, Weirton Steel announced a \$12,000,000 modernization plan. Weirton Steel continues today as an important independent steel producer.

In addition to those already mentioned, there were other steel and iron industries active in Steubenville during this period. The Steubenville Furnace and Iron Co. was formed in 1874 and was later purchased by the Riverside Iron Co. of Wheeling. Its plant was located immediately north of the Pittsburgh, Cincinnati, Chicago and St. Louis Railroad (later Pennsylvania Railroad) bridge. In 1897, the plant had a capacity of 200 tons of pig iron/day and employed 150 people. All of the plant's product was consumed by Riverside steel plant and finishing mills in Wheeling and Benwood, West Virginia. The company had ceased business by 1915. The Steubenville Foundry and Machine Works, also known as James Means & Co., was one of the first foundries established in the area, in 1816. At the turn of the century, the company manufactured steam sewer pipe presses, pipe and tile dies, pug mills, clay grinding and tempering pans, blast furnace and coal mining machines, steam engines and all kinds of brass and iron castings. It was located on the river, adjoining the Cleveland & Pittsburgh Railroad between Dock and North Streets, and employed 60 men. The company was out of business by 1921.

Steubenville's iron and steel plants generally were located along the Ohio River, usually in the southern part of the city; this is where the largest mills eventually grew. This location enabled them to take advantage of abundant open land and both rail and water transportation, modes which remain important today. The same patterns can be seen in nearby communities such as Follansbee, Weirton and Mingo Junction, where the steel mills occupied flat bottom lands on the river, adjacent to or straddling rail lines. Other steel facilities, both south to Wheeling and north to Pittsburgh, made the upper Ohio Valley almost a continuous industrial corridor.

In fact, Steubenville's iron and steel plants were part of the great axis of midwestern steel production which extended from Lorain and Cleveland on Lake Erie, through Youngstown and the Mahoning Valley, to Pittsburgh and on southeast to the Connellsville Coke Region. This vast assemblage of iron and steel facilities formed an elongated oval oriented northwest-southeast and measuring some 175 miles long by 50 miles wide. Steubenville was located a little east of midway along this axis, very close to the area's southern limit.

In the past, Steubenville's iron and steel facilities, big and important as they were locally, were dwarfed by steel centers such as Cleveland, Youngstown and Pittsburgh. As late as the early 1970s, these centers outweighed Steubenville in just about every measure — employees, number of blast furnaces, amount of production, and range of products. In the intervening two decades, however, the wrenching changes in the steel industry have changed the picture. Because of labor costs, foreign competition, poor management, plant obsolescence, automation, and other factors, great structural changes have reshaped "Big Steel." Reductions in the labor force, closing of plants, and downsizing of facilities have meant that steel is no longer produced within the city of Pittsburgh; Youngstown

has no steel plants; and Cleveland's steel facilities are much reduced in size. There have been changes in steel facilities in and around Steubenville as well, but in relative terms the city, and neighbors such as Weirton and Mingo Junction, have fared much better than other places. As a result, Steubenville is a relatively more important steel center than in the past, with more of its historic steel infrastructure surviving and still in use.

### **The Glass Industry**

As noted above, glacial deposits of sand in the terraces and river bottoms along the Ohio Valley at places such as Steubenville provided a ready source of raw material for glassmaking. Reddick, McKee & Co. formed a glass company in 1874 and purchased the building of the Acme Mower and Reaper Works (established in 1870 and in operation for only a few years), located along the Pittsburgh, Cincinnati, Chicago & St. Louis Railroad tracks near Franklin Avenue. Reddick, McKee was succeeded by Gill, Mitchell & Co. that same year. Business was conducted under this name until the spring of 1875 when Mr. Mitchell retired and Gill Brothers & Co. was formed to operate the Acme Glass Works. At the time the Gill brothers took over Acme Glass, there was one small 11-pot furnace and the old Acme Reaper buildings. In 1877, an additional 13-pot furnace was built and the buildings expanded; in 1887 a 16-pot furnace was added. It was at this time one of the largest glass furnaces in the U.S. All of the furnaces operated 24 hours a day. The business was limited to the production of lamp chimneys (over 100 styles were available) and silvered glass reflectors. The Steubenville Centennial publication described Acme Glass in 1897 as the "leading manufacturing plant in Steubenville and the largest lamp chimney factory under one roof in the United States." The capacity of the works at the time was from 400,000-500,000 chimneys per week, employing about 700 workers, growing to 1,000 employees and 600,000-800,000 chimneys per week in 1911. With the growth of electric lighting, the lamp chimney business proved to have a limited future, and Acme Glass ceased operation between 1924 and 1926.

In addition to Acme Glass Works, there were other glass manufacturers formed in the city. The Beatty Glass Co. was established in 1836 and erected a new plant on South Third Street in 1846. By the mid 19th century, the old plant was demolished and a new building constructed. The plant was sold to the U.S. Glass Company in 1890 and was out of business by 1921. The Steubenville Flint Glass Bottling Co. was established in 1882 and ceased production by 1921. The Jefferson Glass Co. was established in 1901 and manufactured crystal, colored and opalescent tableware and novelties. Its plant was located at 324 North Seventh Street and employed 200 people by the 1940s, but it had disappeared from the city by 1950.

### **Extractive Industries: Coal Mining**

As has already been noted, numerous strata of coal and other minerals were laid down during the geological history of the Steubenville area. Specifically, the coal veins identified as Numbers 1, 2, 3, 4, 5, and 6 are located in the strata around and under the city, as is the vein called Pittsburgh #8. #6 was known as the "Big Vein," and it extended from Perry County, some 100 miles west of Steubenville, eastward all the way through the state of Pennsylvania. Pittsburgh #8 was very accessible and easily

mined in the hilltops near the city, and Doyle's history estimated that this vein provided about 3/4 of domestic coal consumption in Jefferson County for nearly all of the 19th century. All of the coal in the Steubenville area — indeed in all of Ohio, West Virginia and all but a small part of eastern Pennsylvania, was bituminous rather than anthracite coal. The local coal was adequate for coking purposes, but it had a relatively high level of impurities such as sulfur, so the iron and steel producers used coal from other areas in their coking ovens as soon as adequate transport facilities were in place.

During water-drilling operations in 1829, a vein of coal was discovered within the city limits. In 1856 a company was organized by Messrs. Borland, Reynolds and Manful who took a five-year lease on the "High Shaft" located near the corner of Market and Liberty Streets. After sinking the shaft, they found only a 3-3/4' thick vein rather than the 8' predicted and abandoned the lease. By 1858, William Averick purchased Mr. Manful's investment and had the coal tested for its qualities for gas purposes. It was excellent and the company established a market for "High Shaft Coal" for the manufacture of gas, coke and iron. The lease was extended and by 1919, the output of the mine was 125 tons/day with a tipple capacity of 75 tons. By 1947, the output had increased to 700 tons/day and the tipple to 1400 tons/day. The seam, which is Ohio #6 coal or the upper Freeport vein, was also found in Pennsylvania and West Virginia in thicknesses ranging from 3' to 6'. In 1947, it was predicted that there was enough unmined coal acreage to supply the city of Steubenville for at least the next 40 years. It turned out, however, that the Steubenville Coal & Mining Co., into which the High Shaft's owner had evolved, lasted only until about 1970, and no coal appears to have been taken from the High Shaft since the mid-1960s.

The Teramana Brothers Coal Co. was established in 1924 for both strip and deep coal mining. The mine, located on Fernwood Road, employed 45 men in 1947. The sales offices were located at 323 N. Fifth Street. The company lasted into the late 1970s. The Brettell Coal Co. was established by Thomas and Edward Brettell in 1902. During its peak years of operation, the company employed 100 people. It was still operating at the time Steubenville celebrated its sesquicentennial in 1947 and moved to Mingo Junction by 1959. Jefferson Coal and Iron operated "Bustard's Shaft" located near the river at the northern limits of the city. At the turn of the century, 75 miners were employed and 10 cars of coal were mined daily. The company furnished coal to the Cleveland & Pittsburgh Railroad's engines and cars. The date of its inception is unknown, and it had ceased business by 1950.

### **Extractive Industries: Pottery/Clay Industry**

Clay suitable for pottery and firebrick production was abundant in the geologic strata of the Ohio Valley. Once adequate rail transportation was put in place in the period after the Civil War, the ceramics industry developed rapidly. The city of East Liverpool, some 25 miles north of Steubenville and also located in the Ohio Valley, had the greatest concentration of ceramics producers in the Ohio Valley region, and it remains (together with its "sister" city of Newell, West Virginia) a ceramics center today. Steubenville, by comparison, had a much smaller ceramics industry, but it was important locally as a source of jobs and tax revenues. As noted below, Steubenville Pottery Co. gained lasting fame

because of its line of contemporary ware designed by Russell Wright. Though the plant closed in the 1950s, the company is well-remembered today because of these well-designed (and now very collectible) items.

The Steubenville Pottery Co., one of the earliest potteries in the area outside of East Liverpool, was established in 1879 and expanded rapidly during the 1880s. It was located at the intersection of the Pittsburgh, Cincinnati, Chicago and St. Louis (later Pennsylvania) Railroad and South Street. In 1889, two kilns were added and the existing buildings were enlarged. The company produced dinnerware, and semi-vitreous Canton China. Over 75% of its products were decorated, and its wares were shipped to every state in the U.S. At the turn of the century, the plant occupied a tract of land measuring 470' by 210' and had 200 employees. In 1924, the company announced plans for the construction of a new \$250,000 factory and office along the Ohio River, north of the city. At the time the new plant was constructed, Steubenville Pottery was one of the major dinnerware manufacturers in the country. It employed a number of designers, including the internationally recognized industrial designer Russel Wright. An expansion program in 1945 more than doubled the capacity of the plant. The company ceased operations late in 1959.

The Ohio Valley Clay Co. was established in 1882 by J.J. and J.W. Gill. The plant was located at the foot of Washington Street in two large buildings formerly used as a cotton factory. At the turn of the century, the company manufactured glass house pots, tanks and furnace bricks. Its products were shipped throughout the U.S. and Canada. The company went into receivership, ceased operation, and the property was sold at an auction in 1944. It was not used for clay production after that.

## **Paper Products**

Papermaking was one of the oldest industries in Steubenville. The business that later became the Liberty Paper Board Co. was established by Thomas Cole, the father of the painter of the same name, in 1819. The Clinton Paper Mill operated until 1889 when it was purchased by Richard and August Hartje. The Hartje company employed 125 people and produced newsprint, wrapping paper, straw and pulp board at the turn of the century. During its peak production, the capacity of the plant, located on North Third Street, was 80 tons of paper daily. In 1921, the company was placed in receivership until 1926 when it was purchased by L.J. Cullen. It was purchased again in 1927 and began operating under the name of Liberty Paper Board Co. The new owner invested \$400,000 immediately to convert from production of butcher's paper to paperboard. In 1928 the average plant capacity was 78 tons/day, which was increased to 170 tons/day by 1947. Federal Paperboard Company acquired the plant in 1956, and paper production had ceased by 1978.

The Chicago Wall Paper Co. was founded in 1899 in Illinois. This corporation purchased the assets of the Steubenville Wall Paper Co. in 1905. The Steubenville plant operated as a branch of the Illinois plant until 1909, when the Chicago plant was sold and the company moved its headquarters to Steubenville. It was incorporated under Ohio law in 1925, and the name was changed to Chicago Wall Paper Manufacturing Company. In 1947, the company employed 75 people and produced 6,000,000 rolls of wall paper annually. It ceased operation sometime between 1958 and 1960,

probably in 1959.

### **Utilities: Electricity**

Though production of electricity is defined as a utility, it has been included here because the property types employed are industrial in nature, design and appearance, and because electric power was important to many of Steubenville's industrial processes. The study of the electrification of industry is a large and complex subject. It can be said, however, that Steubenville industries followed the national trend in this area: as electric power generation and equipment became more available and reliable during the 20th century, industries converted from steam to electric power. The pace and the rate of change varied from industry to industry, but overall there was a great increase in demand for electric power as this conversion took place.

In 1886, John Flood and a representative of Western Electric Company discussed with William Beall (Beall & Steele Drugs) the possibility of establishing an electric company. Within 16 days there were enough investors to form a company. Two dynamos (23 arc and 20 arc light capacity) were purchased from Western Electric. Steam to run the dynamos was supplied, under contract, by Wagoner's sawmill at the foot of North Street. There were 42 subscribers immediately. In 1887 work was started on a new plant on North Street. Interest in a new electric railway resulted in a new generator to operate 6 cars being placed in service in 1887. Steubenville was reportedly one of the first cities in the country to have an electric street light system. In 1889, incandescent lamps were installed in the city and a new plant was completed at the corner of South and High Streets. By this time, the company was known as the Steubenville Gas & Electric Co. In 1901, the property was sold to the Steubenville Traction Co., which in turn sold the company in 1916 to the Buckeye Power Co. Buckeye Power was taken over by Ohio Power in 1921, which is still an important regional power supplier in Ohio. Transmission lines, substations and the distribution system were rebuilt, and in 1945 Steubenville was tied in with the Tidd Power Plant downriver at Brilliant, Ohio.

As has been illustrated by the tremendous growth of the Tidd plant at Brilliant, to give only one example, the development of electric power during the late 19th century and through the 20th century has provided an important local market for Ohio coal. Prior to electric power development, coal from the Steubenville area was used principally for coking purposes, for domestic consumption, and as locomotive, steamboat and stationary engine fuel. Electric power generation, whether used for powering streetcars, lighting streets or electrifying homes and businesses, was a new market for coal that grew rapidly and provided a ready market as other traditional uses of coal gradually declined. Electric power generation helped ensure survival of the coal industry in and around Steubenville and Jefferson County during the 20th century.

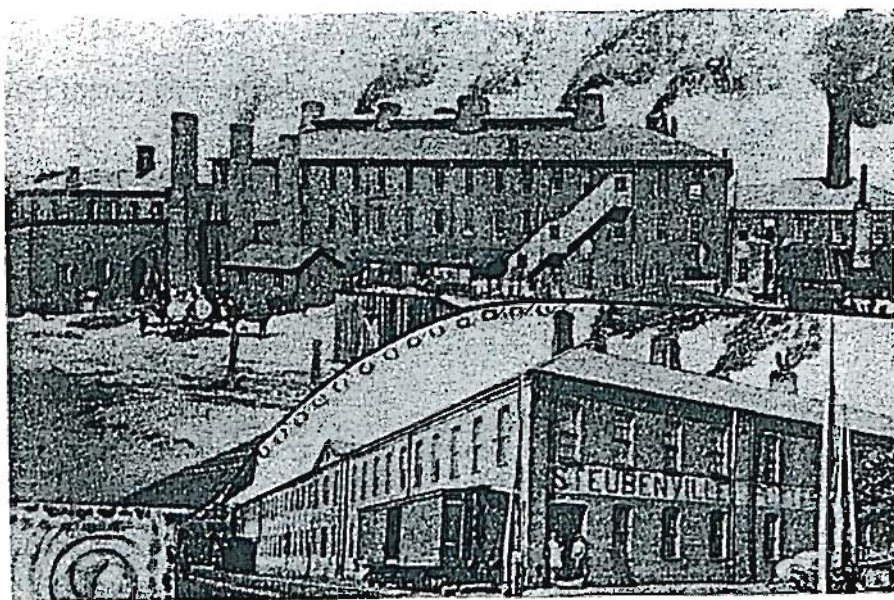
### **Other Industries**

The manufacture of ice was another important Steubenville industry. The Steubenville Ice Co. was incorporated in 1909 and a plant with a capacity of 60 tons/day was built at Third and Clinton Streets. That same year, the

retail ice business of Central Sewer & Pipe Supply Co. was purchased. The plant was remodeled and expanded in 1916. Another plant, in Toronto, Ohio, north of Steubenville, was constructed in 1920. In 1925 an existing business in Weirton was acquired. The Steubenville facilities were remodeled and modernized in 1940 and the Toronto plant was merged into the Steubenville plant in the mid-1940s. The company had ceased operations by 1954.

The Steubenville Ice Manufacturing and Cold Storage Co. was started in 1891 and was located in a brick building (formerly used as a nail factory, railway paint shop and foundry) west of the Panhandle Railroad Depot. Another building was also erected at the time. The plant was purchased in 1896 by George and James Koehline who made technical improvements to the plant. By 1897, the capacity of the plant was 45 tons daily, and it was the largest ice manufacturer in eastern Ohio. The plant serviced railroad trains as well as producing ice for use in the city. With the advance of mechanical refrigeration, the ice industry in general went into decline; this plant was closed by 1950.

Floto Brothers Stone Company, which was established in 1874, was responsible for the construction of many fine stone buildings in Steubenville. The company operated two large quarries within the city limits -- Stony Hollow (buff sandstone) and Wells Run (blue sandstone). Among the buildings constructed by the firm in the late 19th century were Hamline Methodist Episcopal Church, Water Works Pumping Station, St. Paul's Parish House, buildings and bridges at Union Cemetery and Beatty Park, and homes for Charles Steele, George Henry and J.M. Trainer. The firm had gone out of business by 1946.



One of Steubenville's best-remembered industries was Steubenville Pottery, which moved to north of the city in 1925. To Depression-era children its site was the "Pottery Lot" baseball field.

## **ASSOCIATED PROPERTY TYPES**

Despite their importance to the history and development of the city, almost nothing remains of most of the industries that grew, prospered and died out in Steubenville. This is perhaps to be expected, since these industries were businesses first and foremost, affected by local, national, and international economic forces largely beyond their control. Any business undergoes change and re-structuring over time; the changes that took place in Steubenville industries were so extensive and so sweeping that little physical evidence of these enterprises has survived.

Historic survey work in the city has revealed that only a few structures associated with the industries described above are standing. Most former industrial sites today are either vacant lots or have more recent unrelated structures standing on them. In other cases, older industrial facilities exist but have been so extensively altered that they no longer are representative of the industries that built and used them.

Archaeological investigations and further research into the appearance of demolished industrial structures, tasks which are beyond the scope of this project, should be undertaken if a more detailed description of these vanished resources is desired.

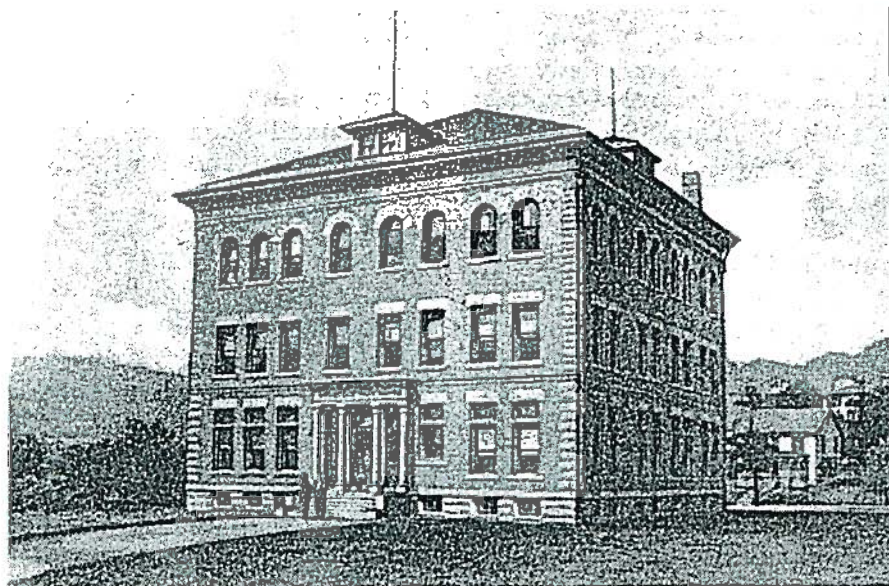
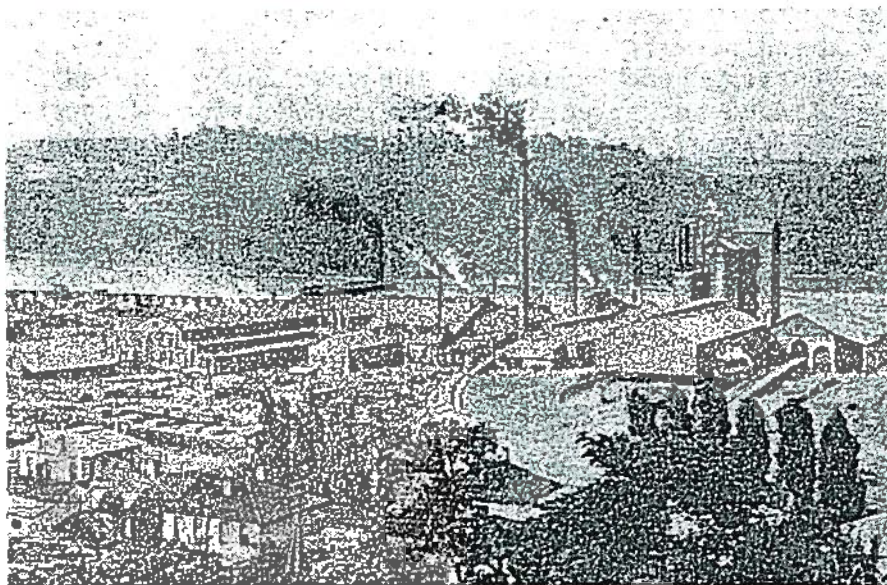
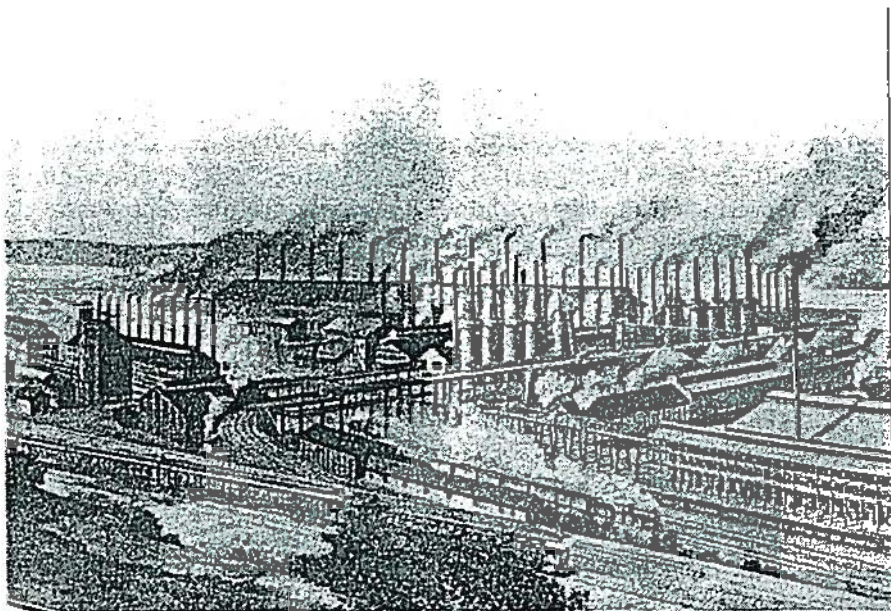
The steel plants in the south end of the city are an exception to the general lack of surviving industrial historic resources. Both Weirton Steel Co. and Wheeling-Pittsburgh Steel Co. have active facilities, but access to the properties is restricted, so detailed survey work has not been undertaken. If special arrangements can be made for access to these plants, it appears from historic maps and from a "windshield" survey from outside the properties that several older structures are still standing. Based on what is known of the steel plants in the absence of detailed investigation, included below are descriptions of the steel industry property types that might be expected to exist; an assessment of the significance of these properties; and a listing of the registration requirements for them.

Following the section on the steel industry are analyses of other Steubenville industrial property types, including an assessment of what, if any, properties still exist and the registration criteria for them.

### **The Steel Industry**

#### **Description**

The structures and buildings at Steubenville's steel plants usually were arranged so that product flow was in one direction, starting with raw materials and ending with finished products. This did not mean that the flow was always in a straight line; site conditions, land availability, and other factors usually meant that this was impossible. Usually the product flow involved right-angle turns or 180-degree changes in direction, but this was unimportant as long as the flow itself was maintained -- so that raw materials and intermediate and finished products were handled only once and were transported a minimum distance between stages in the process. This usually meant that buildings and structures were placed close to each other in an apparently random, jumbled pattern; this pattern became more



Its smoking stacks symbolizing prosperity, LaBelle Iron Works was an important early industry whose descendant is still a major employer. The early 20th century office building is still in use.

complex as the facility's size grew or new products or processes were added, but the paramount consideration was the product flow and the effort to maximize efficiency through judicious placement of structures and buildings.

The following types of properties could be expected at Steubenville iron and steel facilities:

### 1. Blast Furnaces

Basic to the iron- and steel-making process, these were large tapered cylindrical iron or steel structures lined with firebrick. They were open at the top and could be tapped through openings at the bottom; raw materials (iron ore, coke and limestone) were charged at the top, and waste slag and molten iron were drained off from the bottom. Large air pipes carried hot air and blew it into each furnace creating the "blast" that initiated and sustained the burning of the coke that produced iron by reduction (the limestone served as a flux that combined with the impurities in the ore to form slag). Blast furnaces usually were not contained within buildings but instead stood outdoors without any roofs. Blast furnaces were usually built at least in pairs so that if one furnace needed repairs at least one other could take over for it.

### 2. Materials Storage Bins/Stock House and Charging Equipment

Located immediately adjacent to the blast furnaces were the structures, sometimes covered and sometimes open-air, in which iron ore and other materials were stored until needed. Between them and the charging hole at the top of each furnace was a sloping track on which a cable-hauled skipcar ran. The charging materials were loaded into this car, which then was hauled to the furnace's top and automatically dumped. When "in blast," a furnace ran continuously, 24 hours a day, for as much as several months at a time. To permit it to burn out or run out of any of its raw materials was a very expensive matter usually requiring re-lining with new firebrick. This meant that a constant ready supply of raw materials was critical, so the storage facilities and charging equipment were essential to furnace operation. Bins usually were of brick or poured concrete, stock houses usually were of steel frame construction, often without any sides, and the charging equipment was made of structural steel.

### 3. Engine and Boiler Houses

Also critical to furnace operation was a steam engine to power equipment generating the air blast which sustained the reaction in the furnaces by steadily supplying oxygen. The engine or others like it might also be used to pump water, generate electricity and perform other useful functions. Boilers to supply engine steam were usually close at hand. Sometimes boilers were in a separate boiler house, and sometimes boilers and engines were in the same structure. These were usually of brick and were between one and two stories in height depending on the size of the equipment. One or more brick, concrete or steel pipe stacks served the boilers to carry away smoke. Buildings housing such equipment typically had only a few small windows and might have large doors to facilitate moving equipment in and out.

#### 4. Coking Oven Batteries

These were large structures, boxy in shape and built of structural steel, brick and concrete. They were divided into compartments into which coal was loaded and then heated in the absence of air. The heat drove off volatile components of the coal, leaving behind nearly pure carbon, known as coke. Coke was used as blast furnace fuel because it minimized the amount of impurities introduced into the iron as it was reduced from iron ore. Coking batteries generally were large, perhaps 30 feet wide, 20 feet high and 100 or more feet long. Tall, narrow doors along the sides provided access to each end of the heating chambers, or ovens, which typically were a maximum of 18 inches wide. After the coal had been heated long enough, about 18 hours, a pushing mechanism forced each oven's load out into a waiting hopper, which then was quickly run into a nearby quenching shed, where large amounts of water kept the hot coke from igniting in the open air.

#### 5. By-Product Plants/Gas Recovery Facilities

The coking process produced gases, tars and other substances from the volatile components of coal. Many of these were useful, so iron and steel plants employed equipment to recover the various products for sale or for use in the plant. Flammable gases, for example, were piped directly to the stoves that pre-heated the air used to create the blast in the blast furnaces. The by-products plants and gas recovery facilities usually were of brick construction, simple in design and with a minimum of ornamentation. Typically they had few windows and doors and were filled with and surrounded by piping, tanks, pumps and other equipment.

#### 6. Casting Houses

Nestled against the blast furnaces, usually on the side away from the stock piles and charging equipment, were the structures into which the molten iron flowed when tapped from the furnaces every few hours. Usually only a single structure served two or more blast furnaces, but sometimes separate structures served each individual furnace. The molten iron was poured into molds to form ingots, bars ("pigs") or other shapes, or into special railroad cars that transported the liquid iron to other processing locations; usually iron directly from the blast furnace was not immediately cast into finished products, since the large quantities of molten iron that had to be handled made any kind of quality control impossible. Casting houses were simple in design -- usually rectangular in shape, with gable roofs. The earlier ones were built of brick and the later of structural steel.

#### 7. Mills: Bessemer/Open Hearth, Rolling, Plate, Sheet, Rod, Wire, Tube

These were the facilities that converted iron into steel and then formed it into useful shapes and products. Both the Bessemer and the later open hearth processes involved the heating of molten iron in the presence of oxygen to burn out impurities. The resulting purer form of iron was called steel. Once the steel had been produced, it was usually poured into molds to form large rectangular ingots, and these were later processed into numerous shapes such as structural steel, railroad rails, plates, sheets, wire, rods and tubes. These all involved a variety of equipment, but the

mill buildings in which the processes were housed usually did not vary much. Earlier mills usually were brick, but from the 1890s on they most often were built of structural steel with fireproof metal roofs. Roofs typically were gabled, and the sides of these mills often were left partially or fully open. When they were sided, corrugated sheet metal was the most common siding material. Railroad tracks often ran into these buildings to facilitate movement of products around the plant.

#### 8. Machine Shops and Blacksmith Shops

Usually of brick construction and small in size compared to other facilities, these were essential components of an iron or steel plant. Machine shops handled tool- and machine-making, while blacksmith shops undertook repair of equipment already in use. Typically these were one story in height, of brick construction, and they usually had numerous large windows to facilitate the sometimes precise work going on inside.

#### Significance

The Weirton Steel and Wheeling-Pittsburgh Steel plants are significant under Criterion A in the field of INDUSTRY. Under Criterion A, steel mill buildings are significant because they are a surviving physical link with the city's most important industrial enterprise; and because they reflect the process of iron and steel manufacturing.

Perhaps no enterprise is more symbolic of Steubenville than steel production: it was among the earliest of local industries; it has provided by far the greatest amount of employment for Steubenville citizens; and it is the only industry in the city with an unbroken history extending from today back to before the middle of the 19th century.

The steel industry affected all aspects of life in Steubenville, from its physical environment (it has been said that the air once was so dirty that a businessman needed three white shirts a day) to its ethnic makeup (the mills were a major attraction for both skilled and unskilled workers, especially those immigrating from southern and eastern Europe). At one point the mills employed workers equal to nearly 20% of Steubenville's population.

Significant changes in the nation's steel industry (new processes, consolidation of plants, and downsizing of staff levels), particularly since the 1960s, have affected the Steubenville mills of the Weirton and Wheeling-Pittsburgh firms. In both cases, actual conversion of iron to steel and further processing of that steel occur elsewhere (Weirton's principal plant is across the river in Weirton, West Virginia; Wheeling-Pittsburgh's main plant is in Mingo Junction, Ohio, a few miles south of Steubenville). No longer is the entire integrated process from raw materials to finished steel products housed solely at the Steubenville mills. The most important activity today at Steubenville is at Wheeling-Pittsburgh's two blast furnaces, where raw materials arrive by rail and cars of molten iron are sent out by rail for conversion to steel at Mingo Junction. However, most of the buildings and structures representative of the entire integrated process do survive at Steubenville, even though most of their manufacturing equipment has been removed; the spatial relationships between buildings and structures remain intact enough

to communicate the iron- and steel-making processes which occurred there. A thorough inventory and study of the mills at Steubenville has not yet been undertaken; but, based on current knowledge, the mills appear still to be representative of the iron- and steel-making process. In order to make a more informed evaluation, it will be necessary to undertake an inventory of the equipment and processes still in place, and of the buildings and structures in which they are housed.

Consideration was given during research of this context to whether the Steubenville mills might have significance for association with the steel industry strike of 1919. Available documentation suggests that the Steubenville mills did not play a role in this event of any greater significance than the roles played by other mills; and that the strike itself did not have a lasting impact in the Steubenville area.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

The property should be on its original site or, if it has been moved, its site should be the place where the property made its contribution to the history of Steubenville and the local iron and steel industry.

##### 2. DESIGN

The property should retain important design elements of steel production facilities, including original form and massing, roof shape, structural systems, and pattern of window and door openings. Alterations over time to accommodate changes in industrial process or to accommodate new systems or products will not disqualify a property from listing as long as the design of the property still clearly communicates its industrial role.

##### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey its historical relationship to the steel industry in Steubenville.

##### 4. SETTING

The physical environment of the property should remain intact and should be of substantially the same character as in the past when the property made its contribution to the history of Steubenville. For the Steubenville steel mills, for example, the spatial links to the Ohio River and to rail and road transportation should still be evident, and there should not be significant non-steel-related new construction or introduction of environmental elements out of character with a steel mill.

## The Glass Industry

### Description

While efficient product flow was as important to other industries as to iron and steel, usually other industries did not require the same large land areas and big buildings as iron and steel. More often than not, as in the case of Steubenville's glass industry, most or all of the processes could be contained under a single roof.

The glass industry, like iron and steel, had a certain product flow. Raw materials (principally sand and various chemical coloring agents) were heated in furnaces to make glass which was then shaped, molded and processed. The products then were warehoused, packed and shipped as demand dictated. This industrial process mandated the presence of certain facilities: storing and mixing rooms for sand and other materials; furnace rooms; molding and working areas; storage rooms for molds and equipment; finishing rooms; and warehouse and packing space. Engine and boiler houses and machine and blacksmith shops were also commonly required.

Rather than a series of separate buildings, a glass plant usually had one or two principal buildings onto which many additions were built over time as business grew or processes changed. The typical plant began with a single building, usually of brick and one or two stories in height, with a gable or flat roof. Often a higher portion of the building housed the upper portions and the stacks of the furnaces. Other space was added as needed. Separate wings housing the furnaces were common, as were rooms for materials storage, finishing, packing, warehousing and shipping.

Separate office buildings, usually of brick and one or two stories in height, were sometimes used. These tended to have more architectural detail than the plants themselves.

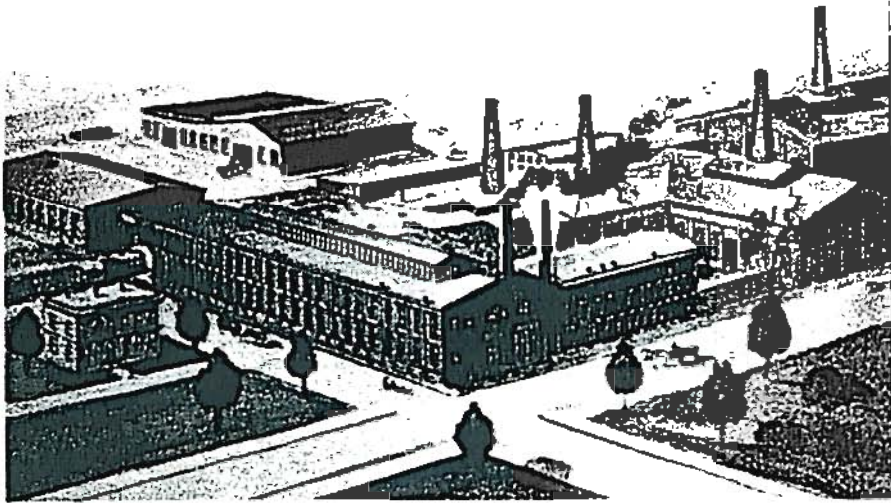
### Significance

At one time, Steubenville was as well known for its glass works as for its iron and steel works, but the glass-making industry faded rapidly in importance after the turn of the 20th century. Like the steel industry, glass-making meant major employment for skilled, semi-skilled, and unskilled workers; unlike the steel industry, the glass industry employed large numbers of women at tasks such as packing finished items.

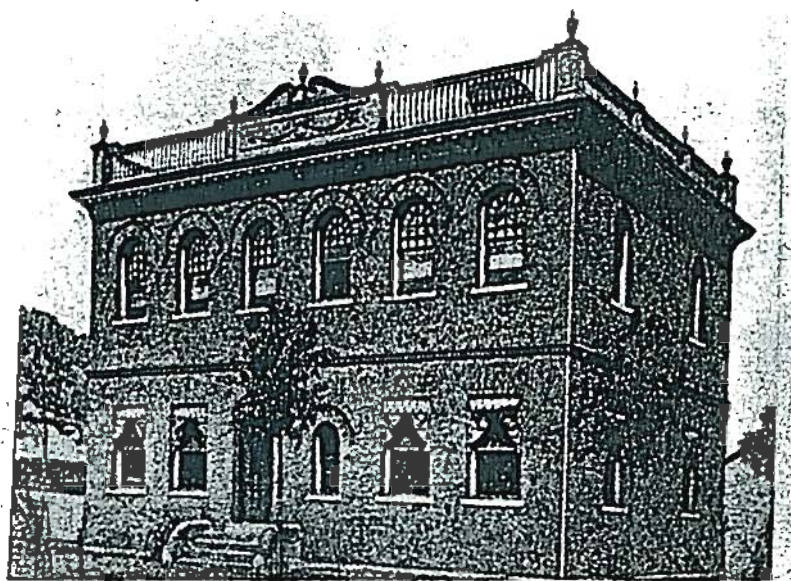
Like other industries in Steubenville, the glass-making industry was significant for its economic contribution to the community; for its tendency to increase the city's population by attracting workers; for its effect in increasing the city's ethnic diversity through its attractiveness to newly-arrived immigrants as a place of employment; and for its impact on the physical character of the city.

### Registration Requirements

Two buildings from the Acme Glass plant are the only surviving resources related to the glass-making industry in Steubenville. One, located just west of the ConRail tracks on the north side of Franklin Avenue, is the former office building; the other, opposite the office building and south



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The office building of Acme Glass and a part of the  
 production plant survive today as reminders of a  
 large Steubenville firm.

of Franklin Avenue, is part of the former production facilities. Both buildings, which date from the late 19th century, are currently vacant.

No equipment associated with glass-making appears to have survived, here or elsewhere in the city, and as a result the actual glass-making process cannot be discerned in the surviving buildings. However, the Acme Glass Works buildings are rare Steubenville industrial buildings; they qualify for listing under Criterion A because they have the following aspects of integrity:

#### 1. LOCATION

The buildings are in their original locations on either side of Franklin Avenue just west of the Panhandle (ConRail) railroad tracks.

#### 2. DESIGN

The buildings retain nearly all the elements of their original design: both are constructed of brick and are two stories in height; the former office building has round-arched windows (some with original sash) with hoodmolds, a terra cotta beltcourse and a simple brick cornice; the production building has corbelled brickwork at the cornice line and brick piers placed between the bays.

#### 3. ASSOCIATION

The buildings are sufficiently intact to communicate their importance in the industrial history of Steubenville, retaining the original form and details of the period when they were built and used in the glass industry. Though glass-making equipment is gone, the buildings survive in a form that communicates at least some information about the glass industry.

### **Extractive Industries: Coal Mining**

#### **Description**

Mining properties used headframes to house the hoisting equipment needed to raise and lower men, equipment and cars of coal; washing or sorting sheds where the coal was cleaned and sized for market; tipples where the coal could be held for loading into wagons or railroad cars; and engine and boiler houses for steam and electric power generation.

Headframes, washing and sorting sheds, and tipples were very plain and functional, usually employing heavy timber construction, gable roofs and siding of wood planks or corrugated sheet metal. They were usually rectangular in shape and arranged so that product flow was continuous in one direction; for this reason they often formed elongated structures with components linked end-to-end. These facilities also were often elevated above the ground on structure or built into a hillside so that gravity could be employed in loading processed coal and in removing waste materials.

Boiler and engine houses commonly were of brick, one to two stories in height, and usually with gable roofs. Sometimes they were of timber

construction like other mine properties.

#### Significance

Historically and today, Jefferson County is one of the important coal-producing counties of Ohio. Though coal mining in Steubenville itself no longer occurs, it remains an important local industry, and a significant portion of Steubenville's wealth was built on and today is sustained by coal mining.

Coal mining was one of the earliest industries in Steubenville, and it developed into an important component of the economic base of the city. Like other industries, it was a source of employment for area residents and contributed to Steubenville's population and business growth through the late 19th century and most of the 20th century.

#### Registration Requirements

No known properties of this type are extant.

### **Extractive Industries: Pottery/Clay Industry**

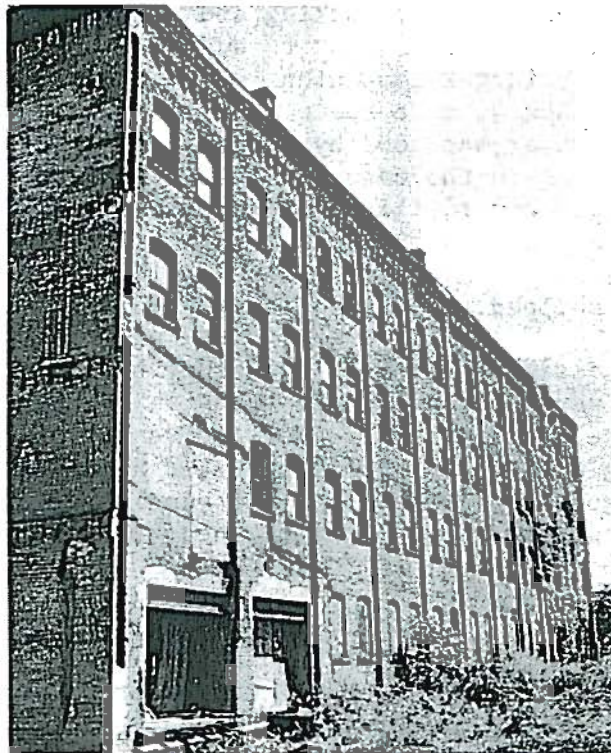
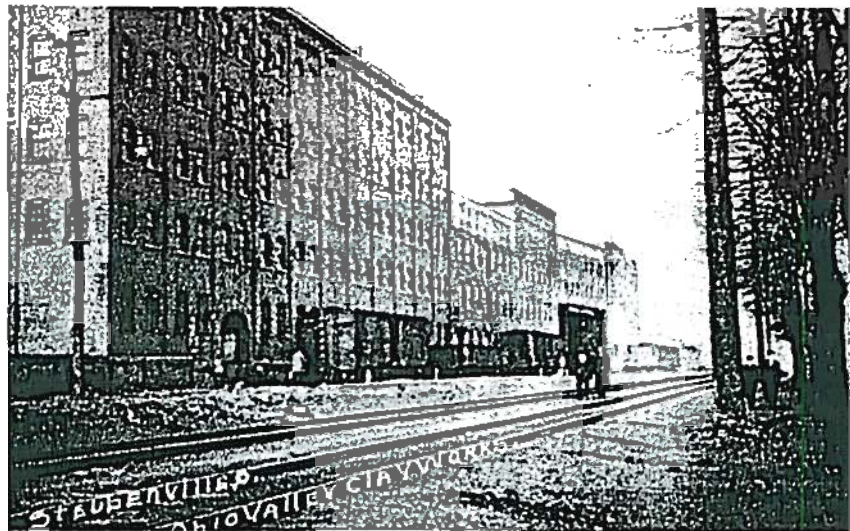
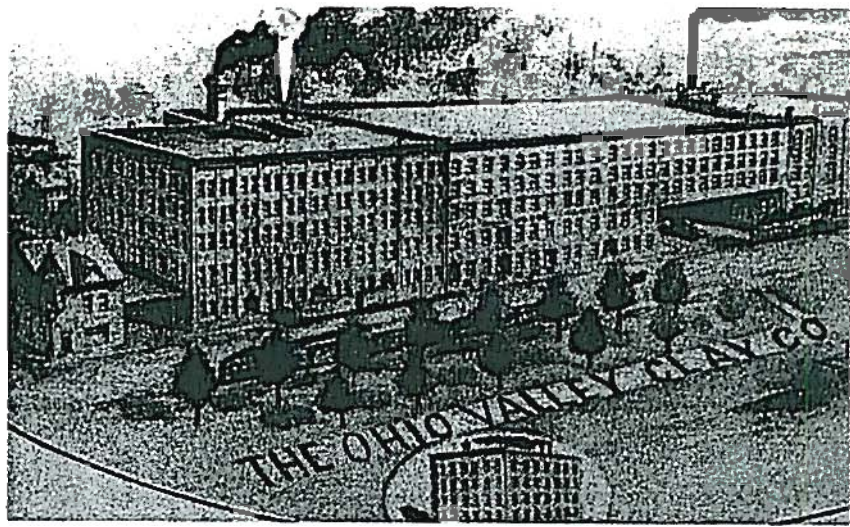
#### Description

The process of converting clay into finished ceramic items involved clay washing and mixing; casting liquid clay ("slip") or molding/shaping plastic clay into the desired objects; allowing the products time to dry out; glazing with surface colors and treatments; firing at high temperatures in kilns or furnaces; and storage, packing and shipping. In a small ceramics shop all these processes might be adjacent to each other in a single building. In the larger companies, which often began with a single building, the process took up more space as business grew over time; but usually it all stayed under one roof by means of additions to the original structure. Sometimes, as in the case of Steubenville Pottery, a firm moved entirely into completely new facilities, and these too were typically under one roof. Sheds and structures built near the main building sometimes were used to make barrels, and to store them and packing straw to cushion the finished items being shipped.

Properties varied in design. One- and two-story brick structures, with gable or flat roofs, were very common in both the 19th and the early 20th centuries. There were usually many large windows to provide adequate work light. Large sliding or swinging doors provided access to and from railroad cars and wagons for inbound and outbound shipping. There usually was an office area and sometimes a showroom near the sidewalk, easily accessible to the public.

#### Significance

Although Steubenville was to the south of the major historic ceramics-producing region of eastern Ohio (this was centered in East Liverpool, some 25 miles to the north along the river), the city's ceramics enterprises were locally important as employers of numerous workers; and they even gave the city a national presence in this industry in the World War II and



A small part of Ohio Valley Clay Works, in an advanced state of decay, survives on the east side of Route 7 near the downtown.

post-war periods (this arose from Steubenville Pottery's production of some 80 million pieces of American Modern dinnerware designed by Russel Wright).

Requiring a large work force of skilled, semi-skilled, and unskilled people, the city's ceramics producers were a major place of employment. They were another of the relatively few industries that employed large numbers of women.

#### Registration Requirements

The Ohio Valley Clay Co. building is the only extant property associated with this industry. Though large portions of the Ohio Valley Clay complex have been demolished, it appears that the surviving structure may have been a free-standing building at one time. It is a five-story brick building with a flat roof (largely gone) concealed behind a parapet. The parapet has a decorative cornice composed of projecting brick courses. Windows are 6/6 double-hung sash, set in pairs between brick pilasters. Segmental-arched door openings are along the east side on the first floor. The property is currently listed in the National Register.

#### Utilities: Electricity

##### Description

In electricity generation, steam was the initial product, which then was used to run turbines and generate electricity. Power stations usually had two major components: the boiler house for generation of steam, and an engine or turbine generation house to convert the steam to useful power. Some facilities that began by generating power through steam alone were later converted to electric power stations. Facilities were generally of brick. The boiler house typically did not have many windows, if it had any at all. One or more metal, brick or concrete smokestacks were found either in the roof or adjacent to the boiler house. If the fuel was coal, these structures might also have coal bunkers or bins built adjacent to them. Boiler houses sometimes were attached directly to the engine or generator houses and sometimes each was free-standing. Usually the two functions were not housed in the same building, apparently for fear of boiler explosions. The engine or turbine/generator house usually was symmetrical in plan and had many large windows to provide ample natural light. Buildings often were quite tall, since they usually required a traveling overhead crane on the interior for lifting heavy parts during repair and maintenance. Roofs generally were gabled or hipped, and slate was the most common roofing material.

##### Significance

Generation of electrical power became an industry in Steubenville in the late 19th and early 20th centuries, as it did in other cities. Though not as significant as other industries in terms of employment or invested capital, the electric utility industry was significant for its effects: it both enabled the electrification of other industries, freeing them from the limitations of mechanical power sources, and it permitted the development of streetcar and interurban systems that had major impacts upon the physical look and the development patterns of the city. Further impacts upon the quality of urban life and upon Steubenville's social and

business activities -- largely for the better -- could also be credited to the electric utility industry.

### Registration Requirements

The Steubenville Traction & Light Co. boiler house at 283 S. Lake Erie Avenue is the only surviving building associated with this industry. It is a two-story structure set against a hillside, so the west elevation appears to be only one story high. It is a brick building, rectangular in shape, with a gable roof and a parapet wall at its north end. Window and door openings have been extensively altered and filled in, and a new brick facade has been applied to the south end. Because of these alterations, the property does not appear to have sufficient integrity to qualify for listing.

Ideally, a property associated with the generation of electricity should have both its power source (which drove the electric generators) and the generators themselves intact. Lacking these, a property may still be considered qualified if the buildings that housed the power and generating equipment survive with a high level of integrity. If the boilers or other power source were housed separately from the generators (as was sometimes the case), then a property may still be considered qualified even if only one of the buildings has survived, since a high level of integrity will still permit the resource to communicate at least some information about the process of generating electricity.

Aspects of integrity required for registration include location, design, and association. Because of the extensive physical changes made in the past to the Steubenville Traction & Light Co. boiler house, it has lost its integrity of design and is not considered qualified for listing.

### Other Industries

Ice manufacturing plants required an engine and boiler house, a compressor plant where ammonia was made into liquid form, the main plant where water was frozen in metal pans around which the ammonia flowed, and a storage building for the ice inventory. Sometimes all this was done under one roof, and sometimes separate structures housed the power equipment, freezing equipment and storage. Buildings were generally of brick and were two or three stories in height, with slate-covered gable roofs.

The Steubenville Ice Co. building survives today as the Fort Pitt Distributing Co. building. It is a brick building, one story in height and in an L shape. The brick walls, which have few windows, are built in a pattern of pilasters separating each bay from the next. The roof is gable, with a low pitch. The south end bay on the west has a modern garage door. No other ice manufacturing plants appear to have survived. Because of extensive alterations to the building, it does not appear to have a level of integrity sufficient to qualify it for listing.

Ideally, a property associated with the manufacture of ice should have both its power source and ice-making equipment intact. Lacking these, a property may still be considered qualified if the building or buildings that housed the power and ice-making equipment survive with a high level of

integrity. In order to be considered qualified for listing, a property should have integrity of location, design, and association. The Steubenville Ice Company building, because of extensive alterations to windows and doors and introduction of new openings, has lost its integrity of design and is not considered qualified.

Foundries, stoneworks, paper companies, and other enterprises were housed in a variety of structures, most of which shared some common features: brick construction; rows of vertically-proportioned windows, large shipping and receiving doors on hinges or rollers, hip or gable or flat roofs, and generally restrained architectural design.

The Hartje Paper Co. building, which later became Federal Paperboard, survives as a portion of a large, irregular building which entirely surrounds the older building except on the east side. The Hartje building is brick, one story in height, with a row of closely-spaced freight doors along the east side. In design and appearance it is very similar to a railroad freight house, but it was not built or used as one. The freight doors are arched but have lost their wooden doors; some have been half-infilled with brick to create windows. The roof is collapsing and the supporting wood trusses have been exposed. The gable ends have parapet walls. The Hartje building has undergone such extensive alteration and deterioration that it does not have a sufficient integrity of design to qualify for listing.

The City Brewing Company at 934 Adams Street, a brick structure no longer used for brewing purposes, survives but has been extensively altered and does not appear to have a sufficient integrity of design to qualify for listing.

There are no other known properties extant.

## Homes of Industrialists

### Description

Steubenville's industrialists were among the city's most prominent citizens. They typically built their homes in the north end of downtown Steubenville (late 19th and early 20th centuries) or on Belleview Boulevard and in Brady Estates (early-to-mid-20th century) on the hilltop. As they tended to be wealthy, their homes were built in the latest architectural styles -- including Italianate, Queen Anne, Colonial Revival and Tudor Revival -- of the late 19th and early 20th centuries. These buildings were also noteworthy for their fine materials and craftsmanship such as stained and leaded glass windows, carved exterior trim, decorative porches and grand entrances.

A number of these homes exist in both the downtown and the hilltop neighborhoods. Properties such as the Means Residence at 217 North Street, a fine mid-19th century Italianate home built by the owner of the Means Foundry; Edgehill, 609 North Seventh Street, the home of James Gill, president of Ohio Valley Clay Products and Acme Glass Company; the A.

Joseph Hennings Residence, 611 North Fourth Street, home of the owner of the Chicago Wall Paper Company; and the early 20th century Belleview Boulevard home of J. C. Williams, president of Weirton Steel, all illustrate that these homes represent some of the best examples of late 19th and early 20th century architecture in Steubenville.

#### Significance and Registration Requirements

The homes of Steubenville's industrialists are significant under Criteria A, B, and C in the field of INDUSTRY. These buildings, though built for practical reasons as residences, were designed to represent the power and wealth of the people who built and ran the industrial underpinnings of Steubenville.

These buildings are fully treated as a property type in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910." Refer to this context for an assessment of single-family residences associated with the industrial history of the city.

The following potential and/or listed historic districts all include a number of homes associated with industrialists:

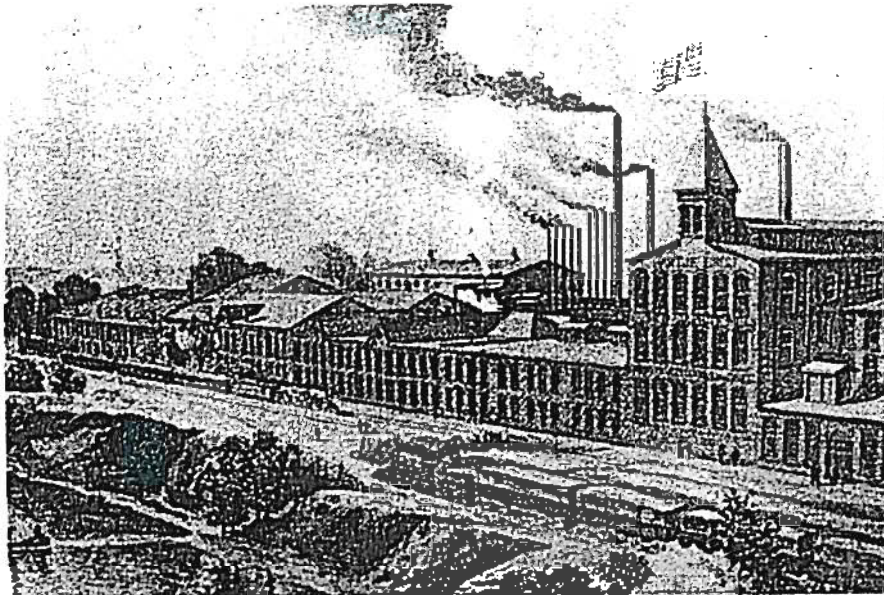
North Fourth Street Historic District - The district, which was listed in the National Register of Historic Places in 1991, includes a number of fine late 19th and early 20th century residential structures. Included among these homes are the following: J.W. Gill Residence (president of Acme Glass Company, 701 North Fourth Street); H.B. Grier Residence (secretary/treasurer of Acme Glass Company, 820 North Fourth Street); Samuel Gill Residence (one of the owners of Gill Brothers Company, 711 North Fourth Street); and A.B. Sharpe Residence (president of the Ohio Foundry and Manufacturing Company, 426 Franklin Avenue).

Belleview Boulevard Historic District - Located on the edge of a hill overlooking the Ohio Valley, this potential district is one of the finest early 20th century neighborhoods in Steubenville. Not surprisingly, a number of the city's leading citizens chose to live here. Among the industrialists' homes included in this potential historic district are the Workman Residence (291 Belleview Blvd.); C.P. McFadden Residence (349 Belleview Blvd.); M.C. McConnell Residence (405 Belleview Blvd.); J. Potter Residence (515 Belleview Blvd.); J.N. Leech Residence (715 Belleview Blvd.); J.C. Williams Residence (718 Belleview Blvd.); W. Forstram Residence (1401 Belleview Blvd.); J.A. Denning Residence (1711 Oregon Avenue); C.A. Hobstetter Residence (300 Reserve); and the H. Hildenbiddle Residence (330 Reserve).

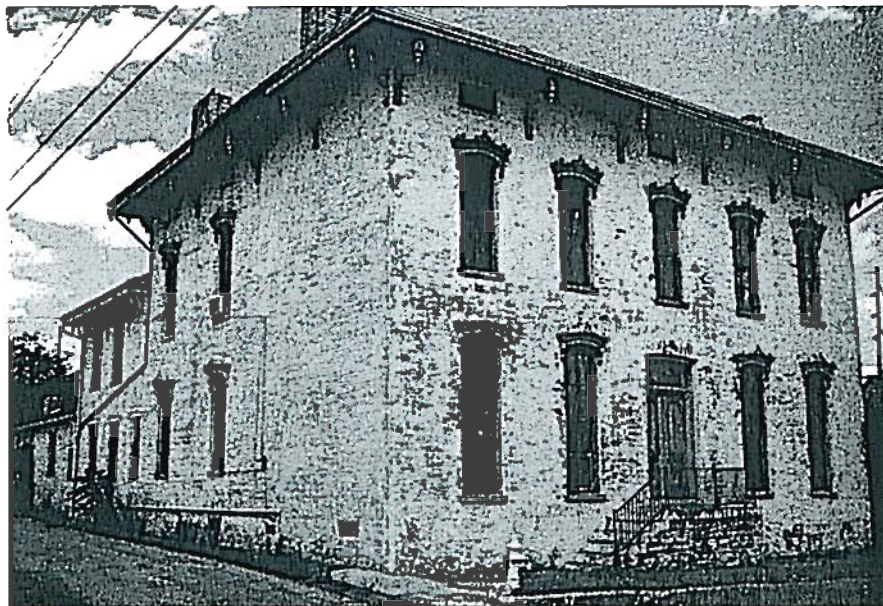
Brady Estates Historic District - This potential district dates from the 1930s and 1940s. It is unusual in that it was platted with landscaped islands, curvilinear streets, and cul-de-sacs, which is not typical of Steubenville neighborhoods. Many of Steubenville's important businessmen, especially those associated with the steel industry, lived here. Homes associated with the industrial context are the Joseph Echols Residence (606 Granard); Albert Baker Residence (702 Granard); A.S. Earp Residence (703 Granard); Erle Leathers Residence (801 Granard); Charles Cowher Residence

(1721 Hamilton Place), Daniel Schiappa Residence (1804 Hamilton Place); J.W. Unroe Residence (1807 Hamilton Place); and the Guy Wehr Residence (1811 Hamilton Place).

Ross Park Historic District - This potential district is very small and consists of only a single block. Among the homes included in this district are two that are related to the industrial context. They are the Frank King Residence (646 Ross Park) and the W.R. Siedenbergh Residence (658 Ross Park).



The Hartje paper plant, along the river north of the downtown, was a major industry in Steubenville.



The James Means residence on North Street is one of Steubenville's finest surviving homes associated with an industrialist.

## **TRANSPORTATION AND DISTRIBUTION IN STEUBENVILLE, 1853 - 1959**

Steubenville's location along the Ohio River has always been advantageous. Lying on a broad gravel terrace formed by the river in post-glacial times, Steubenville had the advantages of easy access to the river, flat land and level approaches that facilitated railroad development, and room for industry to grow in close physical proximity to both the railroads and the river. When the railroads came in the 1850s, in fact they had little choice about where to locate: the lower levels of the Ohio Valley were the only possible locations and forced the railroads to run parallel to the river, but the deep side gorges cut by intersecting streams made it relatively easy for rail lines to enter and leave the valley on their way to destinations such as Pittsburgh, Cleveland and Columbus.

The river -- and a limited system of roads -- comprised the transportation network for the first half of the 19th century. However, the introduction of the first railroad in 1853 led to enormous industrial growth in the Steubenville area. Generally, large-scale heavy industry could not begin until a railroad line arrived and provided essential transportation for raw materials and finished products; rivers could meet only a portion of this need because of their fixed courses and exigencies such as winter freeze-ups and periods of low water.

The railroads, on the other hand, made it possible to receive raw materials and ship finished products to all parts of the U.S. and Canada. This, combined with the advantages the river did offer and with the attractiveness of local land and mineral resources for industrial development, made Steubenville a major break-bulk point: it was the destination for large amounts of raw materials and the origination point for a wide variety of finished products.

More than any other, the iron and steel industry relied upon and provided traffic for the railroads. Incoming shipments of coal, iron ore and limestone were balanced by outbound loads of iron and steel products ranging from cut nails and tin cans to coil steel.

The passenger transportation system was excellent and included railroads, interurbans, and both local and long distance buses. The passenger railroads were extremely important to Steubenville up through the World War II era, but after 1950 there was the beginning of a shift from rail to highway for transporting people, a trend that was firmly established by 1959.

### **The Ohio River**

The river played, and continues to play, a role in transportation in Steubenville. Its importance grew in the era before the railroads, and it gradually waned after the mid-19th century due to railroad competition. The Ohio River today is used in the Steubenville area principally by the iron and steel industry for inbound coal shipments.

In the city's earliest years, the river was the primary means of transportation for passengers and goods. As early as 1794 a line of

keelboat packets was in operation between Pittsburgh and Cincinnati. The round trip was nearly 1,000 miles and required four weeks. In 1811, steam power was introduced on the Ohio in the form of the pioneer boat New Orleans, which passed Steubenville on its voyage from Pittsburgh to its namesake city. While boat-building was an early Steubenville industry, no information has yet come to light regarding where this took place, and no buildings or structures associated with boat-building have been found.

Regular steamer service began in the 1820s, and locally-owned packet boats began running by 1831. By the 1850s, Steubenville interests served the river trade between Pittsburgh and Wheeling with numerous well-known boats such as Forest City, Steubenville, Convoy, Diurnal, Brilliant, and Buckeye State. Both locally-based steamer lines and lines based elsewhere provided service to and from Steubenville. Steamers stopped at the Steubenville wharf boat, which was moored in the river at the foot of Market Street. It was in fact a barge with enclosed space for passengers and freight. Because of the steepness of the riverbank on the Ohio side, Steubenville never had a broad, flat paved wharf area like many other cities and had to rely on the wharf boat as its only dock. Freight shipments from Steubenville came from local industries such as Ohio Foundry, Acme Glass and Steubenville Pottery, and livestock shipments were common as well. The wharf boat appears to have been the only steamboat facility on the river at Steubenville; there appear not to have been any warehouses along the river. Heavier industries such as LaBelle Iron Works established their own docks and wharfs, generally downriver from the main part of the city, to handle inbound bulk shipments of coal and ore, and outbound shipments of iron and steel products.

Natural conditions caused significant variation in the river's water level, which often hampered traffic. In 1907, the water level was so low that one could wade to the West Virginia side at the LaBelle Iron Works. To remedy this problem, the federal government built, between about 1910 and 1929, a series of 54 locks and dams along the Ohio. These created a series of slackwater pools and ensured regular water levels of at least nine feet all along the river, making it navigable year-round. The first dam on the Ohio was actually built at Davis Island, below Pittsburgh, for a cost of \$1 million in 1885, but the big push to canalize the entire river did not begin until about 1910. A 1910 Steubenville industrial history mentioned the government's plan for the lock and dam system, and in that year Lock and Dam #10 was built at Steubenville, on the Ohio shore not far above the Panhandle railroad bridge.

As early as the 1930s the government began a program to replace the original locks and dams, which were of relatively low lift, with a series of fewer but higher-lift locks and dams. This project continued into the 1970s, with the result that the original 54 installations have been replaced with about one-third that number along the 982-mile length of the Ohio. Lock and Dam #10 was eliminated during this project, including the main control buildings and two residences that sat at the lock's west side, and today only the lock's west wall, built of concrete, is visible.

The immediate effect of the Ohio canalization project was a boost in river-borne freight traffic, even though passenger traffic continued the decline that had begun years earlier in the face of railroad, interurban and automobile competition (passenger packet service to Steubenville ended in the 1920s). The steady water levels made barge transportation reliable and inexpensive, particularly for bulk commodities, and the river has remained an important mode for transporting these products.

## **Railroads**

Steubenville was served by three rail routes from the late 19th century to the mid-20th century. The earliest and most important rail line was the east-west line today owned by ConRail. Crossing the Ohio River at the north end of the city, this line runs west of the main part of downtown Steubenville, between 6th and 7th streets, then south along the river to Mingo Junction and west to Columbus. Until the 1980s this line was part of a through route between Pittsburgh and St. Louis. Today it does not carry freight west of Mingo Junction but remains important to the Steubenville-area steel industry.

The first predecessor to this route was chartered in 1848 as the Steubenville & Indiana Railroad to run between Steubenville and Newark, Ohio, where it was to connect with the Central Ohio Railroad to form a through route to Columbus. A section of the S&I between Steubenville and Unionport was completed in 1853, and on October 8th three locomotives with two cars rolled into town. The line was completed to Newark in 1855, but it was not until 1857 that through service to Columbus was begun.

East of the Ohio River, the Pittsburgh & Steubenville Railroad was chartered in 1849 to run between Pittsburgh and the Pennsylvania/(West) Virginia border, in the general direction of Steubenville. Construction began in 1852 and proceeded slowly during the 1850s.

In the meantime, having failed to receive a charter from the Virginia legislature, two area investors proceeded anyway to buy right-of-way from the Ohio River's east shore opposite Steubenville in the direction of Pittsburgh, stopping at the Pennsylvania border. Construction began in 1853, and in 1854 six miles of the railroad were completed across what is now the West Virginia panhandle. The first trip on the line was made July 4, 1854. However, since the line was isolated at both ends, operation was suspended after about six months and the track lay idle.

In 1860 the Virginia legislature chartered a new company, the Holliday's Cove Railroad Co., to operate a line of railroad from the Virginia/Pennsylvania border to Steubenville. This company used the earlier but idle line across the panhandle and, between 1861 and 1865, built a bridge across the Ohio at Steubenville. Late in 1865, having crossed the river and linked up with the now-completed Pittsburgh & Steubenville, the Holliday's Cove became part of a through route between Pittsburgh and Columbus, with connections east and west from those cities.

According to one history from about the 1880s, the Pittsburgh & Steubenville then "experienced difficulty in retaining the approving smile of Dame Fortune" (it went bankrupt) and was re-organized late in 1867 as the Pan-Handle Railway Co. In May of 1868 the three railroads (PH, HC and S&I) organized as the Pittsburgh, Cincinnati & St. Louis, also retaining the informal name of Pan Handle (or Panhandle) Route. Even at this time, the Pennsylvania Railroad had a large investment in the line and it was for practical purposes part of the Pennsylvania system.

In 1890 the PC&StL became part of the Pittsburgh, Cincinnati, Chicago & St. Louis, which operated a network of midwest lines between those four cities. Close association with the Pennsylvania Railroad was continued, and in 1921 the PCC&StL was leased to the Pennsylvania. The Pennsylvania merged in 1968 with the New York Central to form Penn Central, which went bankrupt in 1970 and was re-born as ConRail in 1976. Since the creation of ConRail, the former Panhandle line between Pittsburgh and St. Louis has gradually lost traffic to other ConRail routes, and parts have been torn up. Though the track is in place between Pittsburgh and Columbus today, the line is in regular use only between Pittsburgh and Mingo Junction.

The Cleveland & Pittsburgh Railroad was extended to Steubenville in 1855. This road had a through route between its namesake cities via East Liverpool, on the river 25 miles north of Steubenville. At Yellow Creek, several miles below East Liverpool, a branch was constructed south along the river to Steubenville, where it ran along the bank of the river. The C&P ran underneath the west approaches of the Panhandle's Ohio River bridge, and a connecting track from the Panhandle ran north off the bridge's approach fill and descended to join the C&P. This connection was severed at some point early in the 20th century, and the two roads then did not make physical connection until Mingo Junction. The C&P eventually was extended south of Mingo Junction along the river to tap southern Ohio coalfields. The C&P became part of the Pennsylvania Railroad system and then part of Penn Central and ConRail. The C&P route today remains an important means of shipping iron ore from the Cleveland docks to the mills of Steubenville, Mingo Junction and the Pittsburgh area, and of shipping out iron and steel products.

Across the river, the Pittsburgh, Wheeling & Kentucky Railroad opened from Weirton to Wheeling in 1878. This line was leased to and later acquired by the Pennsylvania RR, connecting with the main line at Weirton Junction.

The third railroad that served Steubenville was not nearly as large as the Pennsylvania and provided only local service, since Steubenville was the end of a branch line. The railroad was the Wheeling & Lake Erie, which was leased in 1949 by the Nickel Plate (New York, Chicago & St. Louis Railroad). The W&LE was founded in 1871 to exploit southeast Ohio coalfields ignored by other railroads (the Pennsylvania and its predecessors would have been an obvious choice, but these lines were not interested in creating competition for the coal mines in Pennsylvania that they already served). The W&LE entered Steubenville in 1890, coming up the river from the south. The Wheeling's main line from Toledo ran directly to

the Ohio River at Warrenton, 14 miles below Steubenville. Here branches ran south to Wheeling and north to Steubenville. The Nickel Plate became part of the Norfolk & Western in 1964, but in 1990 the former Wheeling & Lake Erie was sold by N&W and became a regional independent once again, operating under its historic name. W&LE passenger service consisted of two daily accommodation trains into and out of Steubenville in the 1890s, with service ending around 1931 or 1932. The track into downtown Steubenville has been gone for some time, but the W&LE still provides freight service to the steel mills at the south end of the city. Nothing remains of the W&LE passenger and freight facilities in the downtown area.

Though Steubenville was a destination and point of origin for many materials and products, it was not a railroad terminus: it was an intermediate station on the way to someplace else. The Cleveland & Pittsburgh line passed through the city on its way from Cleveland to the mills at Mingo Junction and the coal fields to the south. The Panhandle line ran through Steubenville on its east-west route between Pittsburgh and Columbus, once again making the city a point along the way. The Wheeling & Lake Erie did terminate in Steubenville, but it was a low-density branch line and not a major freight or passenger route.

Though it was not a terminus, Steubenville did have moderate-sized servicing facilities on the Panhandle line for locomotives and cars, located near the passenger station in the downtown area. This would have served mostly switch engines which shifted cars to and from local industries. No traces of these facilities have survived above ground.

A new Pennsylvania Railroad passenger depot was built in 1911 at Sixth and Market streets in Steubenville. The depot it replaced, which was demolished, was a two-story brick structure in the Italianate style. It had a gable roof, two-over-two double-hung windows, and incised hoodmolds, and it had a projecting central bay two stories in height. The new depot was built of buff-colored brick and was one story in height. It had a central pavilion where the ticket window and waiting room were located, and flanking wings housing mail, baggage and express functions. The central pavilion was higher than the wings; all three had low-pitched hip roofs covered with rounded roofing tiles. This depot was demolished in stages beginning in the 1970s; the last portion came down in the late 1980s.

Although the city once had three passenger depots, the Pennsylvania (Panhandle) depot was the only one remaining by 1947 (the C&P depot was on the west side of the tracks at Market Street, and the Wheeling & Lake Erie depot was on the east side of Lake Erie Avenue between Washington and Market, near the stub end of that line). To date no information about the appearance of these other two depots has been found.

Steubenville was on the Pennsylvania (Panhandle) main line between New York and Washington to Cincinnati and St. Louis. As a result, important name trains provided Steubenville passengers with through service, including The Spirit of St. Louis, The American, The Jeffersonian, The Cincinnati

Limited, and The Golden Arrow. In addition, un-named locals provided service between smaller towns and flagstops. Plans were announced in 1947 to provide coast-to-coast service, though this lasted only into the mid-1950s due to the expenses of operation and the general decline in passenger traffic due to increasing auto use for inter-city travel.

The Pennsylvania also had extensive freight facilities. In 1947, 14 cars could be loaded at one time at a warehouse with over 10,000 square feet of space including a 350 foot truck loading platform. An additional 40 cars could be worked from team tracks that were located at various places in the business district. The railroad also had spurs that extended into the steel yards, to warehouses, and to other major industries. The Pennsylvania freight house was east of the Panhandle tracks, north of the passenger depot. Except for some brick paving, nothing today remains of this facility. No rail-served warehouses or industrial plants, other than the steel mills, have survived.

The C&P line had no freight houses or other freight facilities. The W&LE had a small freight house east of the tracks between Market and Adams streets, but no trace of it survives.

### **Interurban and Streetcar Lines**

S.T. Dunham and other investors from New York City in 1888 built a 2.4-mile streetcar line in Steubenville, one of only four electric street railways running in the United States at that time. Research so far has not uncovered the factors that induced these men to come to Steubenville to build an electric car line. It is known that an electric light company formed by Steubenville interests was formed in 1887 and had generating capacity available to run streetcars. This was a very early installation and there were few others in the country, and this no doubt influenced the streetcar-line builders. The line went down Sixth Street from Stanton Boulevard to Franklin Avenue and then followed Fourth Street ending at Wilson's Corner. It was absorbed by the Steubenville Gas & Electric Company in 1889. Because such companies both had electric generating capacity, it was common for streetcar and interurban lines to become associated or merged with electric utilities. Later, when the car lines were abandoned, the utility company often survived and used the old rights-of-way for power lines.

In 1900 Steubenville Gas & Electric was sold to Philadelphia interests and became the Steubenville Traction and Light Co. This eventually evolved into the Ohio Power Co. (what appears to be a former Steubenville Traction and Light boiler house is still standing near South Street adjacent to an Ohio Power Co. switching yard). However, in 1906 the streetcar operations were split off from the power company and became part of a series of city and interurban lines connecting Steubenville, East Liverpool and Beaver, Pa.

In the early 1900s, George Henry promoted a new car line which led to Pleasant Heights, a new development on one of the hilltops overlooking downtown. The Oregon Avenue streetcar loop was completed in 1925. These

were instances of car lines being developed specifically to make new hilltop residential developments more appealing to buyers in the era before widespread auto ownership. The ability to hop a streetcar and ride up the steep hills to one's home overcame a serious obstacle to hilltop development, and in Steubenville the car lines may be credited with starting the move uphill away from the traditional downtown residential areas.

None of Steubenville's electric car lines appears to have had a horsecar line as a predecessor. They were built as electrically-powered lines. Because the routes were short, substations were not needed, and there appear to have been only two carbarns, both located in the city's north end. One small barn was on the west side of Sixth, north of Franklin, and the other was nearby, on the east side of Seventh just below where Sixth ran into Seventh.

Three companies began in the early 1900s which supported the transportation boom in Steubenville. The Steubenville & East Liverpool Railway & Light Co. was organized in 1906, marking the beginning of interurban lines in Steubenville, as opposed to purely in-city car lines. Dohrman J. Sinclair was the organizer of the two other companies -- the Steubenville Bridge Co. and the Tri-State Traction Co. The bridge company constructed a bridge across the river at Market Street in 1905 and the traction company laid lines across the bridge that split on the other side, with one going to Wheeling and the other travelling north toward Weirton. To facilitate the construction of these lines, a boulevard was cut into the bluff across the river opposite the city. It was the construction of these transit lines that enhanced industrial development along the river by providing affordable transportation for workers. Other than this effort by a Steubenville industrialist, there is no evidence that the steel companies played a role in developing streetcar lines or the utility companies that powered them. Conversely, Sinclair appears to have been the only industrial leader involved in developing urban rail transportation in the Steubenville area.

The period of interurban and trolley transportation was cut short by mass acceptance of the automobile. The last streetcar was driven over city streets in December, 1939.

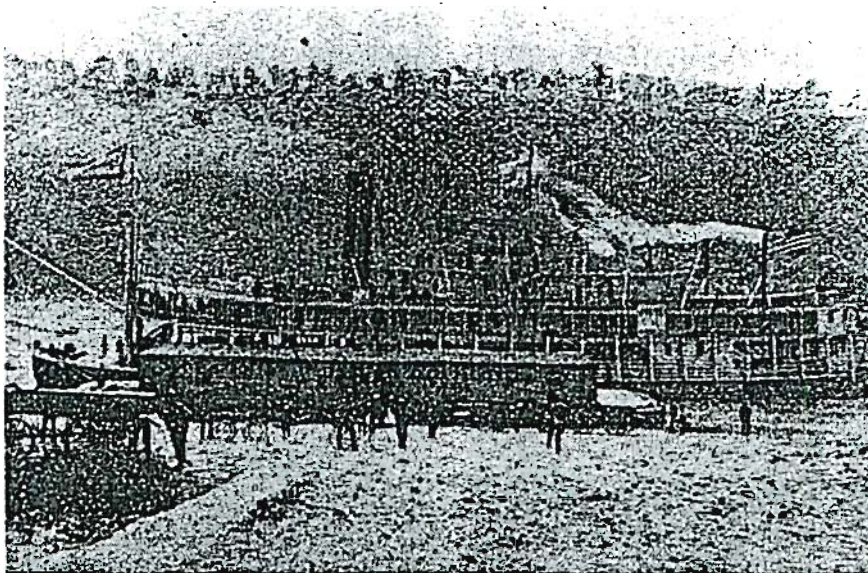
### **Roads and Bridges**

Steubenville's street pattern is regular in nature -- it employs the traditional urban grid -- but its development was strongly affected by local topography. In the downtown area where development first occurred, the grid pattern was followed almost exclusively; it terminated at the north as the river turned west, and at the south in the area of the steel mill. To the west, the grid pattern was followed until the increasing steepness of the hill made streets impossible; even today there are rough hairpin-turn trails up the hillsides. Another grid developed in the Pleasant Heights area around the turn of the century, linked to the downtown by a few streets which climbed the hills. The main axis to the

west was Market Street, and the Market Street Hill was a formidable obstacle. Even today there can be seen, on the abandoned portion of the hill, an old horse watering trough located halfway up the hill for the benefit of hard-working horses. Most hilltop development occurred after the 1920s, when concepts of suburban design moved away from the urban grid in favor of curvilinear streets and cul-de-sacs.

Steubenville was a leader in paving its streets. In 1884, Third Street between Market and Adams became the first street in the state paved with vitrified brick. No sign of this pavement is evident today, though it may be covered by modern materials. The Market Street Bridge, which is mentioned above, also carried automobile traffic. The Ft. Steuben Bridge was dedicated in 1928. The state acquired the bridge for \$1,600,000 in 1936 and when revenues paid off the cost of the bridge, it was made toll-free. The bottleneck created by Pennsylvania Railroad grade crossings was eliminated in 1931 when \$300,000 was approved to build a street underpass at Washington Street.

The only significant railroad bridges in Steubenville were the 1925 Panhandle (now ConRail) bridge in the north end, and the steel plant's bridge connecting facilities on both sides of the river. As was noted above and in the industrial context, the Panhandle Bridge is the third on the site, and the steel plant bridge dates from 1915.



Until arrival of the railroads in the 1850s, river steamboats were the principal means of transportation and communication between Steubenville and other cities and towns.

## Automobiles

Automobile transportation in Steubenville developed in patterns similar to those elsewhere early in the 20th century. Within the first decade of the century, Steubenville had auto dealers in the downtown area, and during succeeding decades both the number of dealers and the number of associated enterprises such as filling and service stations, repair shops, and parts suppliers grew rapidly.

The earliest dealerships appear to have grown out of other businesses such as machine shops and carriage and boat dealers; they apparently added autos as a new line of business, and new start-up dealerships devoted solely to automobiles appear to have been rare during the early years. By 1911, though, the city could boast dealerships such as the Auto Shop Co. on North Alley, Simpson & Hunter on North Court St., Collins Brothers in the 500 block of Market Street, and The Automobile and Motor Boat Company on South Fifth Street.

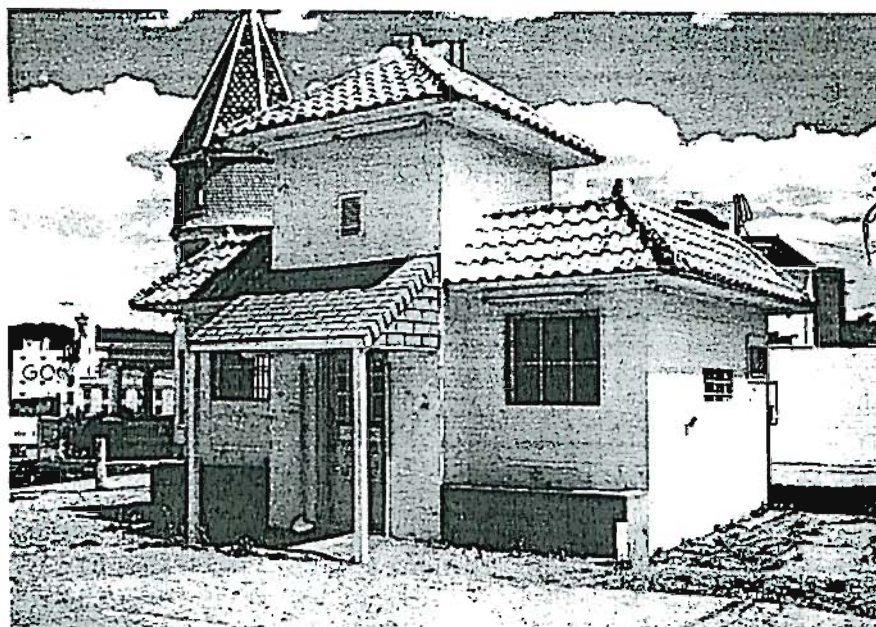
During the 'Teens and early 'Twenties, as the auto proved itself mechanically, began to gain wide acceptance, and the "good roads" movement began to result in government-financed hard-surfaced roads, automobile-related businesses in Steubenville flourished. By 1913 there were nine dealerships, ten garages for repairs, and allied services such as auto painters and upholsterers. The Auto Shop Co. and Simpson & Hunter were no longer listed, but active dealerships in 1913 included the other two noted above, as well as O.J. Daugherty, the Motor Inn, Ross Motor Co., Steubenville Motor car Co., and James Workman & Sons.

This period saw the rise and fall of many makes and models of autos, as hundreds of small manufacturers attempted to win a portion of the mass market for cars that large firms such as Ford had helped to create. Along with this diversity of makes came dealerships to sell them. By the mid-1920s, Steubenville had 19 auto dealerships, and, in addition, the city directories show 2-1/2 pages of listings for allied services such as auto financing, cylinder re-grinding, spring dealers, repairing, tire dealers, and towing. Dealers and their car lines included J.S. Cookson Motor Co. (Jordan), Thomas J. Duffy Motor Co. (Oakland and Pontiac), Fort Steuben Motor Co. (Studebaker and Erskine), J.S. Moore Motor Co. (Fargo), B.D. Rickey Motor Co. (Cole 8 and Hupmobile), M.E. Ryder Motor Co. (Packard), and J.W. Sanders Motor Co. (Chandler). Nearly all of these dealerships were located in the downtown area within three to seven blocks of Market and Fourth Streets. The Nash-Brandt Co., at 1448 Belleview Boulevard, appears to have been the first to follow the growing shift of the city's wealthier population to the hilltop neighborhoods.

From the late 1920s on, the automobile industry nationally went through a period of consolidation, when the smaller, more obscure manufacturers failed or were bought out and the huge auto companies such as Ford, Chrysler, General Motors, American Motors, Packard, and Studebaker came to dominate the business. This trend was reflected in Steubenville, where the number of auto dealerships generally declined over time. At the same time, the number of service stations and other auto-related businesses grew, as more and more people became able to purchase automobiles.

In 1929-30, Steubenville had 17 auto dealerships and 14 "filling" (not "service") stations. Several earlier dealerships had been replaced by new ones (from the beginning, this probably was due to a combination of business failures, mergers, and name changes). By the mid-1930s there were 14 dealerships, but the number of service stations had grown to 38; this pattern remained fairly constant through the early 1950s when, including 16 dealerships and 34 service stations, the city directory showed five pages of auto-related businesses.

By 1959, the auto business in Steubenville had largely assumed a form typical of today -- a relatively small number of dealers selling the products of a few very large carmakers, with several of the dealers located outside the downtown area. At the same time, there were many service stations and repair shops. Of eight dealerships listed in 1959, three were located in the growing suburban area along Sunset Boulevard, and Steubenville had 45 service stations.



This former auto service station, located west of the downtown core, is reminiscent of the automobile's early days in Steubenville. It retains its original form and its clay tile roof.

## **ASSOCIATED PROPERTY TYPES**

Historic properties associated with transportation in Steubenville have not survived in large numbers; level of survival varies with transportation mode. Properties associated with the Ohio River, the railroads, and interurbans and streetcar lines are almost entirely gone, as are those associated with truck or bus transportation. Properties associated with the automobile, including roads and bridges, auto dealerships, filling/service stations, and repair shops, have survived in greater numbers but vary greatly in significance and integrity.

### **The Ohio River**

No known property types are extant.

### **Railroads**

No known property types are extant, with the lone exception of the Panhandle bridge across the Ohio River.

The Panhandle bridge should be considered eligible for the National Register under Criteria A and C in the field of TRANSPORTATION. It survives as evidence both of the role played by the railroad in the industrial, commercial, and social development of Steubenville, and of early 20th century railroad engineering practice. The bridge is important because it linked Steubenville to national rail network and the Industrial Northeast of the United States; and because its cantilever design represented a solution to the problem of building long-span bridges over wide navigable waterways without piers that obstructed navigation.

The Pandhandle bridge qualifies for listing under Criteria A and C because it has the following aspects of integrity:

#### **1. LOCATION**

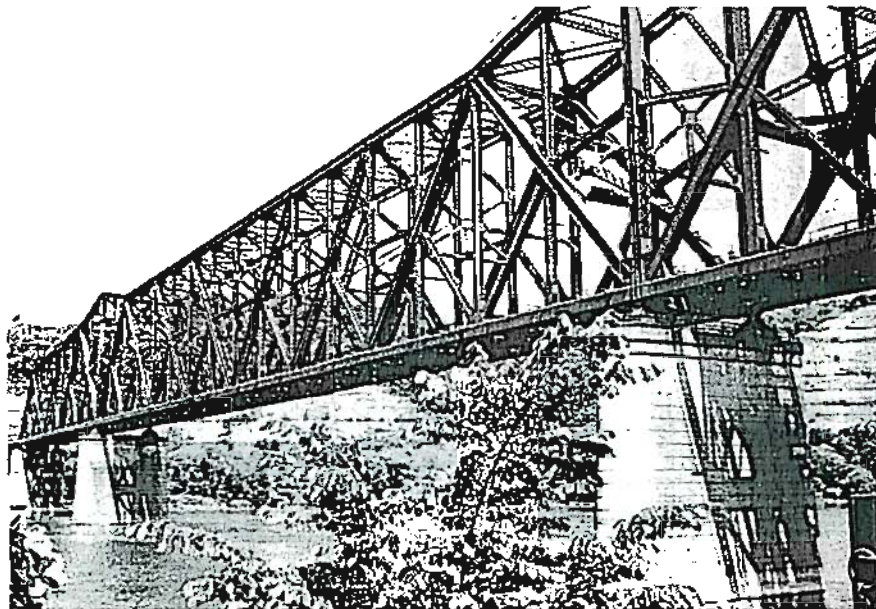
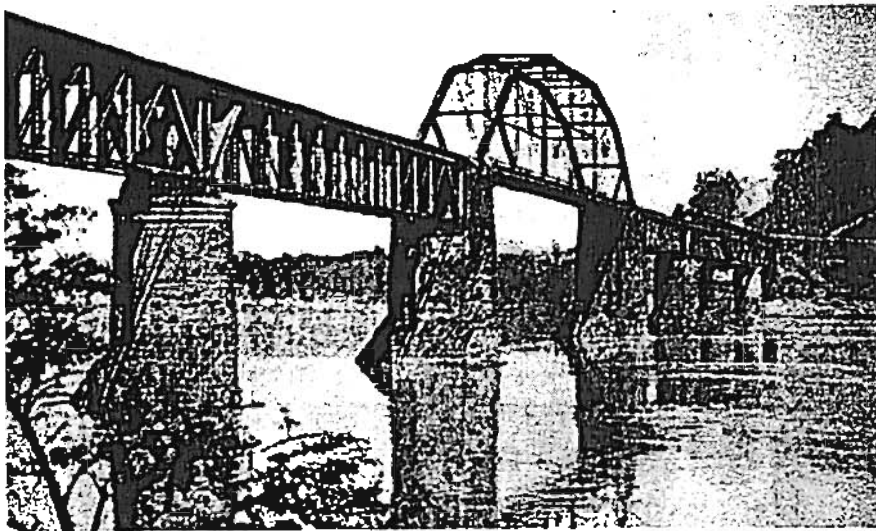
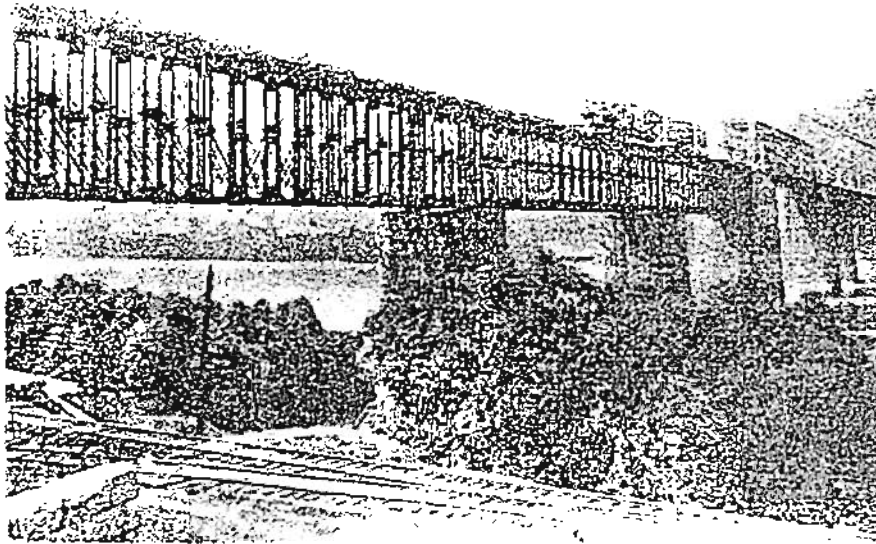
The bridge is in its original location, which is the same location as earlier rail bridges across the Ohio.

#### **2. DESIGN**

The bridge is an example of a subdivided Baltimore truss that has not undergone significant alteration since its construction in 1925. All of the important elements of its design -- its cantilevered construction, its truss design, and its use of only two river piers set close to either shore -- remain intact.

#### **3. MATERIALS**

The historic materials of the bridge have not been altered since construction, though one of its two original tracks has been removed.



There have been three "Panhandle" bridges on the railroad line between Steubenville and Weirton, the earliest dating from the 1860s and the last from 1925. It remains in heavy daily use.

#### 4. ASSOCIATION

The bridge was originally part of a through rail route between Pittsburgh and the Midwest, but it also served an important local function by providing a means of moving freight cars associated with the Steubenville-area steel industry between plants on both sides of the river. Though the bridge's role as part of a through route is much reduced, it still is vitally important to the flow of raw, intermediate, and finished materials between local steel plants. The bridge is currently owned and used by a railroad company (ConRail) that is a direct descendent of the original builder (the Pennsylvania Railroad), and of the builder of the first bridge here in the 1850s (the Steubenville & Indiana).

#### **Interurban and Streetcar Lines**

No known property types are extant.

#### **Roads and Bridges**

Property types include roads and bridges themselves, as well as warehouses and terminals used by trucking and bus companies. In Steubenville, only the Market Street Bridge and the Mosel Johnson Company Warehouse qualify for listing in the National Register. A bypassed portion of the Market Street Hill and its associated horse watering trough have been listed in the National Register as unique reminders of the importance of road transportation in the city's history. No other property types are extant.

The Market Street bridge qualifies for listing under Criteria A and C because it has the following aspects of integrity:

##### 1. LOCATION

The bridge is in its original location at the foot of Market Street in downtown Steubenville.

##### 2. DESIGN

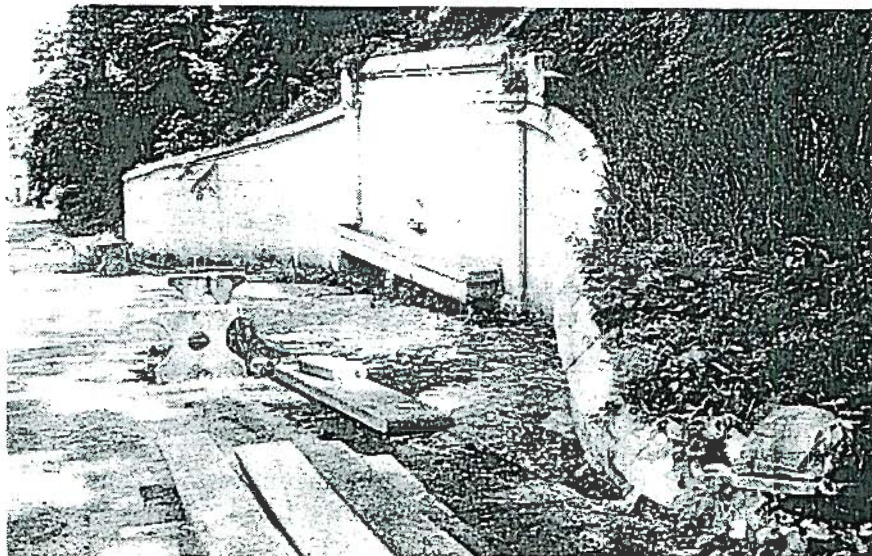
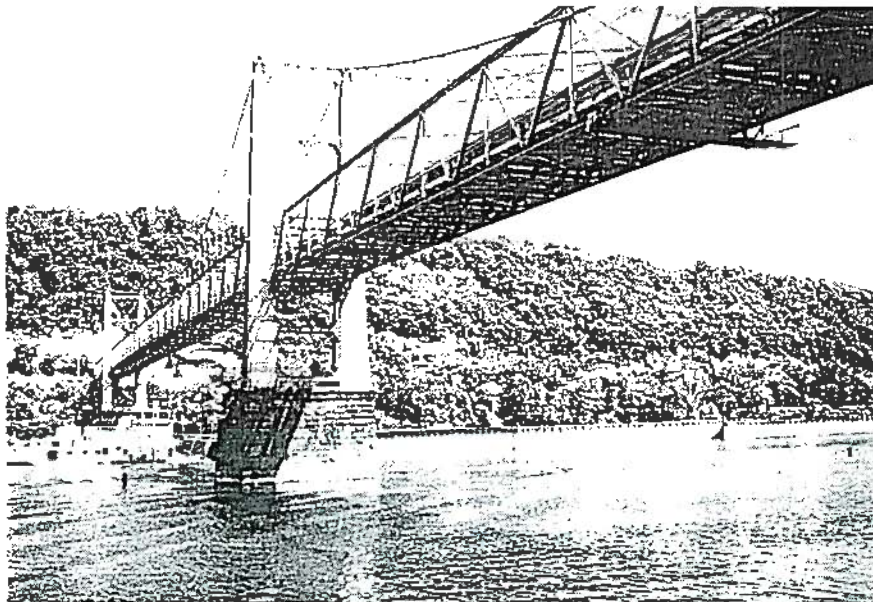
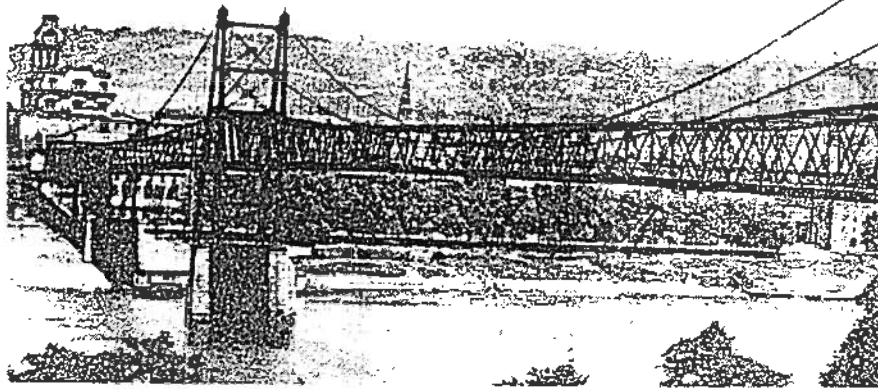
The Market Street bridge is an early 20th century suspension bridge designed for auto/truck and streetcar traffic. The earliest extant Ohio River Bridge at Wheeling, WV (1849) was a suspension bridge; the suspension type of bridge was a favored design for vehicular crossings along the Ohio, since a wide clear span with its high point in the center was ideal for avoiding interference with river traffic.

##### 3. MATERIALS

The historic materials of the bridge have not been significantly altered since construction, though the streetcar tracks have been removed and the deck surface replaced.

##### 4. ASSOCIATION

The bridge is sufficiently intact to exemplify Steubenville's growing



The Market Street Bridge, shown in both an early view and a recent one, has spanned the Ohio River for nearly all of the 20th century. The bottom photo shows the "uphill" end of the horse watering trough on the old Market Street hill.

importance during the late 19th and early 20th centuries as a regional center of trade, commerce, and industry. Overcoming the barrier presented by the Ohio River was essential to permitting a free flow of goods and people in the Tri-State area, and doing so with a bridge anchored in downtown Steubenville helped ensure its regional hegemony.

The Mosel Johnson Company Warehouse, at 311 N. Seventh Street, dates from 1911 and is the lone survivor of many goods warehouses once located in Steubenville. A flat-roofed three-story building of utilitarian design, it was located to permit both truck and rail shipments to be sent out or received.

The Mosel Johnson Company Warehouse qualifies for listing under Criterion A because it has the following aspects of integrity:

1. LOCATION

The warehouse is in its original location between North Seventh Street and the Panhandle (ConRail) railroad tracks.

2. DESIGN

The warehouse retains all the elements of its original design: a multiple-story, flat-roofed form; a street elevation with some effort at architectural design and detailing; and plainly-finished side and rear elevations with a minimal number of openings.

3. MATERIALS

The warehouse retains virtually all of the materials dating from the time of its construction.

4. ASSOCIATION

The warehouse is sufficiently intact to communicate its importance in the transportation/commercial life of Steubenville, retaining essentially its original form and details.

## **Automobiles**

Sufficient automobile-related property types survive to justify inclusion here of descriptions, statements of significance, and registration requirements. The property types include automobile dealerships and service stations.

### **Automobile Dealerships**

#### **Description**

Though early Steubenville auto dealerships appear to have been existing businesses that added autos as a new line, by about 1920 there were enough dealers handling only autos that a distinct type of building suited to this business evolved. Usually built of brick, often with cut stone trim, the dealership typically was between one and three stories in height (taller

ones were known, however) and had a flat roof. The upper wall usually terminated in a parapet; upper floor windows sometimes were residential-type one-over-one sash but also might be larger steel framed industrial-type windows.

The building usually was set right at the edge of the sidewalk, and the first floor featured several large plate glass display windows behind which automobiles were displayed. Entrance doors were sometimes centrally located but in some cases were offset to one side. The front portion of the first floor usually was a single large showroom, and often the rear was garage or working space. Side or rear doors were made large enough for cars to drive through for showroom and/or garage access.

### Significance

Steubenville's automobile dealerships are significant under Criteria A and C in the fields of TRANSPORTATION and ARCHITECTURE. These buildings were the places at which the public became acquainted with the transportation mode that would dominate most of the 20th century. Their designs reflected the specific purpose of displaying and selling automobiles, with ancillary functions such as repairs, supplies, and other services. They represented a new adaptation of the traditional form of the commercial building, which relied on large display windows for showcasing goods and providing natural light on the interior.

### Registration Requirements

To qualify for listing under Criteria A or C, a property should have the following aspects of integrity:

#### 1. LOCATION

The property should be on its original site or, if it has been moved, its site should be the place where the property made its contribution to the history of Steubenville.

#### 2. DESIGN

The property should retain the important design elements of an early automobile dealership, such as large display windows opening onto a first floor showroom; but also overall features such as original form and massing, any original ornamental treatment, roof form and shape, and original door and window openings.

#### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey its historical relationship to the automobile industry in Steubenville.

### **Automobile Service Stations**

#### Description

Beginning originally as "filling stations" where gas and oil could be obtained, these facilities evolved quickly into repair and supply shops

that could meet most of the needs of a car owner. Though sometimes set in a corner of a larger auto repair facility, usually the service stations were free-standing small structures of frame or wood construction. The earlier buildings typically had gable roofs and were just large enough for an office and, sometimes, restrooms. Some were built to standard designs that identified particular oil companies. Gasoline pumps sometimes were free-standing on a concrete island in front of the building; and sometimes they were placed under a roof or overhanging upper story. Later buildings often included one or more service bays attached to the office area.

### Significance

Steubenville's automobile service stations are significant under Criteria A and C in the fields of TRANSPORTATION and ARCHITECTURE. Like the automobile dealerships, service stations represented a new type of building created to serve a new and very specific market. Thus they tended to be visually distinctive and easily identified. In Steubenville as elsewhere, they were an integral component of the new automobile mode of transportation that grew rapidly in acceptance and popularity after World War I. They were of necessity located in areas of high auto traffic; early on, this meant in and close to the downtown area.

As streets were extended and new residential commercial areas developed, service stations followed their market and became established in high-traffic locations in suburban areas. They rapidly became a new type of architecture, often with identifying characteristics that differentiated the various oil companies that owned and operated them. Pure Oil stations, for example, employed steeply-pitched blue tile roofs, while Texaco stations used green and white enamelled metal panels as an exterior material.

### Registration Requirements

To qualify for listing under Criteria A or C, a property should have the following aspects of integrity:

#### 1. LOCATION

The property should be on its original site or, if it has been moved, its site should be the place where the property made its contribution to the history of Steubenville.

#### 2. DESIGN

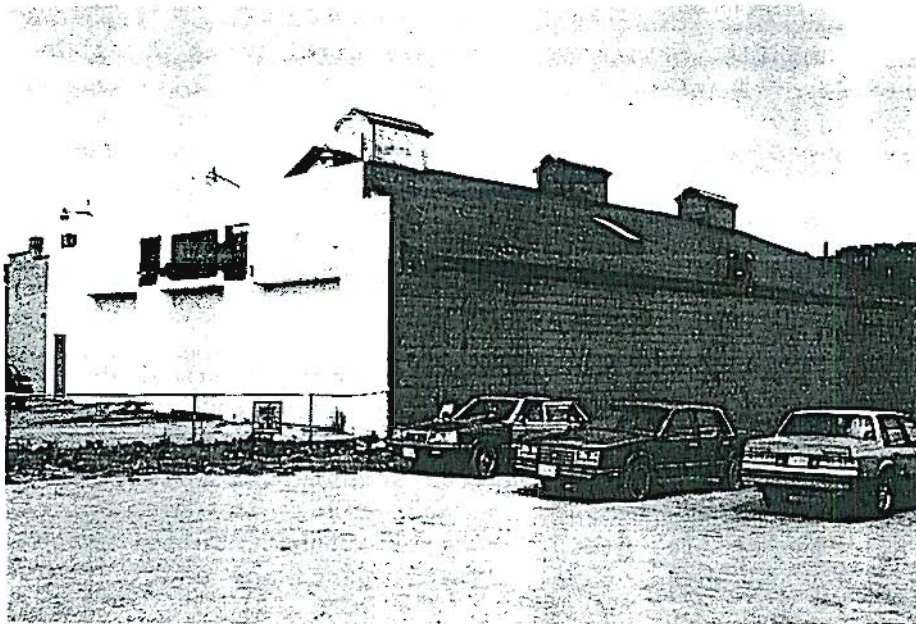
The property should retain the important design elements of an early automobile service station, such as original form and massing, any original ornamental treatment, roof form and shape, and original door and window openings. If the service station had repair bays, these should be intact, as should their large door openings. Since gasoline brands and pump styles tended to change over time, it is not necessary that gasoline pumps survive in order for a property to be qualified for listing.

#### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of

integrity to convey its historical relationship of the automobile industry in Steubenville.

To qualify for listing under Criterion C, in addition to the above aspects of integrity, a property should have integrity of MATERIALS. The property should retain a majority of the historic materials of which it was built, including original exterior surface materials, windows and doors and their trim, and identifying ornamental or stylistic elements, or distinctive elements associated with a particular corporate design.



This former bus garage on South Third Street evokes the earlier days of rubber-tired public transportation, which supplanted streetcars in Steubenville in 1938.

### **Potentially Eligible Properties**

As has been stated before, very few historic resources remain in Steubenville which are related to this context. Two of them -- the Panhandle and Market Street Bridges -- are very significant. Both date from the early 20th century and have played a major role in the development of transportation in Steubenville. Additionally, they both possess a high degree of integrity, and are highly visible landmarks. It should be noted that for designation purposes, they are technically under the control and jurisdiction of the State of West Virginia. Any effort at designation would require cooperation with the West Virginia State Historic Preservation Office.

The W.B. Collier Residence, located at 411 North Seventh Street is related to the context of transportation; however, it derives its primary significance as an unusual and early example of residential architecture in Steubenville.

## Other Surviving Industrial and Transportation Historic Properties

Because so few properties related to the industrial and transportation contexts appeared to have survived, part of the research for these contexts was a study of what actually did exist, either above or below ground. It was not possible to investigate every associated property, but research and field investigation for the major industries and transportation modes resulted in the findings below.

Included are recommendations for how surviving properties or their remains might be protected and enhanced so that the information they contain might not be lost, and so that these properties might contribute to a greater awareness of the city's industrial and transportation history.

### Recommendations

#### A. Protect remaining significant industrial and transportation resources.

Both National Register listing and local designation should be undertaken to increase appreciation and awareness of the city's industrial/transportation history and to provide as much legal protection as possible. There should be an effort to work with property owners to help them understand the importance of remaining industrial/transportation historic resources. They should also be provided with technical assistance to help find new uses for unused or under-utilized property, and to assist them in following recommended preservation techniques when rehabilitation is undertaken.

#### B. Undertake archaeological survey of industrial- and transportation-related sites.

The attached map locates the sites of major industrial enterprises and transportation facilities in Steubenville. As can be seen from the notes below, very few associated buildings and structures have survived, but in many cases the sites remain either open or have only been "capped" by paving materials; the map locates those which may have archaeological potential. It should be noted that this is not an effort to assess archaeological significance but instead simply indicates sites which may yield data if properly tested and excavated. Also, the map does not include every site associated with the industrial and transportation contexts, just those which appear to be of the greatest significance and which seem to have potential for archaeological remains.

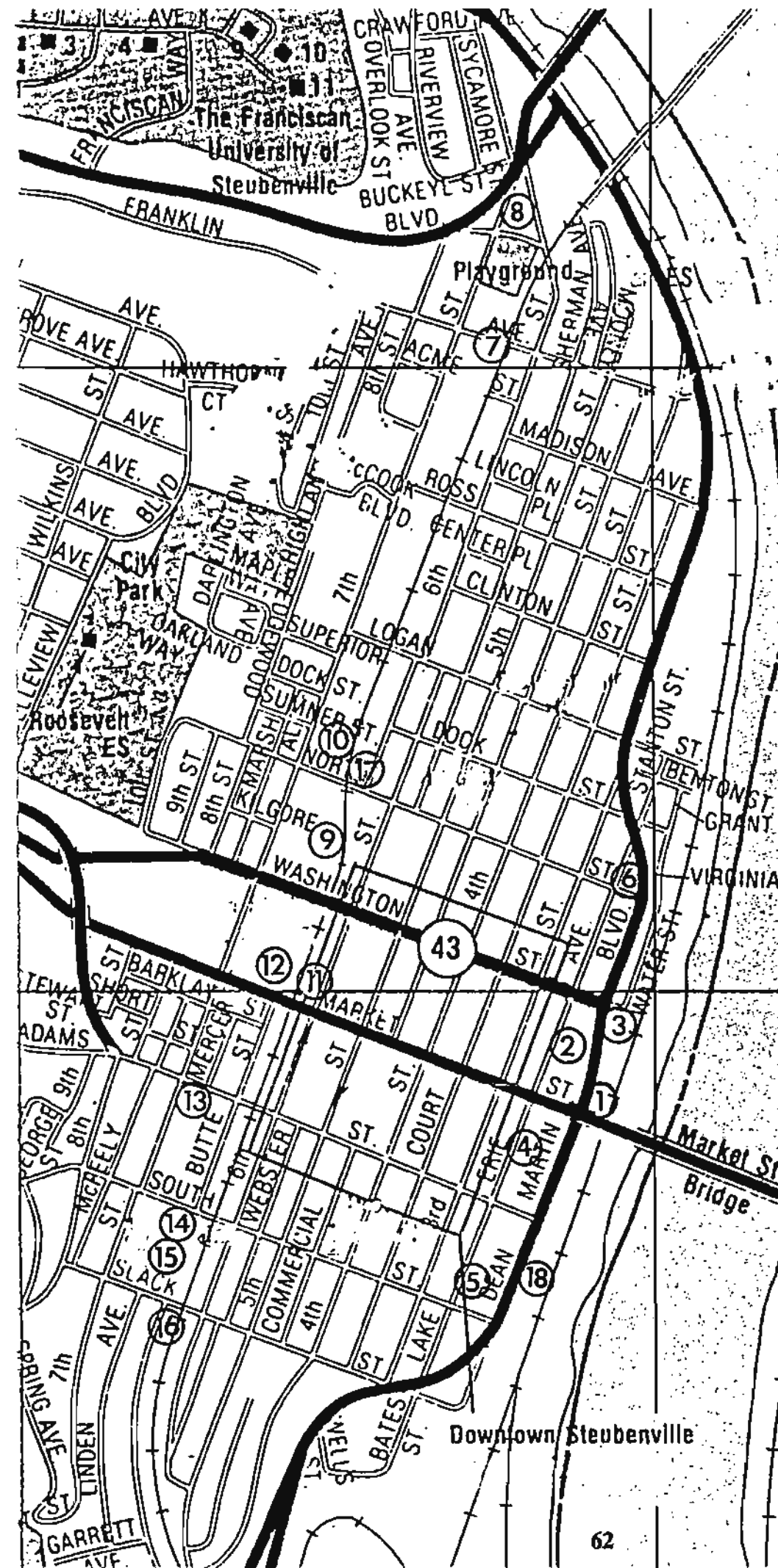
1. Cleveland & Pittsburgh Railroad Depot. Located nearly under the Market Street bridge on the west side of the tracks. Depot demolished; no sign of foundations or passenger platform, but stone paving is visible under asphalt.

2. Wheeling & Lake Erie Depot. Depot demolished. Part of site appears to be covered by new post office, but part may be under paved parking lot.

3. Ohio Valley Clay Company. One portion of building still standing; demolished portions were to north and south of it. Foundations still visible in some areas; considerable ground disruption in other areas. Located west of C&P tracks and east of State Route 7 at Washington Street.

## Steubenville, Ohio Historic Transportation and Industrial Sites

1. Cleveland & Pittsburgh Railroad Depot
2. Wheeling & Lake Erie Railroad Depot
3. Ohio Valley Clay Company
4. Wheeling & Lake Erie RR Freight House
5. Steubenville Traction & Light Company
6. Means Foundry
7. Acme Glass Company
8. Steubenville Traction & Light Co. Car barn
9. Panhandle RR Shops & Freight House
10. Jefferson Glass Works
11. Panhandle (Pennsylvania) RR Depot
12. Ohio Valley Ice & Storage
13. Steubenville (Chicago) Wallpaper Company
14. Steubenville Pottery
15. Ohio Foundry
16. Humphrey Glass
17. Panhandle RR Freight Office
18. Cleveland & Pittsburgh RR Freight House



4. Wheeling & Lake Erie Freight House. Demolished. Site is part of what is now site of Fort Steuben reconstruction. Freight house site may be under paved parking area. Site may also include components from occupation period of Fort Steuben in late 18th century.
5. Steubenville Traction & Light. 1902 map shows round "gasometers" and other buildings, including the surviving boiler house and now-demolished attached structure which probably was the engine house. Located north of South Street, on east side of former W&LE track location.
6. Means Foundry. East end of North Street at State Route 7. Most of foundry site appears to be covered by SR7 pavement, but the James Means house still stands just west of SR7 on the north side of North Street.
7. Acme Glass. Demolished, with the exception of the two buildings located on the north and south sides of Franklin Ave. Southern portion of site is open, and some foundations are visible.
8. Steubenville Traction & Light Car barn. Newer buildings occupy part of the site, but the west foundation wall (stone) and part of a concrete floor are visible.
9. Panhandle Railroad Shops and Freight House. All structures demolished. Site east of tracks entirely open; freight house was here. West of tracks, half of site covered by large retail stores of recent date. These appear to cover the car shop site, but the roundhouse and turntable sites appear to be open.
10. Jefferson Glass Works. Demolished. Mosel Johnson warehouse occupies most of site, but part is covered by only a parking lot.
11. Panhandle (Pennsylvania) Railroad Depot. Last portion demolished during late 1980s. Some foundations and floors still visible. New 1-story building on portion of site. Located on east side of PH track just north of Market and just west of Sixth Street.
12. Ohio Valley Ice & Storage. Demolished. Site covered by Ohio Bell paved parking lot, one brick building, and a roofed, open-sided parking area.
13. Steubenville (Chicago) Wallpaper Company. Demolished. Open, grassy lot with irregular surface. Some foundation walls visible; portion of rail spur still in place on east side of site.
14. Steubenville Pottery. Demolished. U.S. Tool & Supply, which appears to use part of a Kroger store built on the site in the late 1930s, occupies site, but about a third of site is under a paved parking lot.
15. Ohio Foundry. Demolished. Large lumber warehouse covers most of site.
16. Humphrey Glass. Demolished. Site open, brick driveway partly visible.
17. Panhandle Railroad Freight Office. Demolished. Site is open but covered by paved parking lot of Herald-Star newspaper.

18. Cleveland & Pittsburgh Railroad Freight House. Demolished. Location was along west side of tracks between Adams and South Streets. Junkyard covers at least part of site.

Most of these sites appear not to have experienced extensive ground disturbance since the demolition of the buildings on them. In many cases nothing further has occurred except growth of weeds and brush; in other cases paving has been put down over the sites.

Historic maps indicate that little development occurred along the riverbank east of the C&P tracks, probably because the bank was always steep and did not permit easy construction. However, just west of the C&P tracks was the earliest occupied area of the city. It is unfortunate that so much of this area has been covered by State Route 7, but there are accessible parcels where archaeological investigation may reveal information about early Steubenville. These areas, as well as the industrial/transportation sites discussed above, should receive first priority if archaeological investigations are undertaken.

C. Develop an educational program on Steubenville's industrial and transportaton history.

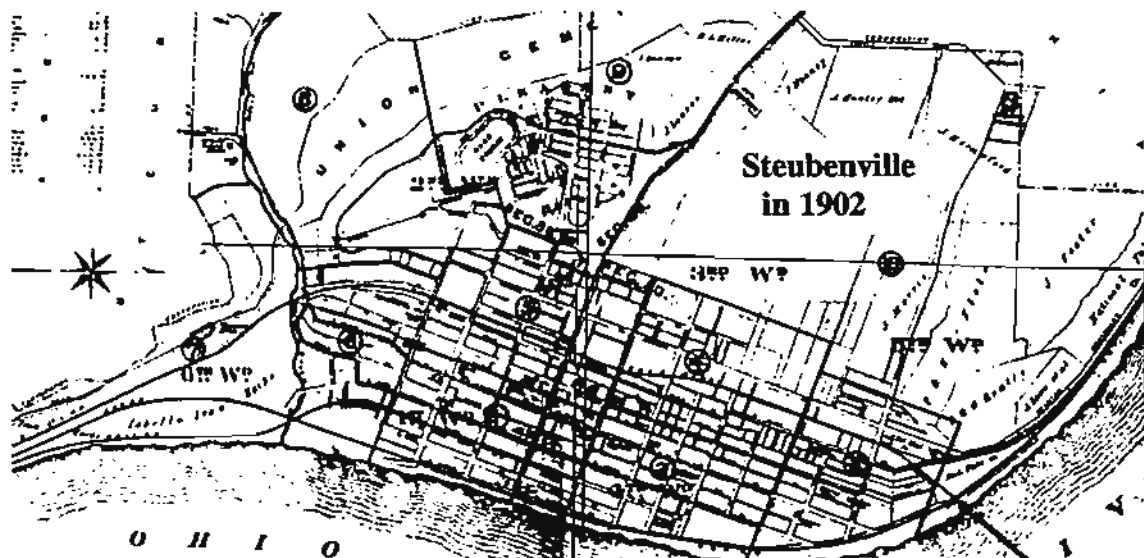
Efforts in this area could include both a slideshow and a printed brochure featuring Steubenville's industrial/transportation past. Schools in particular would be a prime audience, but so would local civic groups, as well as out-of-town visitors (there are, for example, numerous bus tours which come to visit the city's historical wall murals. Several of these depict industrial or transportation subjects).

## DEVELOPMENT OF DOWNTOWN NEIGHBORHOODS IN STEUBENVILLE, 1870 - 1910

The forty-year period between 1870 and 1910 saw Steubenville's downtown neighborhoods assume the character which they still have today, despite considerable loss of building stock since World War II. During the period between 1870 and 1910, most of the existing homes, businesses, churches and institutions were built; those constructed before and after this period are relatively few. During this time, Steubenville's population rose from about 8,000 to more than 20,000, principally as a result of the growth of its industrial base.

North of the downtown commercial core, a north end neighborhood developed along Third, Fourth, Fifth and Sixth Streets. Because it was the extension of the main north-south downtown commercial street, and because it was the most removed from the railroad and industrial facilities east of Third and west of Sixth, North Fourth Street was the preferred address for the businessmen, professional people, industrialists and politicians of Steubenville. Here were built the largest, most ornate and most expensive homes the city had seen to that point, and it remained the city's premier residential street until intensive development of the western hilltop suburban areas after 1910.

Among the North Fourth Street homes of important local figures in this period were the Queen Anne style residence at #306, built in 1893 as the home of Dr. Andrew Elliott, a physician; the home of attorney J.P. Donnan at #319, built about 1905 in the Colonial Revival style; the 1915 Queen Anne style home of S.J. Anathan, one of the founders of The Hub department store, at #502; the home of dentist E.C. Chandler at #504, a Queen Anne style house built about 1915; #506, an excellent frame Italianate style house from about 1870, the home of J. Taggart, who was in the wallpaper business; the Judge Hooper home at #600, a brick Queen Anne style house dating from about 1890, originally the home of W.O. Johnson, a wholesale grocer; the Mansfield-Hennings house at #611, a large and well preserved example of the Colonial Revival style dating from about 1910, whose builder was John A. Mansfield, attorney and probate judge, and it later was owned by A.J. Hennings, president of the Chicago Wall Paper Co.; the Italianate style David McGowan house, dating from about 1880, at #612, the home of a wholesale grocer and bank president; the Gill-Cochran house at #701, built about 1880 in the Italianate style, the home of J.W. Gill of Gill Brothers Glass Co. and Ohio Valley Clay Co., and later occupied by G.W. Cochran,





The Mansfield-Hennings House (McClave Funeral Home) at 611 North Fourth Street is an excellent example of the Colonial Revival style.

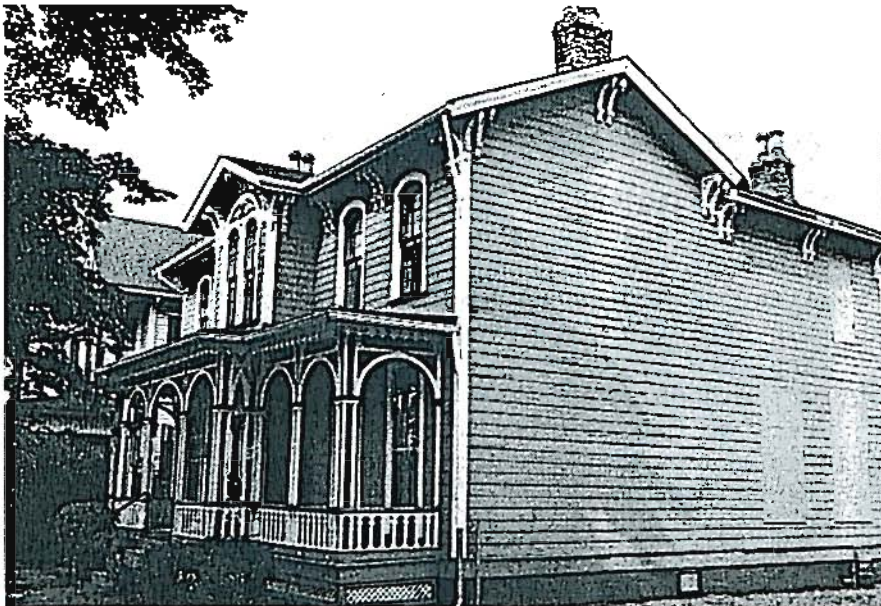
manager at Ohio Valley Clay; the Samuel Gill house at #711, home of another Gill brother involved in glass, clay and banking, built about 1880 in the Second Empire style; the Sulzbacher house, built in the Shingle Style about 1900, located at #736 and the home of the owner of the city's major department store at the time; #741, an excellent Italianate house dating from about 1870, the home of H.G. Garrett, a local real estate owner; #801, the B.F. Payne house, an unusual house from about 1870 combining Gothic Revival and Italianate elements, the home of a prominent late 19th century physician; #827, a Colonial Revival style home from about 1900, home of P.P. Lewis, and attorney, and later J.K. McKee, a banker; the Queen Anne style house at #832, dating from the 1890s and the home of A.S. Buckingham, vice-president of Union Savings Bank and Trust; #836, a Queen Anne style house from 1895, the home of W.M. Beall, one of the owners of Beall & Steele Drug Co., a major Steubenville business; #847, an excellent Queen Anne style house from about 1903, home of Charles McConnell, a clothing store proprietor; and "Grandview" at #848, named for its view of the river, built in 1894 in the Queen Anne style, home of G.N. Henry, who was involved in grocery, produce and land development enterprises.

From the construction dates of these North Fourth Street homes, it can be seen that this street was the preferred place to build between 1870 and 1910. Even after 1910, when suburban development of the western hilltops was well underway, North Fourth still attracted the business, professional, industrial and political leaders of Steubenville.

Between 1870 and 1910 North Fifth Street, too, was an attractive place to live, and much building took place here. However, the builders and occupants on this street were business managers, lower-level professionals and other white-collar workers rather than business and industry owners and officers; the resident mix also included upper-level blue-collar workers. Such people also were more likely to be renters than owners. Examples include #417, a vernacular I house from the 1850s, the home of a railroad conductor; #418, an 1888 Queen Anne style house, the home of a salesman; #605, a late Greek Revival-influenced home from the 1850s, occupied variously by a druggist, a loan company owner, and an insurance man; the Tilney house at approximately #613 (no address on building), a Second Empire style house from about 1880, home of a clothier; #703, a small vernacular brick house from the 1860s, home of a justice of the peace; #719, which has Colonial Revival influences, built about 1910 and home of J.H. Mosel, partner in a wholesale grocery; and #723, an Italianate house from about 1880, home of a lumber and construction man, and later of a real estate agent.

At the north end of Fifth, near Franklin, was a cluster of Queen Anne style houses in an area called "Queen Anne Square" (actually just part of Fifth Street, not an actual square or enclave). These homes were larger and more ornate than typical North Fifth Street homes and had more in common with those on North Fourth. Occupants and owners included an officer of the nearby Gill brothers' Acme Glass Works.

North Third and North Sixth are the other principal streets in the north end neighborhood. Because of their less desirable location near industrial plants along the two railroad lines, during the 1870-1910 period these



The Taggart-Spencer House at 506 North Fourth Street, now used for offices, is an unusually well-preserved frame example of the Italianate style.

streets did not attract the high-style, large and expensive homes found on North Fourth, nor the white collar/managerial homes typical of North Fifth. On both Third and Sixth there are residences built in academic architectural styles, and even some occasional exceptional houses (such as #735) which look as though they belong on North Fourth. Generally, however, homes along these streets tended to be modest in scale and more vernacular in design, with just elements and influences of academic styles. Frame construction is more common on these streets, and the building stock obviously was intended to serve a working-class rental housing market. Because of the closeness of these streets to industrial plants such as Acme Glass, Hartje Paper, and Means Foundry, it was logical that blue-collar industrial workers would live here, a fact supported by census data which indicate that these enterprises indeed employed area residents.

In this area, and in fact in the entire north end neighborhood described here, there were no enclaves of ethnic groups. Certainly there were foreign-born residents here, many of whom became prominent and successful business people and industrialists (especially the English, Irish, Scottish and Welsh, who did not have to overcome a language barrier). However, ethnic groups were not found in groups and clusters here as they were in the south end neighborhood. In the south end, census data indicate that most immigrants were from southern and eastern Europe and that they tended to live in clusters or enclaves. Some of these groups were as small as one or two apartment buildings. Refer to the context entitled "Ethnic Diversity in Steubenville" for more information about such buildings.

In the south end, the neighborhood's character was notably different from that in the north end, even though the south end generally developed during the same 1870-1910 period. Because of the neighborhood's proximity to the large steel facilities in the city's southern end, large numbers of unskilled and semi-skilled workers, often foreign-born and not fluent in English, lived here. There were relatively few business and industry owners, political figures, or upper-level white collar workers.

The architecture of the south end was correspondingly modest, with a much greater ratio of frame construction than in the north end. Double houses were more common, and rental housing was generally more common than owner-occupied housing. Examples of academic architectural styles are uncommon, though most buildings have at least some stylistic elements or ornamentation. There were two general building types, single-family homes and doubles. While the doubles usually were of a symmetrical design, there was no regular pattern of floor or building plan. Single-family homes sometimes has side entrances and sometimes central; doubles also could have either type of entrance, with corresponding variations in floor plan. On the interiors of singles and doubles, central hall, side hall and designs with no halls could be found. Building height was almost universally two stories, with some examples of two-and-a-half stories. Most buildings had porches, usually simple frame porches covering some or all of the main elevation. Some porches were simply stoops with canopies.

Unfortunately, south of Slack Street most structures along South Third and everything west of South Fourth to the railroad have been demolished.

Along South Fourth there remain, however, enough older residential structures to impart a sense of how this area looked, and some can still be found on South Third, on Slack, and on North Fifth, Sixth and Seventh Streets north of Slack. South Fourth tended to have the larger, better-built residences, while the other streets had smaller and more modest structures.



A late-nineteenth century double at 328-332 South Fourth Street has Italianate detailing and a handsome early twentieth century Colonial Revival porch.

Examples include 249 South Fifth, built around 1910 with Colonial Revival elements, the home of a barber; on South Fourth, #236, a frame residence with Queen Anne elements, built about 1905 and the home of a sand processing company officer; #328-332, a double with Italianate and Colonial Revival style elements; #460-462 and 464-466, a pair of doubles dating from about 1900, with Colonial Revival style elements, which were rented over the years by numerous employees of local businesses and industries; #511, a single-family residence from about 1910, with Colonial Revival style elements, the home of a shipping clerk; and #531, a frame house from about 1900, with Queen Anne style elements, the home of a department store employee.

Part of the downtown neighborhoods' fabric were the schools that traditionally served the children of the residents. These often were large and impressive, standing out both on the skyline and along the streetscape. They included public schools such as Steubenville High School at 420 North Fourth Street; Grant Junior High School, 257 South Fourth Street; and Washington School at the corner of Adams and Ninth Streets; and parochial schools such as those associated with Holy Name Cathedral, St. Stanislaus Church, St. Anthony's Church, and St. Peter's Church. These buildings not only had a physical presence but also played an important social role both for the children who attended them and for their parents and families.

## ASSOCIATED PROPERTY TYPES

### Single-Family Residences

#### Description

As Steubenville developed from a frontier settlement into an industrial, commercial and transportation center, its residential architecture evolved as well. From crude early log or plank cabins and houses of vernacular design, Steubenville homes soon came to be built in the popular styles which migrated -- with some delay -- from the east. Blessed with large deposits of good-quality clay, the town early enjoyed the benefits of brick construction. Steubenville founder Bezaleel Wells, for example, as early as 1798 began construction of a brick home in what is today the southeast portion of the downtown. Though it lasted well into the 20th century, the house has been demolished and no above-ground remains are visible. No known archaeological investigations have been made at the site, but it could contain information and is worth further study.

The early vernacular residential structures generally were rectangular in form, sometimes one story in height and sometimes one-and-a-half or two stories. Window and door openings were plain and unornamented; windows typically were either casement-type or double-hung sash, while doors were plainly-executed and had no windows in them. Roofs typically were gabled. Vernacular residential structures generally were occupied by laborers, industrial plant workers and other blue-collar workers. There were no significant differences between north end and south end worker housing.



A one story vernacular cottage enhanced with a projecting bay window and an early twentieth century porch.

By the 1860s Steubenville's commercial and industrial underpinnings were solidly in place, and the city's business, civic and professional leaders sought to express their success through their homes. New residences from this period on typically were built in the currently popular architectural styles, and vernacular structures were built less and less often. High, Third and Fourth Streets, north of the central business district, in this period were the most desirable streets on which to build, and it was here that Gothic Revival and Italianate style homes, the earliest in the city built in academic architectural styles, began to appear.

Brick construction was more favored than frame for these homes. Those in the Gothic Revival style tended to be large, with complex massing and floor plans and with the steeply-pitched gable roofs and pointed-arch windows typical of the style. Windows were double-hung, with 6/6, 4/4, and 2/2 sash common. Residences in this style, or employing its elements, were built only until about the early 1870s.

Italianate style homes in Steubenville had the boxy massing and vertical proportions of that style, as well as the bracketed projecting cornice, transomed entrances and ornamented window trim which also were typically Italianate. Roofs were low-pitched, and windows commonly were 4/4 or 2/2 double-hung sash. Front porches, either full or partial width, also were common features. Italianate residences, or those employing Italianate elements, were built as early as the 1850s, reached their peak in the the 1870s, and still were built as late as the 1890s.



The Judge Hooper House at 600 North Fourth Street has a handsome Eastlake porch.

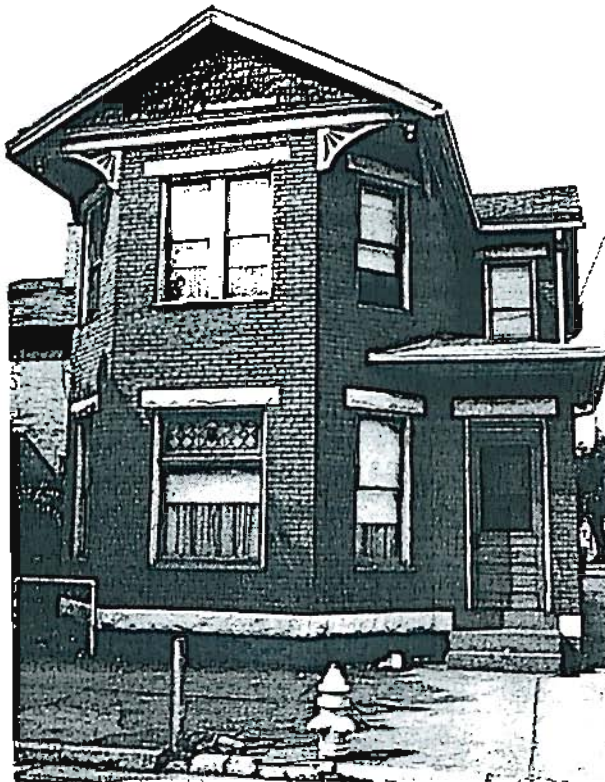
While some homes were built employing French Second Empire style elements such as mansard roofs, this was not a style typical of Steubenville. Builders instead persisted in employing Italianate designs until Queen Anne/Eastlake designs became popular around 1880. Complex massing, asymmetrical compositions, great variation in window shape and size, and extensive use of turned, cut and carved decorative elements characterized these homes. Queen Anne houses and those with elements of the style were built in large numbers in Steubenville until about 1910.

Steubenville's downtown neighborhoods also saw the construction of buildings in two early 20th century revival styles, Colonial Revival and Tudor Revival. Colonial Revival style structures were symmetrical in composition and employed elements such as fluted columns, pedimented windows, roof dormers, fanlights, gable roofs and dentilled cornices. Windows showed a return to multi-paned designs such as 6/6 or 9/9. Tudor Revival homes employed half-timbering, casement windows, steeply-pitched roofs and prominent chimneys reflecting the English antecedents of the style. Colonial Revival and Tudor Revival style residences were built in Steubenville between the late 1890s and about 1910.

During this period there was not a great variety in vernacular design -- there were only three categories of vernacular house type. One was a single-story or story-and-a-half cottage, commonly of frame construction and only occasionally built of brick. These structures were generally small in size and rectangular in shape, sometimes with original or added ells. Roofs were almost always gable in form, with a variety of roofing materials including sheet metal, wood shingles, and slate. Gables were sometimes turned to the street, but gables perpendicular to the street were more common. Window and door openings were sometimes arranged symmetrically and sometimes not, but in all cases were simple rectangular openings with minimal or no decorative trim. Windows commonly were 2/2 or 1/1 double-hung sash. Porches were common features, usually placed at the front entrance. They were sometimes full-width and sometimes covered only the entrance, and porches almost always were very plainly executed with flat sloping roofs and unornamented posts and railings.

The second vernacular house type was an "enlarged" version of the cottage: a two-story gable-roofed structure, most commonly of frame construction. The long side of the building was almost always turned to the street, and these buildings sometimes had original or added ells. The main block of the building usually had windows and doors symmetrically arranged. Features shared with the cottage form included simple, unadorned window and door openings, use of sheet metal, wood shingles or slate on the gable roof, and partial or full-width porches of very plain design.

The third category of vernacular single-family residences was two stories in height. Examples were divided about evenly between frame and brick construction. Most of them derived their plans, massing and proportions from Italianate architecture, though some of the later examples employed plans, massing and proportions typical of Queen Anne style architecture. The Italianate-derived variants had generally vertical proportions, rectangular or ell-shaped plans, and low-pitched hip roofs, often with a



A vernacular single family residence with Queen Anne decorative elements.

flat upper portion. Italianate cornice brackets were sometimes used but generally were not, leaving the eave and cornice area very plain and unadorned. Window and door openings were simple rectangles, plainly trimmed. Porches were likewise plain and unadorned, usually sheltering only the off-center main door. The Queen Anne-derived variants had more complex, irregular massing and a somewhat less vertical sense of proportion. They also had steeper-pitched roofs but otherwise resembled the Italianate-derived variants, employing simply-trimmed openings and very plain full or partial porches. In both variants, 1/1 double-hung sash windows were nearly universal, though 2/2 were sometimes used in structures built earlier in the 1870-1910 time period.

#### Significance

Steubenville's single-family residences may be significant under Criteria A, B or C in the fields of ARCHITECTURE, COMMERCE, INDUSTRY and POLITICS/GOVERNMENT, depending upon their architectural design or the occupations and significance of their builders, occupants and owners. Particularly in the north end of the downtown, and especially along North Fourth Street, single-family residences were symbols of the wealth, power and success of the most important people in Steubenville in the period 1870-1910. It is important to note, though, that important single-family residences can also be found outside the concentrated area of North Fourth Street.

The period between 1870 and 1910 was when Steubenville grew from only local to statewide and national importance, particularly because of its large and varied industrial base. The single-family homes from this period meet

Criterion A as the homes of the commercial, industrial and political leaders who caused and participated in the city's meteoric rise in importance. Under Criterion A, these homes may be important physical reminders of that growth and may help to illustrate the broad patterns of Steubenville's physical and economic development during its most vital period.

Under Criterion B, Steubenville's single-family homes may be significant through association with prominent individual commercial, industrial and political leaders. Their homes reflected the tastes, aspirations and values of these leaders, acting as symbols of their owners' socio-economic status at particular points in time. The relative success and positions of these people can be read, in part, from the location, design, size and expense of the homes they occupied during their productive lives.

Single-family Steubenville residences may also qualify under Criterion C because they represent certain types and methods of construction. Many of these homes are "textbook" examples of academic architectural styles, while many others either have many elements of a style, or in some cases represent unusual combinations of elements from two or more styles.

Vernacular single-family structures may qualify under Criterion C because they represent vernacular design and construction as practiced in Steubenville during the 1870-1910 period.

#### Registration Requirements

To qualify for listing under Criteria A or B, a single-family residence should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

If a high-style building, the property should have its stylistic features intact. For example, a Gothic Revival building should have its steeply-pitched gable roof and pointed-arch windows; an Italianate building should have its boxy rectangular form, vertically-proportioned openings, shallow-pitched roof, projecting bracketed eaves, and other decorative detailing such as hoodmolds or decorated lintels; a Queen Anne building should have irregular massing, variety of roof shapes and exterior materials, and varied window and door sizes and placement.

If a vernacular building, the property should retain its basic form, roof shape, window and door openings, and interior plan. Original porches should be intact, but loss of porches should not keep a vernacular structure from qualifying if other major design features are intact.

##### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between an event or person and the property. For example, the Means Residence is the home of an important Steubenville industrialist. It survives in its original location

and retains the character-defining features of its Italianate style at the time it was built by Means. Even though its setting was originally in the midst of the Means Foundry, and is now on the edge of a four-lane highway, Means would still recognize his house today and it thus has integrity of association.

To qualify under Criterion C, a single-family residence should have the following aspects of integrity:

#### 1. DESIGN

Refer to the comments above for Criteria A and B.

#### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. In high-style structures, this should include the original exterior surface materials, window and door trim, decorative elements, porch elements, and chimneys. For vernacular buildings, retention of original materials is somewhat less critical, as long as the vernacular building traditions or techniques represented by the structure remain apparent. For example, as long as the basic form and plan remain intact, replacement of a vernacular building's wood siding with aluminum siding will not necessarily keep the structure from qualifying under Criterion C.

#### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For buildings built in architectural styles, this should include, for example, hoodmolds, cornice brackets, and window and door trim for Italianate buildings; patterned brickwork, varied siding materials, and decorative features for Queen Anne buildings; and frieze and cornice details, classical columns, and decorative elements for Colonial Revival buildings. For vernacular buildings, items of workmanship should include simple door and window trim, any decorative features, and any surviving porches.

#### 4. FEELING

The totality of a building's components, whether the building is vernacular or built in an architectural style, should express a specific point or period in time.

#### 5. SETTING (for historic districts)

In historic districts, the physical environment into which buildings are placed should survive with its major features intact. In the North Fourth Street area, for example, the setback of houses behind front lawns, the regular spacing of houses along the street, the existence of plantings and street trees, and the use of sidewalks and curbs all are important parts of the area's integrity of setting.

## Multi-Family Residences

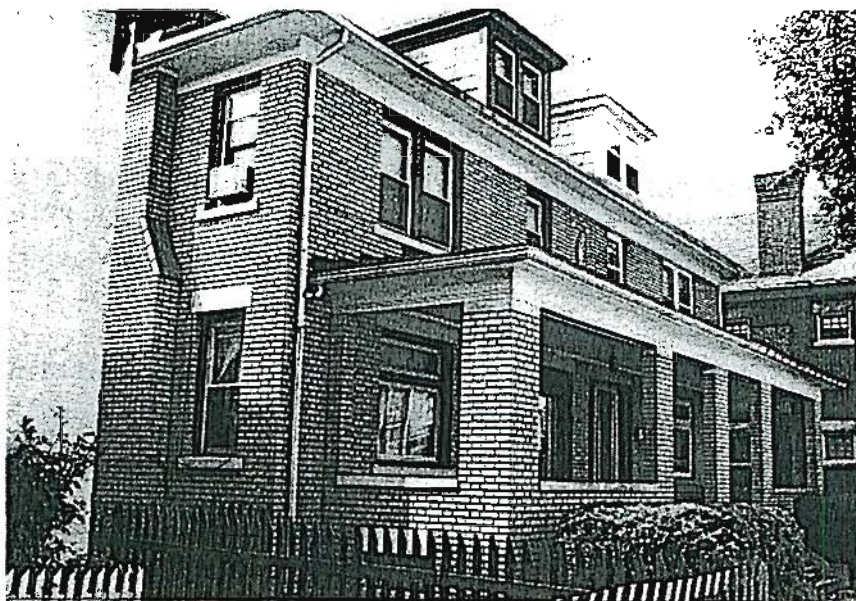
### Description

This property type includes doubles, apartment buildings without commercial spaces, and apartment buildings with commercial spaces. Though the owner of such a building might occupy one of the residential units, usually these were built solely as rental properties, with the owner living elsewhere.

Until about 1870, most residential construction in Steubenville was single-family homes. The rapid growth of industry in the city after then, however, created a large market for rental housing for industrial workers who either could not afford or who did not aspire to single-family home ownership.

Multi-family residential construction occurred randomly throughout the city's downtown residential areas, though over time it became concentrated along Fifth and Sixth Streets in the north end, and along Third through Sixth Streets in the south end. In general, more rental housing was built in the south end than in the north, but much of this was in the form of doubles which blended in well with single-family structures. Because it was the home of the city's most prominent people in the 1870-1910 period, North Fourth Street was given over almost entirely to single-family structures.

Nearly all of Steubenville's multi-family residential buildings were built of brick, but there was considerable variation in style and design. The earliest, which date from the 1870s, are Italianate. Doubles often were fully-developed examples of this style, with the vertical proportions, low-pitched roofs, bracketed cornices and ornamented window trim typical of the style.



A brick double typical of Steubenville's downtown neighborhoods.

Later doubles also tended to be fairly fully developed examples of styles current at the time they were built, the Queen Anne being the most common. Whatever the style or stylistic elements employed, doubles tended to share some common features: orientation of the short side to the street; use of front entrance porches; double-hung sash with 2/2 or 1/1 glazing; and close spacing on small lots.

There are also examples of doubles which only employed elements of Italianate or Queen Anne design and were not full-blown examples of either style.

Apartment houses could be found both in the rowhouse form and in the central entrance form (in the former, each apartment has its own street door; in the latter, there is a main entrance to the building and apartments are reached from interior corridors). Brick construction was nearly universal. As with doubles, apartment houses could be found throughout the downtown neighborhoods. These buildings generally employed features and elements of academic architectural styles but, because they were built as income-producing investments, their builders generally did not go to the expense of creating full-blown examples of the Italianate and Queen Anne styles. Stylistic details, of course, varied, but apartment houses did share some common features. They usually were two or three stories in height; those built in brick (the great majority) usually had stone trim features such as lintels, sills and beltcourses. Often these buildings were built on high foundations, which typically were built of rubble or of cut stone. In some cases entrance doors were reached by simple stone steps without any entrance roof or canopy; in other cases, porches of wood or of brick and stone sheltered the entrance doors. Generally, the rowhouses were more likely to have porches than the central entrance apartment houses.



King Frederick Apartments on North Fourth Street is an excellent example of an early twentieth century downtown apartment building.

A variant of the apartment house form had one or more commercial spaces on the first floor. Sometimes these spaces took up the entire first floor building front, with only a small side or center door leading to the apartments upstairs. In other cases, the commercial space might only occupy a corner or a portion of the first floor; there were no regular patterns to these commercial spaces, their construction apparently being governed by the commercial prospects of their particular locations. The most common use of these spaces appears to have been as small markets and similar enterprises serving the needs of the immediate neighborhood.

Storefronts built as part of apartment houses looked much like those built on purely commercial buildings. They employed large display windows of plate glass, entrance doors with large windows, a signboard, frieze or cornice to separate them from the rest of the building, and they often had retractable awnings. These storefront features tended to be fairly constant regardless of the style or design of the apartment house itself.



The former Floto Bakery retail store is compatible with its neighborhood in scale, design, materials, and details.

### Significance

Steubenville's multi-family structures are significant under Criteria A and C in the fields of ARCHITECTURE and COMMERCE.

Under Criterion A, multi-family residences may be significant as representing a trend in Steubenville demographics in the late 19th and early 20th centuries: the increasing numbers of blue-collar industrial workers, and lower-level white-collar workers, that came to the city as its industrial base expanded and grew. Seldom in a position to buy homes, these people frequently rented residential quarters, often in multi-family structures.

Multi-family Steubenville residential structures may also qualify under Criterion C because they represent certain types and methods of

construction. Those built in academic architectural styles may be significant for representing the use of those styles in an apartment building or rowhouse building form. Other structures may be significant because they represent a form or type of combined commercial/residential structure as used in Steubenville in the late 19th and early 20th centuries.

#### Registration Requirements

To qualify for listing under Criterion A, a multi-family residence should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain sufficient structural, finish and stylistic features to identify it with the 1870-1910 period. This includes retention of window and door patterns, sizes and trim; floor plans, overall massing and roof shape; and decorative or stylistic elements. Though some changes or missing elements will not necessarily keep the property from qualifying, it must retain essential character-defining features. Thus a rowhouse built with entrance porches or stoops should retain its original pattern of entrances, porches or stoops; a central entrance apartment house should retain its central entrance; structures with commercial spaces should retain intact commercial storefronts.

In order to qualify under Criterion C, a multi-family residence should have the following aspects of integrity:

##### 1. DESIGN

Refer to the comments above for Criterion A.

##### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. This should include exterior surface materials, window and door trim, decorative elements, chimneys, and porches if they were part of the original design or were added during the period of significance.

##### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. Multi-family residences should retain ornamentation, original or period light fixtures, door and window trim, stylistically-influenced doors and windows such as diamond-paned windows or paneled doors, decorative brickwork, and decorative porch or stoop elements. If the property originally had a street-level storefront, then elements such as the bulkhead, transom, display window, door and any associated ornamentation should survive.

##### 4. FEELING

The property's components, taken as a whole, should express a specific point or period in time between 1870 and 1910.

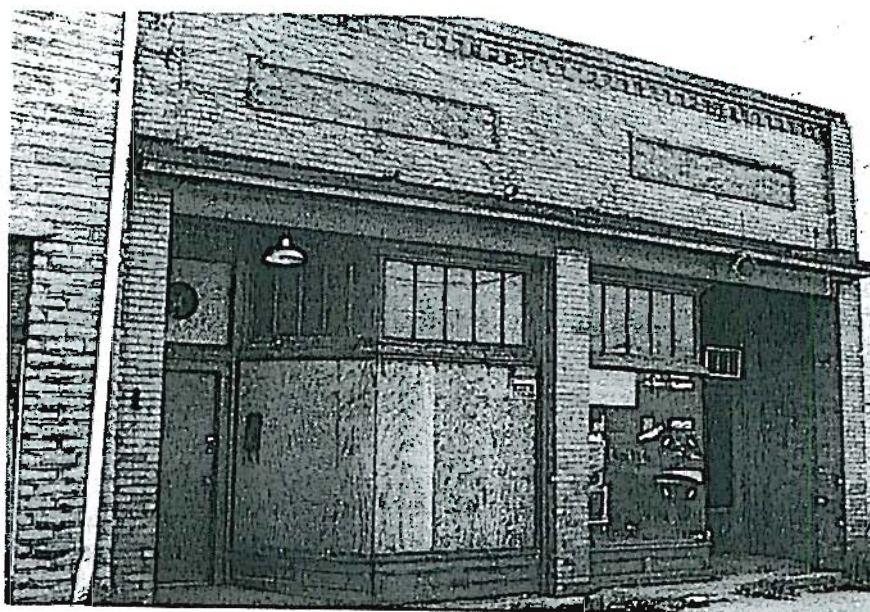
## Neighborhood-Scale Commercial Structures

### Description

This property type refers to commercial structures which were either built as free-standing structures or as additions to existing residential structures. In the cases where they were additions, the commercial structures could survive as free-standing buildings if the original structure were removed. Free-standing commercial structures were the most common.

As Steubenville's downtown neighborhoods grew and filled in with residential structures during the 19th century, there was a need for various commercial enterprises to serve neighborhood needs. These included groceries, produce and meat markets, dry goods stores, drug and sundry stores, and restaurants and taverns. Some enterprises combined two or more of these functions. They typically served an area of just a few surrounding blocks and often provided goods and services tailored to the local population. Many neighborhood commercial structures were built or later owned by foreign immigrants, and the businesses housed in them served immigrant or ethnic groups.

Whether built as free-standing structures or as additions to existing residential structures, neighborhood commercial buildings blended in with the neighborhood as much as possible: they were small, usually confined to a residential-size lot; their architecture was modest in scale and ornamentation; and they used architectural trim, details and elements

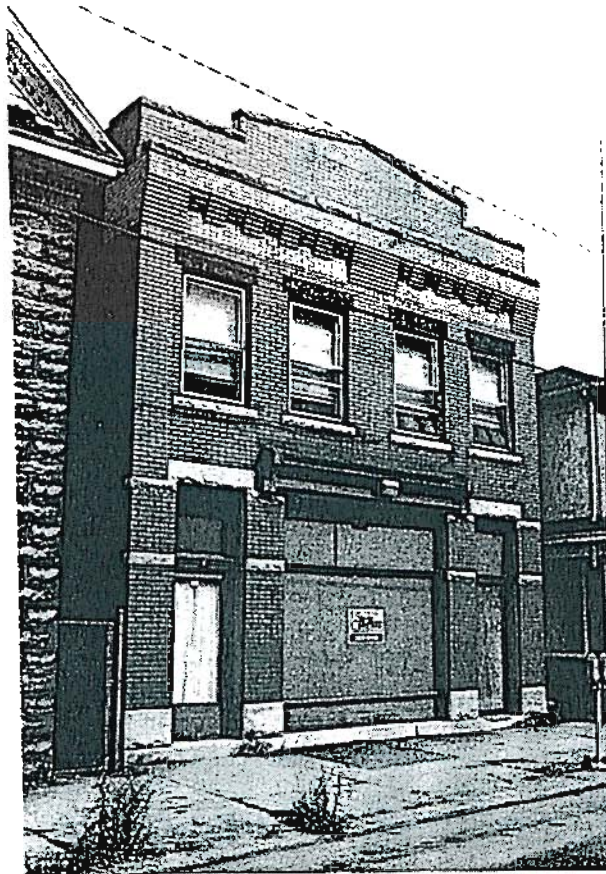


A small-scale commercial building typical of commercial activity in Steubenville neighborhoods.

similar to those used in nearby residential structures. Neighborhood commercial buildings often were built on corner lots rather than mid-block, to maximize visibility and the amount of traffic passing them.

Because neighborhoods had to grow to enough population density to support small commercial enterprises, neighborhood commercial structures were built later rather than earlier in a neighborhood's history; they tended to follow rather than lead neighborhood development. Thus the earliest commercial buildings in the downtown neighborhoods date from about the 1880s, well after these neighborhoods began to grow. These buildings were built well into the early 20th century.

In design these structures followed general architectural trends. Many were built in the Italianate style or with Italianate elements. Always featuring large storefront display windows (usually a single window with the entrance door to one side), they often had projecting bracketed cornices, and roofs usually were flat and sloping. Often the upper portion of the main elevation was carried into a false front or a parapet wall to increase the apparent height. Structures from around the turn of the century or the early 20th century often were very plain in design, with little applied ornamentation.



A good example of mixed-use commercial and residential structure on South Fourth Street.

Both one-story and two-story commercial structures were built. On two-story structures, upper-floor windows typically were of Italianate origin, with tall and narrow proportions and 2/2 or 1/1 double-hung sash. The upper floors were commonly used for apartments. Note the distinction, however, between this property type, whose principal function is as commercial space, and structures built primarily as apartment buildings with incidental commercial space (discussed in the preceding property type).

### Significance

The neighborhood-scale commercial structures of downtown Steubenville are significant under Criteria A and C in the fields of ARCHITECTURE, COMMERCE and ETHNIC/IMMIGRATION.

Construction of small commercial structures was a measure of the growth, maturity and stability of Steubenville's neighborhoods. Unless a builder or business owner had faith in the permanence and stability of the surrounding area, he was unlikely to risk a commercial venture. Thus the very existence of neighborhood commercial structures is a means of tracking neighborhood growth and development.

The specific businesses housed in these structures also said a great deal about the surrounding neighborhood. From about 1870 on, and especially between about 1890 and 1910, European immigration to Steubenville added significantly to the city's population. Attracted by readily available jobs in industries requiring little or no skill, immigrants from northern, southern and eastern Europe came steadily to the city, and they naturally tended to cluster in neighborhoods of people from their country of origin. Thus the existence of a market offering Hungarian foods, or of an agency providing transportation to America from Italian ports, could tell much about the ethnic background and socio-economic status of people in the surrounding neighborhood.

Under Criterion A, Steubenville's neighborhood commercial structures are significant as indicators, over time, of the growth, prosperity, stability and ethnic background of the city's downtown neighborhoods. As part of the fabric of the neighborhoods, these structures are components of the broad patterns of the city's historical development.

Under Criterion C, Steubenville's neighborhood commercial structures may qualify because they represent certain types and methods of construction. Many were built with all the features of the Italianate Commercial style, with storefront windows, transom windows, entrance doors with windows in them, a cornice or signboard above the storefront, and a bracketed cornice at the top of the building wall. If not an example of an academic architectural style, a neighborhood commercial structure may still be significant as a building type, a specialized structure built to perform a specific function in a residential area without disrupting the area's character.

### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

## 1. LOCATION

Refer to the discussion of location in the introduction.

## 2. DESIGN

The property should retain sufficient structural, finish and stylistic features to identify it as a neighborhood-scale commercial structure from the 1870-1910 period. This includes retention of such features as original or historic storefronts (bulkhead, display windows, transoms, signboards, doors and doorways, and associated ornamentation); beltcourse or intermediate cornices; upper-floor window openings or projecting bays and associated trim or ornamentation; exterior surface materials; and bracketed cornices or other terminating features at the top of the main elevation, including associated ornamentation or trim.

To qualify for listing under Criterion C, a property should have the following aspects of integrity:

### 1. DESIGN

Refer to the comments above for Criterion A.

### 2. MATERIALS

A property should retain a majority of the historic materials of which it was built. These include materials comprising the commercial storefront(s) and entrance door(s); upper-floor wall materials including window openings and trim; and original decorative features such as bracketed cornices, false fronts or parapets. Alterations will not disqualify the property as long as the character-defining features and original materials remain substantially intact.

### 3. WORKMANSHIP

The property should retain the major items of workmanship that make it an example of the time and place in which it was constructed. Neighborhood commercial buildings should retain, for example, decorative wood, stone or iron components of storefronts; leaded, prism or decorative glass windows or transoms; paneled or ornamented entrance doors; or decorative or trim elements in signboards, beltcourses or cornices. On the interior, wooden floors and decorative pressed metal ceilings should survive as important elements of workmanship.

### 4. FEELING

The totality of a property's components should express a specific point or span of time in the period 1870-1910. This results from a majority of the property's elements of design, materials and workmanship remaining intact. The specific components of neighborhood commercial buildings that contribute to these aspects of integrity are identified above.

## Churches and Rectories/Convents

### Description

This property type was a component of Steubenville's downtown neighborhoods from their earliest years. Houses of worship, and associated facilities, were as essential to neighborhood residents as places to work and shop, and a relatively small neighborhood area might support a surprising number of churches.

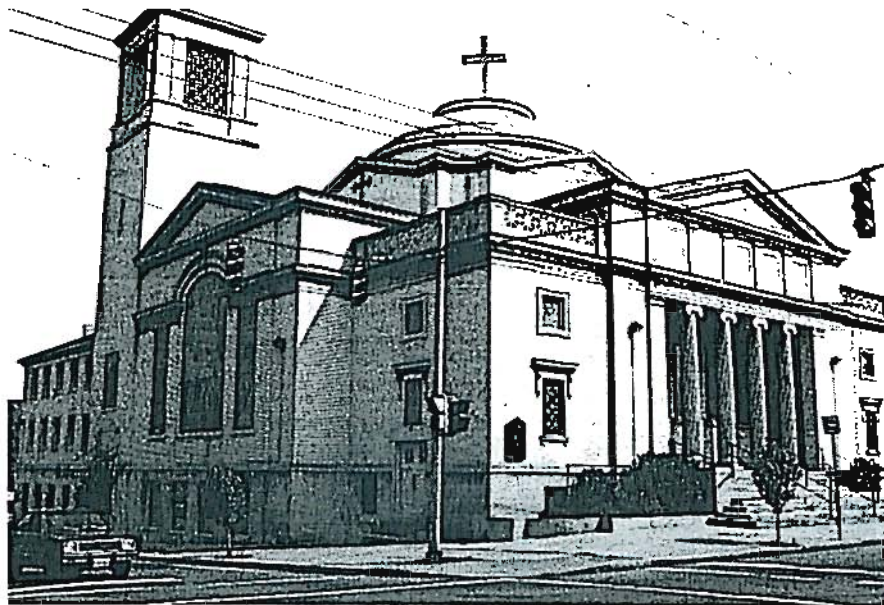
Steubenville churches were often established by defined ethnic groups, so the history of church denominations and the evolution of church architecture tended to track very closely the arrival and establishment of various ethnic groups. The general sequence in Steubenville was that the earliest immigrants were from other states such as Pennsylvania and West Virginia, so they brought with them such established denominations as Roman Catholic and Protestant. Early European immigrants were English and Welsh, which reinforced the presence of Protestant denominations in the city. When eastern and southern European immigrants began arriving in large numbers toward the end of the 19th century, the Roman Catholic Church gained much greater prominence, and various Orthodox denominations such as Hungarian, Greek and Serbian became established. This trend continued into the early 20th century with continued ethnic group immigration.

The designs of churches built by the various denominations were affected both by architectural trends of the time and by specific customs and liturgical requirements, as well as the financial condition of specific



Holy Name Cathedral, Rectory and School are typical of complexes developed by Catholic Churches in Steubenville.

congregations. Thus Steubenville's neighborhoods were home both to churches built in academic architectural styles, and to those built to accommodate specific needs or which reflected religious architecture of their country of origin. In some instances both influences played a role, resulting in high-style designs adapted to fit specific needs. A Neo-Classical Revival style Greek Orthodox church, for example, might combine a classical colonnaded entrance with a traditional Eastern dome. Most church construction was in brick, stone and terra cotta, though several smaller buildings were of frame construction. In general, the wealthier congregations built high-style structures and the less well-off congregations built simpler vernacular structures.



An excellent example of an early twentieth century downtown church. Steubenville still has a fine collection of historic churches located downtown.

As congregations grew and their fortunes improved, there often was considerable re-building of older churches and replacement of older structures with new buildings. Thus most downtown Steubenville churches have datestones which cite the date of the congregation's establishment and the dates of earlier church buildings. Virtually all existing church buildings date from the late 19th century or the early 20th century. Architectural styles include Gothic Revival, Romanesque Revival, Late Gothic Revival, High Victorian (both Italianate and Gothic), and Neo-Classical Revival.

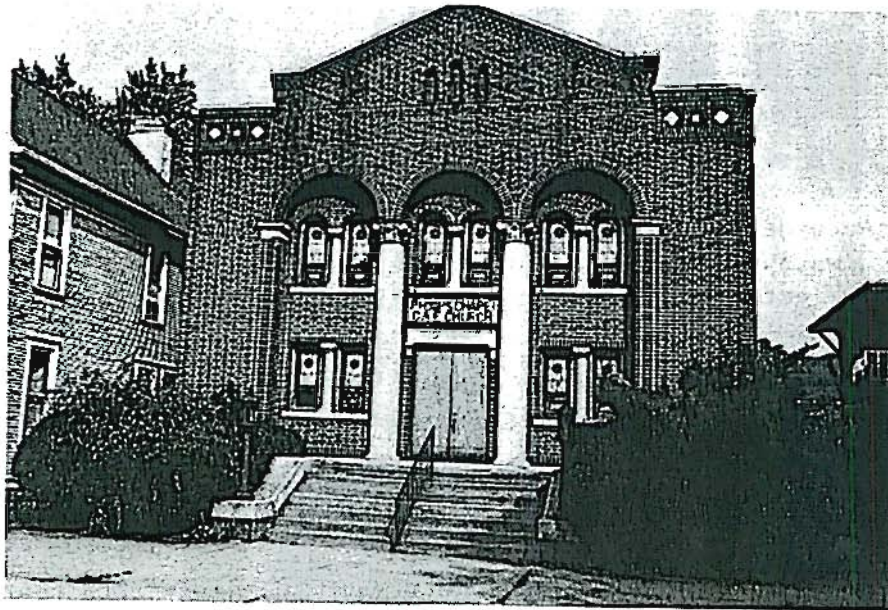
Artistic merit may be an important component of a church's significance. Often, as in the case of mosaic or painted murals that are an integral part of a building such as a church, the architecture and art cannot be separated. Thus while a property may not be especially representative of a style or design of architecture, its artistic features may give it a level of significance warranting registration.

Associated with the church buildings were rectories and/or convents to house personnel associated with the church's operations. Sometimes these were nothing more than an adjacent house purchased by the congregation, and other times a structure was built by and for the church. Often, when rectories and convents were built by the congregation, their designs reflected the designs of the church building. They usually were located close to the church and sometimes were physically attached.

#### Significance

Steubenville's neighborhood churches may be considered significant under Criteria A, B or C. These buildings may have strong associations with the development of Steubenville and its neighborhoods; or with immigration/ethnic history; or with specific important individuals. They may also be significant for their architecture or for the artistic merit of their designs or decorative treatments.

As was true in the construction of individual residences, the congregations of Steubenville's churches sought to express their growth, success and community position through the architecture of their church buildings. While it is true that the architectural design of churches is in part intended to have an effect upon members of the congregation, it is also true that these designs were intended to impress outsiders as well. Thus Steubenville churches tended to be as large, ornate, finely wrought, and substantial as each congregation could afford. Execution in the latest architectural style was part of this effort.



Phillips Chapel is a good example of a small-scale but dignified neighborhood church.

#### Registration Requirements

To qualify for listing under Criteria A or B, a property should have the following aspects of integrity:

## 1. LOCATION

Refer to the discussion of location in the introduction.

## 2. DESIGN

If a high-style building, the property should have its stylistic features intact. For example, a church built in the Gothic Revival style should retain its steeply-pitched gable roof and pointed-arch windows, as well as any ornamentation or details such as decorative bargeboards. The basic form of the building should remain unchanged, as should major window and door openings. Important design features such as towers and steeples should survive, as should the original plan, whether rectangular, cruciform or some other shape.

If a vernacular building, the property should retain its basic form, roof shape, window and door openings, and interior plan.

## 3. ASSOCIATION

The property should be sufficiently intact in the aspects of location and design to convey the historical relationship between an event or person and the property. A parishioner or clergyman returning from the past should be able to recognize the property today.

In order to qualify under Criterion C, a property should have the following aspects of integrity:

### 1. DESIGN

Refer to the comments above for Criteria A and B.

### 2. MATERIALS

A majority of the historic materials of which the property was built should be substantially intact. Thus, elements such as windows and doors must be in their original sizes and locations, and important decorative treatments such as stained glass must be intact. The basic form and massing of the property must not be obscured by later additions, and the original principal facade should be intact. On the interior, the historic or traditional circulation patterns and floor plans must be substantially intact.

### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was built. For example, a church or religious property should retain any works of art of recognized merit such as murals, stained glass or woodcarving. These works should be an integral part of the architecture of the property and should survive with minimal alteration or damage. Exterior items of workmanship that should survive include exterior woodwork in doors, windows, or ornamentation; decorative brickwork; and cut or carved stone.

### 4. FEELING

The totality of a property's components, whether the building is vernacular or built in an architectural style, should express a specific point or period in time.

## Schools

### Description

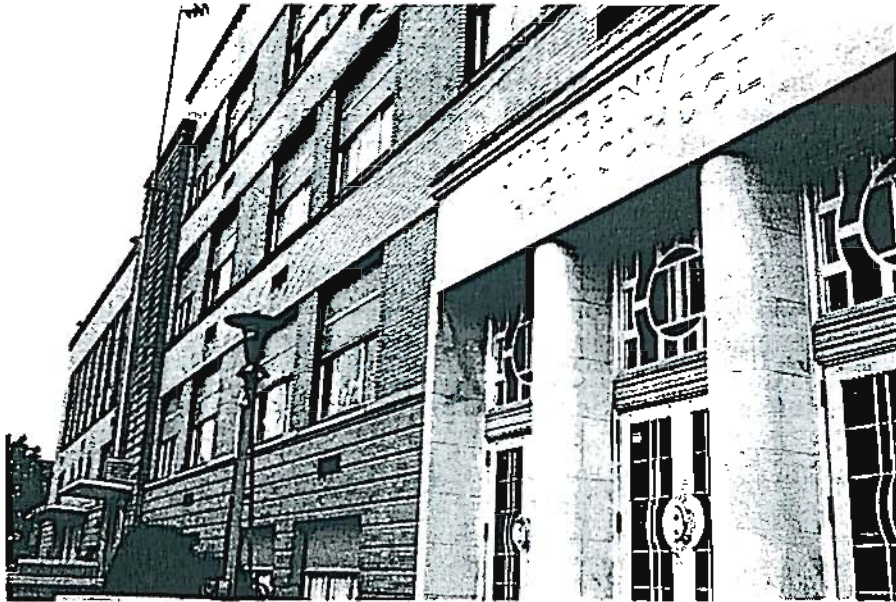
Downtown Steubenville schools from the 1870-1910 period generally were built to serve a city-wide population, rather than just a particular neighborhood. Included in this type were church-affiliated schools, which might be built by specific congregations or parishes but which usually drew students from all over the city.

As a result, school buildings generally were large, often occupying half a city block or more. Because they symbolized the community's highest values and aspirations, they typically were built substantially and well, in the architectural styles popular at the time of construction. Generally the older school buildings were full-blown examples of styles such as Italianate and Romanesque Revival. Italianate structures had strongly vertical proportions, with tall, narrow windows; ornamented trim at windows and doors; low-pitched pyramidal or hip roofs; and bracketed cornices. Features of Romanesque Revival style schools included round-arched openings with substantial, heavy trim elements; highly visible steeply-pitched gable, hip or pyramidal roofs; and an overall feeling of massiveness. All of Steubenville's downtown schools were built of masonry, employing brick, stone and terra cotta in various combinations.



The former Grant Junior High School has finely detailed decorative elements of English inspiration.

The newer school buildings, from the early 20th century, tended not to be done in full-blown architectural styles, but they employed elements of various styles in successful compositions that imparted a sense of permanence and substance. Classical elements such as columns, friezes and pediments might be employed, or features from Tudor Revival architecture such as half-timbering and casement windows. Like the earlier schools, these structures had common features such as masonry construction, large classroom windows, and prominent main entrances.



Steubenville High School (Big Red) is an outstanding example of Art Deco architecture that is a true landmark in its neighborhood.

### Significance

Steubenville's downtown schools are significant under Criteria A and C in the fields of ARCHITECTURE and EDUCATION. As manifestations of the values and aspirations of the community, these schools are a measure of Steubenville's growth, development and self-image over time. As such, they are indicators of patterns in the city's development. From the locations as well as the construction dates and the physical designs of the schools, it is possible to trace where development was occurring and during what time periods it was occurring. Schools may have significance through association with a significant educational theory or event; because they represent a trend in school design reflecting ideas about education and safety; or because they are landmark schools in the city's educational history.

Downtown Steubenville schools may also qualify under Criterion C because they represent certain types and methods of construction. They may have significance as examples of school design theory at various points in time, and they may have additional significance as good examples of architectural styles.

## Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

### 1. LOCATION

Refer to the discussion of location in the introduction.

### 2. DESIGN

The property should retain sufficient structural, finish and stylistic features to identify it with the 1870-1910 period. This includes retention of original form and massing, roof shape, window and door openings and architectural details; interior room arrangements and circulation patterns likewise should remain substantially unaltered. Additions and alterations will not necessarily keep a property from qualifying, as long as character-defining features remain substantially intact. Because changes to windows are a common occurrence in school buildings, the existence of replacement windows will not necessarily keep a property from qualifying, as long as the window openings have not been permanently altered, and as long as other character-defining features such as ornamentation and stylistic elements remain substantially intact.

In order to qualify for listing under Criterion C, a property should have the following aspects of integrity:

### 1. DESIGN

Refer to the comments above for Criterion A.

### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. This should include exterior surface materials, window and door opening trim, decorative or stylistic elements, chimneys, and entrance stoops, porches or canopies. Additions made during the period of significance should also be retained.

### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For schools, these items include patterned brickwork and cut or carved stone on the exterior, as well as ornamental or stylistic exterior features such as Italianate cornice brackets. Interior items of workmanship include wooden floors, slate chalkboards, trophy cases, and decorative or stylistic features such as molded plaster, cut or carved stone, or ceramic tile on walls or floor.

### 4. FEELING

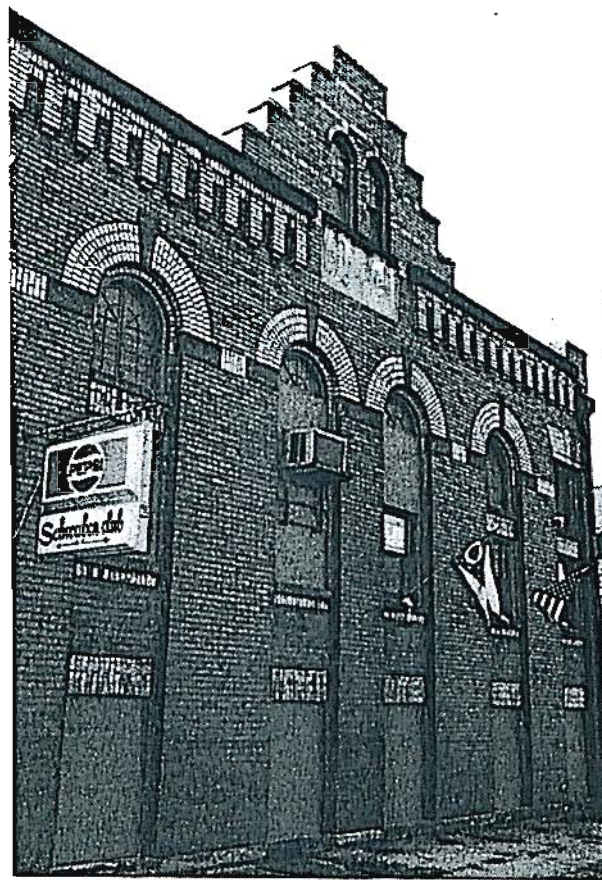
The property's components, taken as a whole, should express a specific point or period in time between 1870 and 1910.

## Social Clubs

### Description

This property type, as found in downtown Steubenville, was an extension or expression of the ethnic background of neighborhood residents. Virtually all social clubs were associated with specific ethnic groups: Italian, German, Polish, Welsh, Irish, and Greek clubs existed in the city.

The clubs' purpose generally was to provide a sense of fellowship among people newly arrived in Steubenville and the United States. They served both to help these people's assimilation into the new culture, and to help them hold onto cultural traditions from their countries of origin. They eased the transition to a new life.



The Schwaben Club is an excellent surviving example of an ethnic social club in Steubenville.

Many social clubs were content to stay in rented storefronts or upper-floor meeting rooms, while others aspired to having their own buildings. In the case of separate social club buildings, there were no set patterns or designs, beyond such common characteristics as large open meeting rooms and kitchen facilities. These structures also were not necessarily located in the ethnic neighborhoods they served. Land cost and availability appears to have had as much influence on location as convenience to club members' homes.

Thus the architectural designs of social clubs varied considerably depending on budget, location and spatial needs. Designs naturally were influenced by architectural trends and tastes of the times, so social club buildings in some cases were built in specific styles, and in others drew only some elements from one or more styles. Construction could be in masonry or frame, with brick the generally favored material.

### Significance

The downtown social clubs of Steubenville are significant under Criteria A, B or C in the fields of ARCHITECTURE and ETHNIC/IMMIGRATION. These structures were physical manifestations of the efforts by ethnic groups to adapt to life in a new country, symbolizing both their desire to assimilate and their desire to retain at least some of the customs, values and relationships of their native lands.

Under Criterion A, Steubenville's downtown social clubs are significant as part of the broad patterns of the city's development. These clubs were an important part of the lives of the city's immigrants, who in turn were an important part of the city's industrial growth and success (the context "Ethnic Diversity in Steubenville" discusses most of these same property types as part of the city's ethnic history). Social clubs are a part of understanding a major component of Steubenville's history. Though they might have some ties to late 19th and early 20th century social reform movements, the primary impetus for formation of the clubs was the immigrants' need for companionship in a strange land: they sought the company of people who were like them, and a setting where they did not feel under the scrutiny of sometimes-hostile outsiders.

Under Criterion B, Steubenville's social clubs may be significant for association with persons important in the city's history. Many of the immigrants who went into business in Steubenville and joined its social clubs later became major industrial, business and political figures.

Under Criterion C, Steubenville's downtown social clubs may also qualify because they represent certain types and methods of construction. They may qualify as good examples of academic architectural styles, but even in the absence of stylistic associations they may have significance as representing social club construction as carried out in Steubenville in the late 19th and early 20th centuries.

### Registration Requirements

To qualify for listing under Criteria A or B, a property should have the following aspects of integrity:

#### 1. LOCATION

Refer to the discussion of location in the introduction.

#### 2. DESIGN

If a high-style building, or one in an eclectic mix of stylistic elements, the property should have its stylistic features intact. These include original form and massing, roof shape, window and door openings, and architectural detailing; interior room arrangements and circulation

patterns from the property's period as a social club likewise should remain substantially unaltered. Additions and alterations will not necessarily keep a property from qualifying, as long as the character-defining features defined above remain substantially intact.

### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between an important person and the property. For example, the Schwaben Club building is a German social club dating from the early 20th century. Because it has survived in its historic form, with design elements such as its stepped gables and exterior brickwork intact, a club member from the early 20th century would recognize the building today.

To qualify under Criterion C, a property should have the following aspects of integrity:

#### 1. DESIGN

Refer to the comments above for Criteria A and B.

#### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. In high-style or eclectic structures, this should include the original exterior surface materials, window and door trim, decorative elements, porch elements, and chimneys. For vernacular buildings, retention of original materials may be less critical, as long as the vernacular building traditions or techniques represented by the structure remain apparent. In this case, the basic form, plan, massing and roof shape should survive, as should the original pattern of window and door openings. Changes such as addition of artificial siding or removal of a porch or chimney will not necessarily keep a property from qualifying.

#### 3. WORKMANSHIP

The property should retain major items of workmanship that make it an example of the time and place in which it was constructed. For social clubs, this includes any decorative elements craftsmanship which represent the ethnic or social group of the club. These might include murals or carved woodwork executed by or for club members, or artifacts that have been incorporated into the building. Other items of workmanship include decorative exterior brickwork; wood siding or decorative elements; ornamentation such as brackets, columns, or eave details; window and door trim; and interior details such as doors, doorways, and ornamental plaster.

#### 4. FEELING

The totality of a property's components, whether high-style, eclectic or vernacular in design, should express a specific point or period in the 1870-1910 era.

## Institutions

### Description

Though not numerous, institutions such as social welfare organizations and homes for the elderly were part of the fabric of Steubenville's downtown neighborhoods. Founded by individual philanthropists or by religiously-affiliated agencies, these institutions were established to meet specific community needs.

The structures occupied by such institutions might vary from commercial storefront space to converted single-family homes; or the institution might build its own facility specifically designed for its needs. In every case, these structures blended in as much as possible with adjacent and nearby residential properties in scale, materials, massing, detailing, and setback. They attempted to fulfill their roles without disruption of the character of the neighborhood, and typically they occupied only a single residential lot.

Stylistic treatment followed the general architectural trends of the surrounding neighborhood. In some cases institutional structures were built in architectural styles, most commonly the Italianate. Typical of Italianate buildings, they employed tall, narrow windows, ornamented window and door trim, low-pitched roofs and bracketed cornices.



Martha Manor on North Fifth Street is an example of an institutional building designed to fit well in a residential neighborhood.

### Significance

The institutional structures in Steubenville's downtown neighborhoods may be significant under Criteria A and C in the fields of ARCHITECTURE and SOCIAL/WELFARE. As manifestations of a concern for the city's citizens,

these structures are a component of Steubenville's broad patterns of development. They represent a community that had achieved sufficient wealth and stability to support philanthropic endeavors and thus are indicators of the level of maturity Steubenville had reached by the late 19th century.

Institutional structures may have significance under Criterion A if they can be shown to have housed institutions that played a major role in the historical development of Steubenville, during the time that contribution was made.

Downtown institutional structures may also qualify under Criterion C because they represent certain types and methods of construction. They may have significance as examples of institutional architecture at given points in time, and they may have additional significance as good examples of academic architectural styles.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

If a high-style building, the property should have its stylistic features intact. For example, an Italianate building should have its boxy, rectangular form, vertically-proportioned openings, shallow-pitched roof, projecting bracketed eaves, and other decorative detailing such as hoodmolds or decorated lintels; a Queen Anne building should have its irregular massing, variety of roof surfaces and exterior materials, and varied window and door sizes and placement; a Colonial Revival building should have its symmetrical plan, colonnaded entrance, and classical cornice with dentils and frieze.

If a vernacular building, the property should retain its basic form, roof shape, window and door openings, and interior plan. Original porches should be intact, as should elements such as chimneys and entrance stoops or canopies. Loss of some of these elements will not necessarily keep a property from qualifying if most of the major design features are intact.

To qualify under Criterion C, a property should have the following aspects of integrity:

##### 1. DESIGN

Refer to the comments above for Criteria A and B.

##### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. In high-style structures, this should include original exterior surface materials, window and door trim, decorative elements, any porch

elements, and chimneys. Integrity of materials includes, for such buildings, such stylistic elements as hoodmolds and cornice brackets for Italianate buildings; varied siding and ornamental brickwork in chimneys for Queen Anne buildings; and dentils, fluted columns, and multi-paned windows for Colonial Revival buildings.

For vernacular buildings, retention of original materials is somewhat less critical, as long as the vernacular building traditions or techniques represented by the structure remain apparent. For example, as long as the basic form, plan, window and door openings, and roof shape remain intact, then loss of a porch will not necessarily keep a property from qualifying.

### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For buildings built in architectural styles, this should include, for example, hoodmolds, cornice brackets, and window and door trim for Italianate buildings; and patterned brickwork, varied siding materials, and decorative features for Queen Anne buildings. For vernacular buildings, items of workmanship should include simple door and window trim, any decorative features, and any surviving porches, stoops or canopies.

### 4. FEELING

The totality of a property's components, whether the building is vernacular or built in an architectural style, should express a specific point or period in time.

## Production/Retail Facilities

### Description

Though they tended not to occupy the principal residential streets, small factories or production facilities -- some with retail storefronts -- could be found in Steubenville's downtown neighborhoods. These structures generally were small in scale and housed light industrial activities such as bakeries, breweries, machine shops and small stone-cutting works. (Reference should also be made to the context "Industrial Development in Steubenville, 1847-1959.")

Brick was the common material for these structures, and they usually were built with minimal architectural ornament or detailing. They typically were not built in academic styles. If the product lent itself to retail sale, then sometimes the building had a conventional street-frontage storefront at the front of the first floor, with the production facilities confined to the rear. These structures could be one story or two in height. If two stories high, they typically had either offices or an apartment on the upper floor, and possibly additional storage or production space.



The former Floto Bakery production facility is at the rear of the commercial block containing the retail storefront.

### Significance

Neighborhood production/retail facilities are significant under Criteria A and C in the fields of ARCHITECTURE, COMMERCE and INDUSTRY. As noted above, the production activities housed here generally were small in scale and purposely tried to blend into the neighborhood without disrupting its residential character. These facilities were important to their

neighborhoods, principally as a source of employment and sometimes as retail outlets serving neighborhood needs. An example is the Floto Brothers' Bakery (no longer operating) in the north end, which combined a modest-sized commercial bakery at the rear with a commercial storefront along the street for the retail sale of bakery products. Its materials, massing, scale and color blend well with the neighborhood's overall character.

Under Criterion A, these properties may be significant as part of the broad patterns of Steubenville's development. This is true when such properties provided employment opportunities in their neighborhoods; when they produced and sometimes sold products important in their neighborhoods (for example, a bakery); or when they produced a product important in one of the city's large industries (for example, a small machine shop turning out components of products finished or assembled in one of the city's large factories).

Under Criterion C, in order to qualify for listing a property must be representative of a type or method of construction. Since virtually none of these properties was built in an academic architectural style, to qualify under Criterion C a property must be an example of neighborhood-scale production/retail architecture as built in Steubenville between 1870 and 1910.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

These buildings tended to be plain and utilitarian in character, with little stylistic treatment; none could be considered an example of an architectural style. To achieve integrity of design, then, a property should retain its basic form, roof shape, window and door openings, and interior plan. Storefronts, if any, should be intact, as should elements such as chimneys and entrance stoops or canopies. Loss of some of these elements will not necessarily keep a property from qualifying if most of the major design features are intact, although intact storefronts, if they were originally part of the design, are critical elements.

To qualify under Criterion C, a property should have the following aspects of integrity:

##### 1. DESIGN

Refer to the comments above for Criteria A and B.

##### 2. MATERIALS

The property should retain a majority of the historic materials of which it

was built. This should include original exterior surface materials, window and door trim, decorative elements, storefronts, any porch elements, and chimneys.

### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. This includes items such as storefront elements, including bulkhead, display window, transom, entrance door, and any associated ornamentation; door and window trim elsewhere on the building; any decorative features; and any surviving porches, stoops or canopies.

Because of the evolution of the enterprises that occupied these properties, it is not common for the fixtures and equipment of any particular production process to survive for long. The smaller businesses that occupied these properties often had limited lifespans, so their production fixtures and equipment frequently was sold or scrapped. Thus a lack of surviving fixtures and equipment will not keep a property from qualifying, as long its character-defining architectural features have survived substantially intact. Should fixtures and equipment have survived, then they would be particularly important items of workmanship.

### 4. FEELING

The totality of a property's components, whether the building is vernacular or built in an architectural style, should express a specific point or period in time.

## **DOWNTOWN AS STEUBENVILLE'S GOVERNMENTAL, COMMERCIAL, AND SOCIAL CENTER, 1870 - 1950**

As the following narrative illustrates, the downtown core of Steubenville from its earliest days has been the center of government, commercial and social activity in the community. Shaped by the same natural and man-made factors discussed in the preceding context narrative, the downtown commercial area measures about four by five blocks. This area is bounded by North, South, Third and Seventh Streets. In Steubenville's earliest days, as already noted, commercial development was mixed in with residential uses and industrial enterprises, and this development was confined to one or two streets closest to and parallel to the Ohio River (an area today outside the downtown core, cut off by highway and industrial development). After transportation opportunities improved with the arrival of the railroads in the 1850s, the downtown focus shifted away from the river, up Market Street to its intersection with Fourth. As early as the mid-1850s, this intersection was Steubenville's "100% corner" (that is, its most desirable business location), and it remains so today. The Steubenville Commercial Historic District, consisting of a portion of the downtown core area, was listed in the National Register of Historic Places in 1986.

Diffusion of downtown core development away from the river was accompanied by a gradual separation of land uses into industrial, commercial and residential areas. This division among uses was never as distinct and rigid as, for example, modern zoning codes might dictate, and even today there are examples of dissimilar uses side by side. Nonetheless, there was a naturally-occurring division of land uses that was largely established by 1870. In general, the far north and far south ends of the city were given over to industrial uses, as were many of the city blocks immediately to either side of the two main railroad routes. In the middle of it all were the twenty square blocks of the downtown core, where most residential and industrial uses had been squeezed out by commercial development, which tended to make land values too high for these other uses. Government functions, because of their need to be in the heart of the city, also were generally confined to the downtown core. At the core area's fringes, before the start of the mainly residential north end and south end neighborhoods, there were many institutional land uses, such as the library, churches, social and welfare organizations, and schools. Beyond the downtown core and the north and south end neighborhoods, there was generally residential development of the lower slopes of the hills west of the city. Streets such as Seventh, Eighth, Ninth and Tenth, as well as many small, short, steep cross streets, provided access to hillside lots which had excellent views of the city and river, but where construction and access often were very difficult. Some early residences were built on these slopes to take advantage of the views, when these sites were largely rural in character. Some of these remain today, intermingled with the late 19th and early 20th century houses that have grown up around them.

### **Governmental**

Concurrent with the founding of Steubenville, in 1797 Jefferson County was made the fourth county in the Northwest Territory, with Steubenville as its county seat. The county's boundaries extended north to Lake Erie and west to the Cuyahoga River, later to be cut back as other counties were

established. In 1800 a federal land office was established in a former private residence to sell public land in the Seven Ranges. In 1840 the land office closed, since all the public lands in Jefferson County had been sold. Moved several times from its original North Third Street site, the land office survives today on South Third Street, north of Adams, in a park setting near the site of Fort Steuben.

By 1809 the first crude wooden courthouse (built about 1798, when the site was acquired) had been demolished and construction of a new brick courthouse had begun; a jail was also built at this time. A new stone jail was built in 1834, but no other changes were made until 1870, when the county buildings were demolished. The new courthouse was built in 1874, located at the northwest corner of East Market and North Third Streets, and the sheriff's residence and jail were in separate structures behind it; the sheriff's residence fronted onto Third Street. The courthouse was the tallest building in the city and had a distinctive tower above an open colonnade, unusual in Ohio courthouse design. Extensively altered by removal of the tower and addition of a new top floor in the 1940s, the courthouse remains in operation today. The jail has not survived.

In 1850 the population of Steubenville reached 6,000, and the town was incorporated as a city. In the early 1880s a new city hall was built directly opposite the courthouse, on the south side of Market, west of South Third. This building was used until 1922, when the city market house just to the south on South Third was converted for use as a city hall, a use it still serves. The old city hall was demolished and the Cohen-Reiner Block, a commercial building, was built on its site in 1929. Today that building serves as a city administration building, a function it has served since the mid-1930s. Two downtown fire stations from the early 20th century still survive. One is the North End Fire Station, formerly Reliance Station No. 2, at 417 North Street; the other is former Reliance No. 1 at 518 South Street, still city-owned but leased to various private entities. Volunteer fire companies in the city dated to 1822, and the city's paid fire department was organized in 1886.

The federal government's presence in Steubenville since the days of the land office has been principally represented by the post office. In the late 19th century this facility was at the southwest corner of Fourth and Market Streets. There was much talk of a new post office early in the 20th century, but one was not built until about 1925, at the northeast corner of Washington and North Fourth Streets. It served until replaced in the late 1980s by a new post facility on South Third Street.

## **Commercial**

Commercial activity began shortly after Steubenville was platted in 1797. Among the earliest was a boarding house at Third and High Streets operated by John Ward, who built the U.S. Hotel in 1800. Steubenville was the largest settlement in Jefferson County at that time, and by 1817 it had a wide variety of commercial enterprises. Included among them were 16 taverns, two banks, four cabinetmakers, five tailors, three bakers, eight shoe and bootmakers, three hatters, a silversmith, three clock and watch makers, six butchers, a printing office, 27 stores, six lawyers and five doctors.

Steubenville's long history in banking began with the Bank of Steubenville, founded by Bezaleel Wells and William Dickinson in 1809. It closed in 1821. The Farmers' and Mechanics' Bank was founded in 1816. The Mechanics Fund Association was incorporated in 1832 and was purchased by Robert Sherrard in the 1850s and operated as R. Sherrard & Co. It was converted in 1865 to the First National Bank and in 1868 became the Commercial Bank of Sherrard, Mooney & Co. The Union Savings Institution was founded in 1854 and became the Union Deposit Bank in 1873 and later the Union Savings Bank & Trust Co. Farmers and Mechanics Bank closed its books in 1863 and the Miners and Mechanics Bank was organized in 1872. J.J. Gill organized the National Exchange Bank in 1873.

The Jefferson National Bank was founded in 1885 and was purchased by the National Exchange Bank (est. 1873) in 1902. A new headquarters building was erected at 428 Market Street in 1904. The Steubenville Building & Loan Co. was formed in 1888 and the Jefferson Building & Savings Co. in 1896. The Commercial Bank of Sherrard, Mooney & Co. became the Commercial National Bank also in 1896. People's National Bank was organized in 1905. The influx of immigrants led to the formation of foreign banks during this period. Vincent Morelli came to Steubenville in 1901, from Italy, and formed the first "foreign bank" in the city. These were foreign in the sense of serving foreign-born immigrants but were not foreign-owned. Such banks appear to have been associated only with the city's Italians and were not a major aspect of banking in Steubenville. Morelli was also a steamship agent and handled passage for immigrants and their families. Two of the major banks moved into skyscrapers built during the 1910's: the Sinclair Building (dedicated in 1915), built by Dohrman J. Sinclair, housed the Union Savings Bank & Trust Co., of which Sinclair was president. The Steubenville Bank & Trust Building, across from the Sinclair Building, was opened in 1919. Both buildings still house banks today.

Commercial activity thrived in the period up to 1870, when Steubenville's population grew from about 1,000 early in the century to over 8,000 by 1870. The McGowan Brothers operated one of the most successful businesses of the period. Founded by David McGowan in 1838, it was a retail and wholesale grocery which continued operation until the mid-20th century and involved four generations of McGowans. Another important business was the Beall & Steele Co., founded by druggists W. M. Beall and Robert McGowan in 1875. When C. H. Steele purchased McGowan's interest in 1878 it was renamed Beall & Steele. Other enterprises from this era included Jones Munker's Outfitters (1847), William Scott's General Store (1847), August Floto Shoe Store (1856), August Falk Bakery (1865), Crumrine and Nichol Book Store (1872), and Gottman & Sons Tailors (1874).

Several large scale retailers were established during this period. I. Sulzbacher opened a notion store in 1888, and by the turn of the century it had developed into a large department store. The store moved in 1904 into the new National Exchange Bank Building on Market Street. The Hub, which was to become the major department store in Steubenville during the first six decades of the 20th century, was opened in 1904 as a small men's store. It expanded gradually until a new three-story building was constructed at Fifth and Market Streets in 1924. It was long a downtown Steubenville landmark and the city's most significant example of a single-purpose commercial building. Unfortunately, because of its extensively deteriorated condition, the Hub building was scheduled for demolition during the summer of 1993. Other stores which lasted for many years

included Cooper Kline (1915) and Denmark's (1910). National retail chain stores also made their first appearances in Steubenville during this period. The J.C. Penney opened a store at 124 South Fourth Street in 1926, and the S.S. Kresge Co. opened its store at 438 Market Street in 1928.

Another major commercial development in Steubenville during this period was the formation of the Fort Steuben Hotel Company, with local stockholders, and the construction of the Fort Steuben Hotel in 1920. Other downtown hotels were also operating during this period, including the Imperial, Plaza and Miller's Hotels on Sixth Street across from the railroad depot.

Development of auto-related commercial activities began during the early 20th century, when auto sales rooms and service and gas stations began to appear. The Pietro DiNovo Co. was founded during the 1920's and is still located downtown on North Third Street. Others included the Stanton Motor Car Co. and the Fellows Motor Co.

For the two decades from the late 1920s to the end of World War II, the central business district of Steubenville continued to thrive as the regional shopping center for the Ohio Valley between East Liverpool and Wheeling. From the late 1940s on, a combination of factors -- affordable residential development on virgin hilltop land to the west, widespread automobile ownership, and public street and highway improvements -- led to a diffusion of commercial activity out of the downtown and into the western suburbs. Today, beginning at the top of the Washington Street hill, U.S. 22 westward out of Steubenville is a continuous commercial strip for several miles, and the population it serves lives nearby. After about 1950 Steubenville shoppers seemed to find fewer and fewer reasons to trade downtown, especially when new commercial developments close to home largely met their needs. The result, of course, has been a classic case of decline of an aging downtown, certainly not unique to Steubenville. In recent years this erosion appears to have slowed and there are even signs of new investment in downtown commercial ventures.

## Social

By 1800 there were several inns and taverns that were the center of social life for the inhabitants of Steubenville. Among these were the Red Lion Inn, the Cross Keys, the White Horse, the Sign of the Ship, the Green Tree and the Black Bear. Also in 1800, "The Grove" was completed on Water Street. This was the home of Bezaleel and Sarah Wells and was the center of hospitality for the community for many years.

The Masonic Lodge was organized in 1817, and four years later the Steubenville Thespian Club was formed. A lending library in Edward Stanton's home in 1830 was the precursor to the City Library Association's organization in 1848. The Steubenville Lecture and Library Club was formed in 1879. The theaters and musical organizations formed in this period include Garrett's Hall (built c. 1870), The Steubenville Philharmonic Society (organized 1868, building built 1872), and the Harmonic Society (1866). The Ladies' Aid Society was organized in 1861, and the YMCA formed about the same time and located at Fourth and Market Streets. It moved many times before it was dissolved in 1875.

Robert Means came to Steubenville in the 1840s and when he died he left a \$10,000 trust fund to the city to help the poor. In 1875 the Pittsburgh, Cincinnati & St. Louis Railroad established a hospital on North Seventh Street for its employees. The King's Daughters Society opened a hospital in apartments at Sixth and North Streets, but grew out of them quickly and moved to a building on the corner of Seventh and Logan Streets. In 1874, Mrs. James Sterling was elected president of the newly organized Women's Christian Temperance Union. The City Temperance Society had been formed in 1845. In 1836, the International Order of Odd Fellows formed followed by the Knights Templar in 1849 and the Knights of Pythias in 1864.

In the 1890s the YMCA was revived and then dissolved again. In 1904 a fund raising effort began for the YMCA and in 1907 \$110,000 was raised in a ten-day period. The cornerstone for the YMCA building was laid in 1908 and the building was finished in 1909. Social activities in the 1890s took place at the Opera House, at Turner Hall on South Third Street, or at the race track in Pleasant Heights. The Wells Historical Society was incorporated in 1893, four years before the Steubenville Centennial celebration began. On August 25, 1897, Steubenville began a three-day celebration of its 100th anniversary.

The turn of the century brought more social and cultural advancements. In 1902 the \$62,000 Carnegie Library opened at 407 South Fourth Street. Known today as the Public Library of Steubenville and Jefferson County's Main Library, the building has undergone several alterations such as shortening of its tower, but it retains a strong presence along South Fourth. In 1908 the Steubenville Country Club opened. 126 acres of the Porter/Starkey farm were purchased for the new club. Construction of the Fort Steuben Hotel began in 1920, and it was completed in 1925. Though a commercial venture, the Fort Steuben was a major component of the city's social life, for here were held weddings, dinners, parties and balls. Among the fondest memories of older Steubenville residents are events at the Fort Steuben Hotel.

The Capitol Theater and the Grand Theater were both completed in 1925.

The city's Recreation Department was created in 1927, and five playgrounds were set up. The contract for a municipal stadium was awarded in 1928, and in 1929 Mrs. Sarah Castner gave to the city 40 acres of woodland on the northwest edge of town with instructions that it be used as a park and dedicated to her husband, Eli Castner. Also in 1929, "talkies" first were run at the Olympic Theater, and a proposal to buy Stanton Park for public use for \$75,000 was rejected (it was a privately held amusement park).

A curb on Sunday social activities was initiated in June of 1930 when the Blue laws were enforced, forbidding movie theaters to open on Sunday. In August, 1930, however, the laws were lifted. The Paramount Theater opened in 1931. After a major fire, the Grand Theater reopened in 1935 in the Griesinger Block on South Fourth Street.

The \$50,000 Community Clubhouse was dedicated in Belleview Park in 1937. Beatty Park was established on part of the land which belonged to Union Cemetery. The cemetery itself had been established in the mid-1850s on rolling land in the hilltop area west of the downtown. Built to the "lawn plan," the cemetery was from the start a major fixture in the city's social

life, seeing regular use as a recreation area.

In the 1930s and 40s there were several celebrations, including the Northwest Territory Sesquicentennial in 1937 and Steubenville's own Sesquicentennial in 1947. Part of the Northwest Territory Sesquicentennial included retracing the original path of the explorers, down the Youghiogheny River from West Newton, Pa. to Steubenville. As the "new pioneers" reached Steubenville, they were met by a river parade of 16 steamboats. Following this was a huge celebration in the city itself and then the pioneers continued down the Ohio to Marietta.

The Benevolent and Protective Order of Elks was formed in 1892 and met in the Specht Building on North Fourth Street. In 1902 the Scottish Rite order of Masonry was established, followed by the Rotary Club (1921), the Kiwanis Club (1922), and the Lions Club (1924). The Knights of Pythias laid the cornerstone for their building on Washington St. in 1922. This later became the Catholic Diocese's College of Steubenville and still later housed local social action programs.

The Steubenville Community Chest began a fund drive in 1931 to help the unfortunate and raised \$103,112. This organization now occupies the Andrew Elliott residence on North Fourth Street.

The Masonic Temple on North Fourth Street was dedicated in 1930, and 12,000 citizens turned out to watch the ceremonies. The centennial of the Jefferson Lodge #6 of the IOOF was celebrated in 1936. Today there are seven fraternal organizations in Steubenville, including Eagles, American Legion, Masons, Elks, Moose, Knights of Columbus, and Pan-Icarian Brotherhood.

Another aspect of social history is the social movements of the late 19th and early 20th centuries. Taking the form of reform movements or social welfare programs, these efforts -- both public and private -- could have a local, a state or a national presence, and they were represented in Steubenville. Included were the YMCA, which dated to 1867 but became firmly established in 1905; the YWCA, founded 1913; Salvation Army, 1888; Girl Scouts and Boy Scouts, established in the 1920s; Family Welfare Association of America Steubenville chapter, founded in 1913; the Phyllis Wheatley Association, founded 1935 to benefit black girls and women; Jefferson County Home for Aged Women, established in 1924; City Rescue Mission, 1928; Jefferson County Chapter, American Red Cross, 1916; and the Jefferson County Tuberculosis and Health Association, founded in 1909.

## ASSOCIATED PROPERTY TYPES

### Governmental Buildings

#### Description

This property type includes city halls, courthouses, fire stations, post offices, markets, and memorial halls.

Steubenville has had three city halls. The first dated from 1816 and was a one-story building called the Market House. Most of the building was used as a public market, with the mayor's office and city meeting rooms housed in a two-story central section. Located opposite the courthouse on the south side of Market Street and the west side of Third, the Market House lasted until 1878, when it was demolished; it was replaced by the three-story Second Empire style City Building completed in 1883. Built of brick with stone trim and sporting a slate mansard roof, the new building housed city offices, a market, the post office and the public library.

The city market in the second city hall was replaced by a new public market building in 1915, to the south along Third Street, and in 1922 this market house was altered for use as a new city hall. The 1883 building was demolished, and in 1929 a commercial building, the Cohen-Reiner Block, was built on its site. In the 1930s the Cohen-Reiner Block became a city annex building. The former market and the annex continue as city buildings today.



The City Annex building has a handsome terra cotta exterior.



The Jefferson County Courthouse, though much altered, still has a commanding presence at the corner of Third and Market Streets.

Like many Ohio county seats, Steubenville has had three courthouses, all on the same site at the northwest corner of Market and Third Streets, opposite the city hall site. The first courthouse, a log building, lasted about twenty years, being demolished in 1807 and replaced in 1809. The second courthouse was a classic first-generation Ohio courthouse: square in plan, with a pyramidal roof and a central tower. It was built of brick and was two stories high. Set a little above the street, it was plainly finished and had little ornamentation apart from pediments at the doorways. Between 1870 and 1874 the present courthouse was built on the same site, a classic second-generation Ohio courthouse. Second Empire in style, it featured projecting pedimented bays at the entrances, supported by paired columns, and it had a convex mansard roof. The mansard roof of the clock tower was supported by an unusual series of arcaded columns. The building is in use today, though with the tower gone and the fourth-floor mansard roof replaced by a flat-roofed top floor of concrete block. These alterations date from the 1940s.

Fire stations were located strategically in the city, with more of them being built as residential development was extended to new areas. Usually built as two-story structures, fire stations were built of brick, with stone trim. The first floor was dominated by one or two large doors for fire equipment, while the upper floors had residential-scale windows (usually 1/1 double-hung sash) and provided living and sleeping facilities. A common feature was a hose tower, usually placed to one side, where hoses were hung for drying after use. Most of Steubenville's fire stations date from the turn of the century or the early 20th century. They usually were not built in specific architectural styles but instead had elements or influences from various styles.



Reliance #2 Fire Station on North Street has a distinctive profile with its steeply-pitched gable.

As noted above, Steubenville's post office was in borrowed space in the city hall in the late 19th century. This was a common means of accommodating postal facilities during the 19th century in small and medium-sized communities. By 1892 the post office was in its own facility at the southwest corner of Fourth and Market, but the design and appearance of this building is unknown. About 1925 a new post office was completed at the northeast corner of Fourth and Washington, where postal facilities stayed until moving to a new facility on Third Street in the late 1980s. The 1925 building is in the Neo-Classical Revival style, featuring arched recessed bays and pilasters on the main elevation. The structure is steel frame, covered with buff-colored brick. Cut stone, probably limestone, is used for trim elements.

The Jefferson County War Memorial Building on North Street opened in November of 1931. It was dedicated to the use, benefit and memory of all the county's service men and women. Though not a full-blown example of the style, it employs details, forms and massing of the Colonial Revival style, including multi-paned windows, corner quoins, a pedimented entrance, a dentilled cornice, and flat window arches. Built of brick and trimmed in cut stone, it sits on a high stone foundation. There appears never to have been another building in Steubenville serving this function.



The former U.S. Post Office is an excellent example of early twentieth century civic architecture.



The Jefferson County War Memorial Building on North Street is another good example of civic architecture. Both it and the Post Office exhibit classical detailing.

## Significance

The governmental buildings of Steubenville are significant under Criteria A, B, and C in the fields of ARCHITECTURE and POLITICS/GOVERNMENT.

Under Criterion A, governmental buildings are significant because they measure the growth and development of Steubenville and thus help to track the broad patterns of the city's history. Buildings built for government use, through their size, location and architectural design, are indicators of the community's success, self-image, and expectations for the future. Buildings such as city halls and courthouses, especially when they are part of a sequence of building and re-building on or near the same site, represent a long tradition of local self-governance; other buildings such as fire stations represent the fruits of self-governance, the benefits that can accrue to self-governing communities.

Under Criterion B, the city's governmental buildings may be significant because of association with important political figures in Steubenville's history.

Under Criterion C, Steubenville's governmental buildings may qualify because they represent certain types and methods of construction. Some buildings may be good examples of academic architectural styles, while others may be important more as examples of building types, such as market houses.

## Registration Requirements

To qualify for listing under Criteria A or B, a property should have the following aspects of integrity:

### 1. LOCATION

Refer to the discussion of location in the introduction.

### 2. DESIGN

If a high-style building or one with some stylistic treatment, the property should have a majority of its stylistic features intact. This includes not only specific decorative elements associated with established architectural styles (such as a bracketed cornice on an Italianate building, or thick columns and arched windows on a Richardsonian Romanesque building), but also overall design elements such as form, massing, roof shape, and size and placement of window and door openings. Because governmental buildings tend to be unique, some loss of original design integrity may be acceptable as long as the basic design is apparent and the building still represents the time period of its construction.

### 3. ASSOCIATION

The property should be sufficiently intact in the aspects of location and design to convey the historical relationship between an event or person and the property. For example, despite some notable alterations, the Jefferson County Courthouse retains enough of its original form, massing and stylistic elements that a county commissioner from the turn of the century would recognize the building.

In order to qualify under Criterion C, to qualify for listing a property should have the following aspects of integrity:

#### 1. DESIGN

Refer to the comments above for Criteria A and B.

#### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. This should include the original exterior surface materials, window and door trim, decorative elements, and materials directly related to architectural style such as cornice brackets, columns, and window and door treatment. As with the aspect of design, the unique nature of most governmental buildings means that some loss of materials will not necessarily keep the property from qualifying, as long as most of the historic materials remain intact. The Jefferson County Courthouse, for example, has lost its tower and mansard roof and has had a boxy top floor added, but enough remains of its late 19th century walls, trim elements and other materials that it would qualify for listing.

#### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For governmental buildings, this includes patterned brickwork, cut or carved stone elements, exterior ornamentation in various materials, and interior features such as ornate woodwork, tile or mosaic floors, decorative plasterwork, and wall or ceiling murals.

#### 4. FEELING

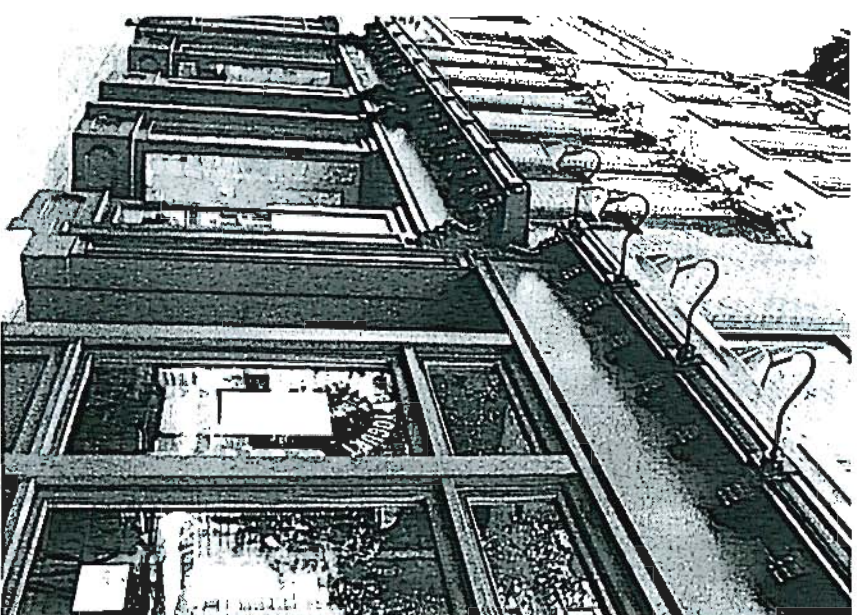
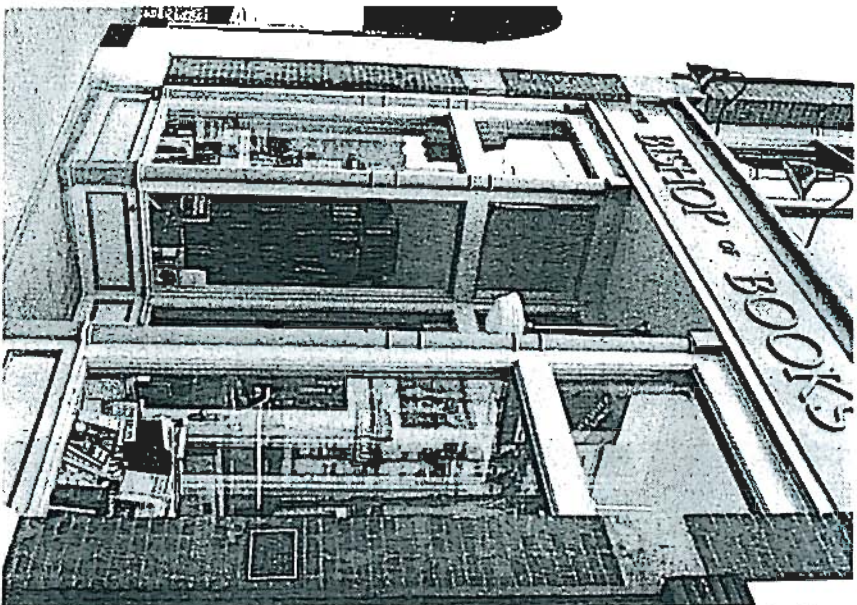
The totality of a building's components should express a specific point or period in time.

## Commercial Buildings

### Description

This property type includes small-scale one-to-three-story brick commercial buildings, both vernacular in design and with stylistic treatment; high-rise commercial buildings; department and chain stores; hotels; banks; wholesale warehouses; and small- or medium-scale production facilities with retail outlets.

Small-scale commercial buildings have lined Steubenville's downtown streets almost from the first day of its settlement. Plank or log construction was common in the earliest buildings, but nearby supplies of good clay meant that brick construction was possible early on. As early as the 1840s, Steubenville's downtown commercial buildings had assumed the form in which they would continue to be built through the end of the 19th century and in the first half of the 20th century: uniform setback along the inside edge of the sidewalk; common use of shared or party walls; a general uniformity of height; placement of storefronts with large display windows side-by-side along the sidewalk; and variation in signage and ornamentation to provide visual variety and individual distinctiveness. Some buildings were executed as full-blown examples of commercial architectural styles, while others only employed elements of those styles or were simple vernacular compositions.



Exce lent examples of recently rehabilitated storefronts in Steubenville's downtown commercial historic district.

Italianate, High Victorian Italianate and High Victorian Gothic were the most common late 19th century styles, while early 20th century commercial styles included Second Renaissance Revival, Colonial Revival, Neo-Classical Revival, and Moderne. Vernacular designs sometimes employed elements from these styles. Small-scale commercial buildings in Steubenville are almost universally of brick, employing cut stone, terra cotta, formed sheet metal and wood in storefronts and decorative elements.



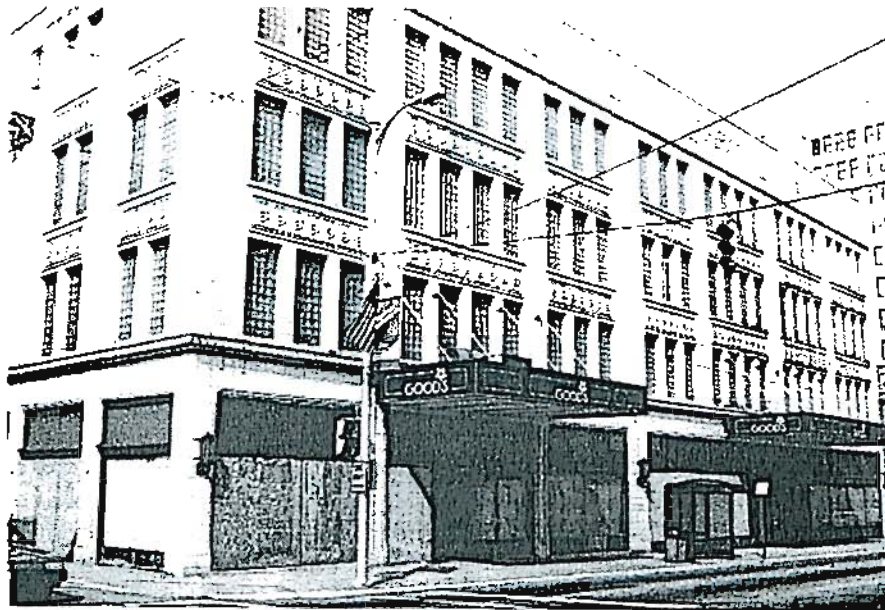
The McKee Building on North Fourth Street is one of many examples in Steubenville of intricately detailed terra cotta facades.

High-rise commercial buildings in Steubenville all are 20th century in origin. The early ones (up to 1910) are of brick bearing wall construction, and thus are of lower height (four to six stories), while later ones (after 1910) are of steel frame construction and rise up to 12 stories. These buildings tend to be heavily ornamented but are not necessarily built in academic architectural styles. When they are built in specific styles or employ stylistic elements, Second Renaissance Revival is the most popular. Typically these buildings have one or two large ground-floor commercial spaces (banks were the most common original tenants), while the upper floors are given over to office uses. Exterior materials include brick, terra cotta, stone and architectural metals.

Department and chain stores in many ways resemble the small-scale commercial buildings, since in Steubenville they seldom exceed two stories in height. Usually they were built between about 1920 and 1930 and thus were late additions to the city's commercial building stock. They shared



The Sinclair Building is a true visual landmark with a highly decorative cornice line.



The former Hub Building, long a downtown Steubenville landmark, is the most significant example of a single-purpose commercial building in the city.

common features with other commercial buildings, such as common walls, common setback, and street-level elevations almost entirely made up of large display windows. They generally had greater street frontage than typical commercial buildings, since they tended to be larger enterprises requiring more space. Stylistic treatment generally was confined to the later 20th century styles such as Colonial Revival and Moderne. In Steubenville, light-colored glazed terra cotta was the preferred exterior, trim and decorative material.



The Fort Steuben Hotel is a classic early twentieth century downtown hotel that dominates the north end of the downtown commercial district.

Hotels in many ways resembled the other high-rise commercial buildings. The older ones, which dated from as early as the 1870s, were of brick bearing wall construction, while later ones, early 20th century in date, were of steel frame construction. Like the commercial buildings, hotels generally were either built in a particular style or employed considerable ornamentation derived from one or more styles. Styles employed in Steubenville hotels included Second Empire, Second Renaissance Revival and Moderne. In addition to public entrance doors and large windows lighting lobbies, restaurants and function rooms, hotels typically included street-level commercial spaces. Wall and ornamental materials included brick, stone, terra cotta and architectural metals.

Banks sometimes were housed in small-scale commercial buildings, sometimes in high-rise commercial buildings, and sometimes in their own separate structures. Separate free-standing bank buildings were rare in Steubenville, with most financial institutions preferring to share space with other businesses in various types of commercial structures. When banks did build their own structures, these were designed in a way that blended in with other commercial architecture -- there were not, for example, any Greek Revival or Neo-Classical Revival banks sporting large columns and heavy pediments. Instead, bank buildings looked much like other commercial structures, with large street-level windows, common setback, shared walls, and upper-floor office spaces. Glazed terra cotta was the preferred exterior material for Steubenville bank buildings.



The former Jefferson Building and Loan Association Building, located across from the Courthouse, has a classically-inspired terra cotta exterior.

Wholesale warehouses typically were found toward the edge rather than the center of the downtown core. This was partly due to land costs, and partly because these facilities needed to have ready access to one of the two main rail routes through Steubenville. These warehouses typically were early 20th century in date and of steel or concrete frame construction. Exterior materials were brick and some stone, terra cotta or formed sheet metal detailing. Sometimes a concrete frame was exposed on the exterior and sometimes not; academic architectural styles typically were not employed in wholesale warehouses. Windows tended to be few and small, usually simple 1/1 double-hung sash. Administrative/office areas often were at ground level on the street and sometimes received some architectural ornamentation to set them off from the rest of the building. Side and rear walls usually had large doors and platforms for movement of goods into and out of the building.

Production facilities with retail outlets were not numerous in the downtown core of Steubenville, but some could be found from the late 19th century well into the 20th. They have been described in detail in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910."

## Significance

Steubenville's downtown commercial structures are significant under Criteria A and C in the fields of ARCHITECTURE and COMMERCE.

Under Criterion A, commercial buildings are significant because they represent the economic life of a community, one of the most important -- if not the most important -- broad patterns in the history of any place. The ebb and flow of commercial activity in a community directly affects political and social life, settlement and development patterns, industrial development and diversification, and the architecture, physical layout, and the very look of the community. Surviving commercial architecture in Steubenville is an indicator which can be traced to specific time periods and which tells a detailed story of how the downtown core evolved, grew, changed and declined over time.

Under Criterion C, a property may be significant because it represents a certain type and method of construction, or because it is a good example of an academic architectural style or combination of styles, or if it employs elements of architectural styles in a manner that is unusual or unique for Steubenville.

## Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

### 1. LOCATION

Refer to the discussion of location in the introduction.

### 2. DESIGN

The property should retain sufficient structural, finish and stylistic features to identify it as a downtown commercial structure from the 1870-1910 period. This includes retention of such features as original or historic storefronts (bulkhead, display windows, transoms, signboards, doors and doorways, and associated ornamentation); beltcourse or intermediate cornices; upper-floor window openings or projecting bays and associated trim or ornamentation; exterior surface materials; and bracketed cornices or other terminating features at the top of the main elevation, including associated ornamentation or trim.

To qualify for listing under Criterion C, a property should have the following aspects of integrity:

### 1. DESIGN

Refer to the comments above for Criterion A.

### 2. MATERIALS

A property should retain a majority of the historic materials of which it was built. These include materials comprising the commercial storefront(s) and entrance door(s); upper-floor wall materials including window openings

and trim; and original decorative features such as bracketed cornices, false fronts or parapets. Alterations will not disqualify the property as long as the character-defining features and original materials remain substantially intact.

### 3. WORKMANSHIP

The property should retain the major items of workmanship that make it an example of the time and place in which it was constructed. Downtown commercial buildings should retain, for example, decorative wood, stone or iron components of storefronts; leaded, prism or decorative glass windows or transoms; paneled or ornamented entrance doors; or decorative or trim elements in signboards, beltcourses or cornices. On the interior, wooden floors and decorative pressed metal ceilings should survive as important elements of workmanship. Alteration or loss of storefronts, cornices or other items of workmanship will not necessarily keep a property from qualifying, as long as at least some items of workmanship have survived.

### 4. FEELING

The totality of a property's components should express a specific point or span of time in the period 1870-1910. This results from a majority of the property's elements of design, materials and workmanship remaining intact. The specific components of downtown commercial buildings that contribute to these aspects of integrity are identified above.

### 5. SETTING (for historic districts)

As noted in the introduction, the aspect of setting is important under all criteria, but especially in the case of historic districts under Criterion C. The physical environment into which buildings are placed should survive with its major features intact. For downtown commercial buildings this includes the historic street layout, sidewalks, uniform building setback, and survival of continuous glass storefronts along the street. There should not be any spaces between buildings that were not there historically (such spaces might result, for example, from demolition of a building with no replacement structure built). Some such open spaces will not necessarily keep a district from qualifying, as long as the historic setting is otherwise substantially intact.

## Social-Function Buildings

### Description

This property type includes live and movie theaters, fraternal lodge buildings, social or welfare organization headquarters, and churches and rectories.

Live and movie theaters generally were designed to fit in with the downtown commercial streetscape and as a result had design features such as common setback; height similar to that of other buildings; general consistency of building materials, level of ornamentation, and overall style; and similar pattern of windows and doors. Often the main elevation of a theater

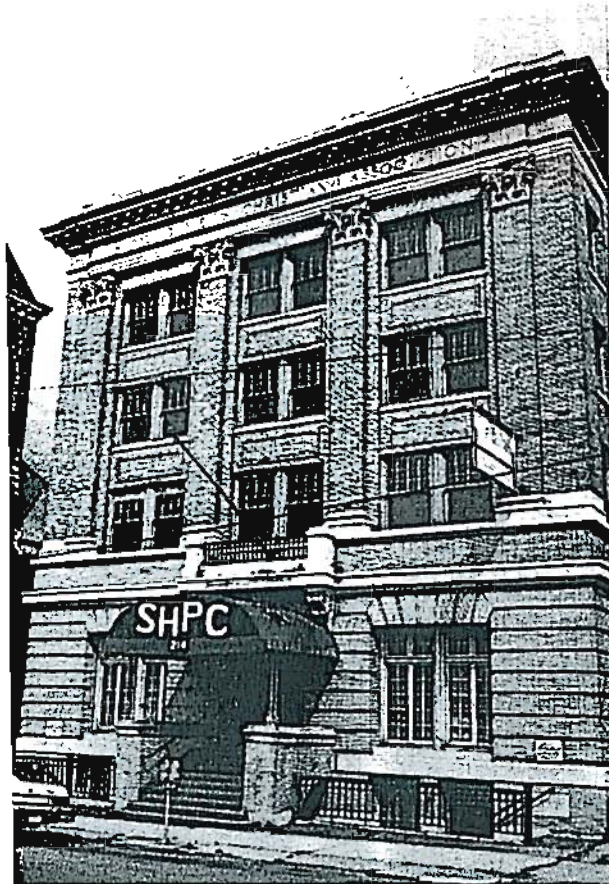


The Masonic Temple on North Fourth Street is an impressive Neo-Classical Revival building dating from 1931.

building had commercial storefronts flanking the theater entrance doors. There usually were two, three or four pairs of entrance doors to handle large crowds of people. Brick was the most common building material, with stone, terra cotta and architectural metals used as trim elements. An Italianate commercial style was the most common stylistic treatment. The stage end of the theater auditorium usually was at the rear, opening onto an alley or another street, permitting access for stage crews and supplies. Sometimes a tall fly space was built over the stage, which appeared on the exterior of the building as a three- or four-story windowless block.

Fraternal lodges very often met in rooms on the upper floors of downtown commercial buildings, but in some instances these organizations built their own buildings. Usually these were built as free-standing structures rather than as part of a row of buildings, in order to make the lodge headquarters more visible and impressive. At the same time, these buildings generally followed design elements of the downtown such as building height, setback, materials, and level of ornamentation. While some fraternal lodge buildings were built in academic architectural styles, others drew elements from one or more styles in a vernacular composition. Early 20th century

designs were favored, including examples from or elements of the Second Renaissance Revival or Colonial Revival styles. In cases where 19th century styles provided inspiration, Second Empire, Italianate and the High Victorian styles were favored. Free-standing lodge buildings did not typically have street-level commercial spaces, and they tended to be large structures that combined meeting rooms, lodge offices, and an auditorium for meetings and events. Brick and stone were the most common exterior materials, with trim and decorative elements of stone, terra cotta and architectural metals.



The former YMCA, together with the Post Office and Masonic Temple terminates the downtown commercial district's north end.

Social or welfare organization headquarters could be in a wide variety of buildings, but they tended generally to be done in plain, unornamented designs. This was particularly true when the agency provided services for the poor and disadvantaged and was sensitive to the issue of not looking ostentatious or wasteful of resources. Brick was a preferred building material, and whatever trim or detailing there was could be in brick, cut stone or formed sheet metal. Generally, vernacular designs were used rather than building in architectural styles. Features such as windows and doors tended to be plain, generally simple wooden double-hung sash and paneled wood doors. Most buildings of this type were constructed in the early 20th century.

Churches and rectories were described in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910."

## Significance

The social-function buildings of Steubenville are significant under Criteria A and C in the fields of ARCHITECTURE and SOCIAL/WELFARE.

Under Criterion A, social-function buildings are significant because they represent the level and extent of social involvement and interaction in a community. This is one of the broad patterns in the history of a community, an indicator of the principles, aspirations and ideals of the community. The level and extent of social involvement indicate how far a place such as Steubenville has come beyond just a concern with simple survival, and how far it is willing and able to address concerns that affect the quality of its citizens' lives. Under Criterion A, it must be shown that the property was used for the activities, programs or efforts of a social organization with an effective presence in Steubenville; the organization may be of local, state or national size or impact.

Under Criterion C, a property may be significant because it represents a certain type and method of construction, or because it is a good example of an academic architectural style or combination of styles, or if it employs elements of architectural styles in a manner that is unusual or unique for Steubenville.

## Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

### 1. LOCATION

Refer to the discussion of location in the introduction.

### 2. DESIGN

If a high-style building or one with stylistic features, the property should have those features intact. For example, an Italianate building should have its boxy rectangular form, vertically-proportioned openings, shallow-pitched roof, and bracketed cornice. A Queen Anne building should have its irregular massing, variety of roof shapes and exterior materials, and varied window and door sizes and placement.

Vernacular buildings should have their basic form, roof shape and pitch, window and door openings, interior plan, and any decorative elements substantially intact.

To qualify for listing under Criterion C, a property should have the following aspects of integrity:

### 1. DESIGN

Refer to the comments above for Criteria A and B.

### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built, including overall massing and form, doors and fenestration, and

decorative elements and details. For buildings with stylistic elements, retention of those elements is important. For vernacular buildings, loss of some historic materials will not necessarily keep a property from qualifying as long as the basic form and plan remain intact.

### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed.

For buildings built in architectural styles or with stylistic features, this should include, for example, hoodmolds, cornice brackets, and window and door trim for Italianate buildings; patterned brickwork, varied siding materials, and decorative features for Queen Anne buildings; and fluted columns, dentils and classical detailing for Colonial Revival buildings. Any artwork, murals or other artistic elements associated with the property's occupants during the period of significance likewise should survive intact. For vernacular buildings, items of workmanship should include window and door trim, any decorative features, porches, and any artwork as described above.

### 4. FEELING

The totality of a building's components, whether the building is vernacular or built in an architectural style or with stylistic elements, should express a specific point or period in time.

# **ETHNIC DIVERSITY IN STEUBENVILLE, 1840 - 1940**

## **Introduction**

"From the unsettlement of Europe came the settlement of America." In this succinct statement, a historian of American immigration summed up the cause, process, and effect of perhaps the greatest sustained mass movement of people ever in the history of the world. Colonized, settled and developed by emigrants primarily from Europe, the United States since its earliest days as a colony has always been a more or less tempting lure for people seeking to find a good life or leave behind a bad one. The American "melting pot" was real, at least in the sense of blending diverse ingredients to form a new product no one had seen before.

The city of Steubenville, Ohio was a microcosm of American immigration, its experiences generally matching national patterns. From the English, Irish, and Scottish immigrants of the city's early years to the eastern Europeans of the early 20th century, Steubenville participated in all the major movements of immigrants to the United States over a 100-year period.

This context depicts Steubenville's growing ethnic diversity during the 19th and early 20th centuries by describing major trends in immigration to the city, presented in light of national trends. The discussion is by time period, the most effective way of presenting the information because different ethnic groups tended to arrive at different times. The final portion of this context discusses the property types associated with the context and establishes registration requirements for them.

## **The Mosaic of American Immigration**

Though often depicted as a series of waves of new arrivals, immigration to the United States -- and to the Colonies before that -- has always been more or less ongoing, with peaks and valleys in the number and variation in the backgrounds of arriving immigrants influenced by conditions and events both here and in the originating countries.

Emigration -- the leaving of one's native land for permanent settlement elsewhere -- has always been the result of both "pushing" and "pulling" forces. Pulling immigrants to the U.S. have been influences as diverse as perceptions of unlimited economic opportunity in the New World; opportunities for religious, social, and personal freedom; and the belief that one could better himself and rise above old class barriers in the New World. African-Americans are, of course, the one notable exception to this perception of America as a land of opportunity, since first-generation black immigrants were brought here by force.

Forces pushing emigrants from the countries of their birth to the United States included equally diverse influences: revolution and civil war; crop failures and blights; rigid class distinctions that perpetuated poverty-stricken lifestyles; and social and religious bigotry and persecution.

These varying influences worked in different places at different times to affect the U.S.-bound flow of immigrants, mainly from Europe but truly from around the world. Influences in the U.S. itself -- the institution of

slavery, periodic anti-immigrant backlashes, the growth of industrial capitalism and its associated need for unskilled labor, and our own Civil War -- all affected who came to the U.S., under what circumstances, and with what prospects for the future.

Immigration into the U.S. grew as employment opportunities grew during the early and middle years of the 19th century. Major draws for immigrants were the building of transportation networks, first canals and then railroads; industrialization and mass production in areas such as pottery, textiles, glass, and steel, and coal mining. These were all inter-related, so that extension of railroads, for example, permitted more development of coal lands, and the ready availability of coal as fuel for stationary steam engines encouraged industrialization which, by providing a traffic base, further encouraged railroad construction.

Not surprisingly, English-speaking immigrants from the British Isles (England, Ireland, Scotland, and Wales) were among the earliest arrivals. With no language barriers, fairly high skill levels, and a long history of moving to the New World, these immigrants assimilated quickly into the culture and into various kinds of employment. Skilled Englishmen worked in industries such as wool, glass, and pottery; unskilled laborers from all four countries found work on the railroads and in the coal mines.

In the mines, not all were unskilled, however; the miners themselves were in fact quite skilled at their craft, but the whole mining industry relied as well on the supporting labor of unskilled workers for coal loading, mule driving, coal sorting and cleaning, and other roles. In fact, the coal industry for decades was nearly the exclusive province of immigrants from the British Isles: as late as 1880, for example, nearly all of Ohio's nearly 6,000 coal miners were British immigrants.

German immigration into the U.S. began in large numbers in the early 1830s, many of the newcomers finding work in canal building and in the brewing industry. Not all the immigrants were working class or poor people, however; exportation of capitalists and capital from Germany in this period was a serious concern to the German government.

By the 1840s several factors were in place that would facilitate American immigration. Steam-powered ocean vessels had evolved to a high level of reliability, leading to shorter and more regular schedules and lower fares (the Liverpool-New York fare, for example, fell from 12 pounds sterling in 1816 to three pounds in the 1840s); improved land transportation in Europe (primarily rail transport) made ports of departure more accessible; and ticket agencies and immigration agents, who could arrange passage in return for work in the New World, made the whole emigration process easier and less forbidding.

These factors showed up in the statistics. During the decade of the 1830s, 600,000 immigrants arrived in the U.S. This rose to 1.7 million in the 1840s, and to 2.3 million in the 1850s. Forces in Europe pushing emigrants out included the Irish potato famine of the 1840s, which also affected Germany, and German political unrest in the late 1840s. In 1847 alone, 100,000 Irish emigrants left their homes, and during the 1850s over a million Germans left.

Between 1815 and 1860 the United States received a total of 5 million immigrants (the national population was about 37 million), including 2 million Irish; 750,000 from England, Scotland, and Wales; 1.5 million Germans; and smaller numbers from France, Sweden, Norway, Switzerland, and the Netherlands. On the West Coast, Chinese immigration, primarily as railroad construction workers, was the primary phenomenon, though this did not peak until after the Civil War.

The American Civil War and the period of political and economic turbulence that followed slowed immigration, but by 1880 the inflow of foreigners was on the upswing again. Rapid industrial, agricultural, and transportation development created a quickly expanding economy with large numbers of unskilled or low-skill job opportunities. The rise of large industrial corporations, which broke down skilled manufacturing tasks into a series of steps requiring little or no skill or training, was a significant draw for emigrating foreigners. Expansion of the American railroad network likewise required large numbers of workers, as did extractive industries such as coal, oil, natural gas, and lumbering.

These factors encouraged the last large 19th century wave of immigrants, which took place between 1880 and 1914, up to the outbreak of World War I. However, the face of immigration had changed. Northern Europeans, though they still came to the U.S., no longer formed the bulk of the new arrivals. German immigration, for example, hit 110,000 in 1880 and 250,000 in 1882, but it then declined as industrial expansion and increased economic opportunity in Germany gave people less reason to leave. The same was true in Great Britain, where a further factor was the ability for people to emigrate to other parts of the British Empire rather than to the United States.

The more than 15 million American immigrants during the 1880-1914 period were instead southern and eastern Europeans from Italy, Greece, Romania, Austria-Hungary, and Russia; later, others came from the Near East and the Far East. These immigrants, coming primarily from agricultural rather than industrial economies, tended to be less skilled and in need of more training for industrial jobs in the U.S. than the northern Europeans, though they were not necessarily poorer or less able to be trained.

Statistics again give some idea of the scale and the increasing pace of immigration in this period. In 1880, for example, 12,000 Italians came to the U.S., but in 1890 this had swelled to 100,000. Just before World War I, 300,000 Italians a year entered the country. Forces giving them a push out of Italy included an archaic landholding system; over-population; and soil erosion and resulting agricultural problems. Similar problems existed in other southern European countries; 186,000 Greek immigrants, for example, arrived between 1890 and 1910.

By the turn of the century, there were a number of shipping lines catering to immigrants, and thousands of steamship agencies sold pre-paid tickets that immigrants could send home so family members who had been left behind could join them in the United States. Relatively safe and inexpensive passage in "steerage" even permitted two-way travel for seasonal workers in the U.S. Both Italians and Greeks employed the "padrone" system, in which labor contractors found jobs and lodging for workers (at a profit for the contractor) and later helped to bring workers' families to the U.S.

Jewish immigrants from Germany, eastern Europe, and Russia began coming to the U.S. in large numbers in the 1880s, as a result of economic conditions, religious persecution, and pogroms in Poland and Russia. As with other groups, the pace quickened in later years: 40,000 eastern European Jews came in the 1870s, but 1.5 million arrived between 1900 and 1914. These immigrants generally brought skills with them -- they were tailors, tinsmiths, jewelers, hat and cap makers, bookbinders -- and tended not to come from an agricultural background.

Poles fled the predations of the Russian Empire in the first decade of the 20th century but had been arriving in large numbers before that. 35,000 came in the decade of the 1870s, and in the 1880s 236,000 left Poland. Annual emigration peaked in 1913, when 175,000 left home for the U.S. Polish immigrants worked in large numbers in American coal mines and steel mills.

The start of World War I cut the flow of immigrants, but in this same period a growing backlash among native-born Americans also contributed to a less open immigration policy. Religious and racial prejudice, a perception that foreigners were usurping jobs and introducing political instability, and a general distrust of foreigners and an isolationist attitude led eventually to restrictions on immigration in the 1920s. After that time, immigrants would still arrive and be admitted, but in much smaller numbers; the great migrations of the late 19th and early 20th centuries were over.

## Steubenville Immigrants and Ethnic Groups

### 1840-1880

The following table showing the population of Steubenville by decade will be helpful in placing the city's ethnic/immigration experience in perspective.

#### Population of Steubenville, Ohio -- 1830 to 1990

<u>Year</u>	<u>Population</u>	<u>% Change</u>
1830	2,937	
1840	4,247	+44.6
1850	6,140	+44.6
1860	6,154	+ 0.2
1870	8,107	+31.7
1880	12,093	+49.2
1890	13,394	+10.8
1900	14,349	+ 7.1
1910	22,391	+56.0
1920	28,508	+27.3
1930	35,422	+24.3
1940	37,651	+ 6.3
1950	35,872	- 4.7
1960	32,495	- 9.4
1970	30,771	- 5.3
1980	26,400	-14.2
1990	22,125	-16.2

Census figures up through 1860 did not provide a breakdown of foreign-born Steubenville residents. For example, the 1850 census listed 6,003 "white" residents and 137 "colored," and in 1860 the same figures were 5,983 and 179 (they varied slightly from the total in the table above). By 1870, however, overall figures (but no breakdown by nationality) were given for foreign-born Steubenville residents: of a total population of 8,107, 6,460 were native-born and 1,647 (20.3%) were foreign-born. The "colored" or black population had increased, but only to 276 (3.4%). Foreign-born and black residents were concentrated in the Second and Third wards of the city, though they lived in all four wards.

1870 census figures for Jefferson County did identify foreign-born residents by country of origin, so these figures can give some sense of Steubenville's immigrant population. County population was 25,043, and of these 3,245 were foreign-born -- Steubenville thus had almost exactly half of the county's immigrant population, but only 32% of the total county population. The immigrants came from British America, or Canada (69 people); England and Wales (1,000); Ireland (1,352); Scotland (220); Germany (531); France (7); and Sweden and Norway (15); and 51 from other places. Clearly, by 1870 the great bulk of immigration to the county was from English-speaking countries, and it can be assumed that Steubenville's immigrant residents were largely English-speaking.

By the 1880 census, country-of-origin figures still were given only for the county, with no breakout for Steubenville or other communities. In the city, of 12,093 residents, 11,582 were white and 510 were black. The native-born population was 10,150, and 1,943 were foreign-born. Countywide, foreign-born residents came from British America (60); England and Wales (739); Ireland (1,178); Scotland (188); Germany (592); France (9); and Sweden and Norway (29). This totaled 2,795 foreign-born people in the county, a 13.9% decline from the prior census, at a time when total city and county population was rising sharply. Now some 70% of the county's foreign-born population lived in Steubenville.

### **Blacks**

During this period, Steubenville's black population was small, but Jefferson County had for some time played a role in black life and history. The first abolitionist newspaper in the United States, The Abolitionist, began publication in Mount Pleasant in 1821, and also located there was the Free Labor Store, where no goods made by slave labor were sold.

In Steubenville, in the early 1870s the School Board purchased a former girls' school building on the west side of North Third Street just below Dock Street. Here was established a "colored" school for black children. However, attendance was consistently low, so in 1883 the school was abolished and the black children were sent to public schools together with white children.

The Quinn Memorial AME Church was organized in 1823 by 18 members; Rev. William Pauly Quinn was pastor and founder. In 1846 the congregation purchased a lot at the northeast corner of South and Third Streets and built a church. It was used until 1873, when it was demolished. In 1878 the congregation purchased a lot at Fifth and Washington; the church and a two-story brick parsonage were built in the 1890s. By 1910 the

congregation numbered 100; and in 1929 the church at 515 North Street replaced the 1890s buildings. Another congregation, Phillips Chapel, was formed in 1926 and still occupies a former synagogue on North Fifth Street.

### **English Immigrants**

Early English immigration to Steubenville was primarily due to the local wool industry, which included the raising of Merino sheep and mills that processed wool. There never was an English enclave in Steubenville, which was to be expected because common language, skills, and customs made assimilation easy; though one small cluster of English citizens lived in the 300-400 block of South Third Street. The local coal industry also attracted English immigrants; these included C.R. Thompson, who came in 1862 and was superintendent of the Jefferson Coal Shaft, and William Smurthwaite, superintendent of the Steubenville Coal and mining Company for 50 years. The English immigrants did not form their own social clubs as did other groups, instead joining established organizations such as the Odd Fellows, the Masons, and the Knights of Pythias.

### **Welsh Immigrants**

Welsh immigrants were lumped with the English in census records and thus can be hard to track. In Steubenville they were not a large group, and they came primarily because of the coal industry. Like the English, the Welsh tended to take supervisory and management jobs in the coalfields, leaving the mining and laboring jobs to others such as the Irish.

### **Irish Immigrants**

The Irish were the largest immigrant group in Steubenville during this period (1840-1880), typically numbering close to half again as many as the English/Welsh. Significant Irish emigration dated from the potato famine of the 1840s and consisted of two groups, the Irish Catholics and the Scots-Irish, who were Presbyterian. The northern part of Steubenville was considered the Irish area, centered around St. Peter Catholic Church. Msgr. Dean Hartnedy, pastor at St. Peter in 1879, selected the site of and platted Mt. Calvary Cemetery.

Irish immigrants generally worked in less-skilled jobs than the English and Welsh and often were considered "lower class" by other immigrants and by native-born citizens. Employment opportunities for the Irish included the railroad, the glass works, and as laborers. Some individuals eventually did quite well, an example being William Sharp, founder of W.L. Sharp & Son, a stove manufacturer. Irish social clubs included the Ancient Order of Hibernians and, in 1881, a local chapter of the Irish Land League of America.

### **German Immigrants**

German immigration to the U.S. peaked in the late 1840s due to large-scale crop failures and civil war in Germany. German immigrants to Steubenville did not reach large numbers but were a large group relative to others. Concentrations of Germans could be found on the city's north end and also south of Market Street on South Fourth and South Fifth Streets. German immigrants generally were not found in large numbers in factories, mills, or on the railroads; they instead gravitated to commercial enterprises.

On June 25, 1861, 62 people organized the German Evangelical Lutheran Church and purchased a building lot at 139 North Fifth Street. The first church apparently was not built at the site until the early 1880s; however, in 1877 the congregation's name had been changed to Zion Evangelical Lutheran Church. The 1880s building was replaced in 1915.

In the city's south end, St. John's Lutheran Church was organized in 1877, and the church building at South Third and South Streets was dedicated in 1880.

German Catholics in the north end went to St. Peter and in the south end to Holy Name Church. The German pastor at Holy Name about 1900, Joseph Weigand, had come to the U.S. in 1879.

A German school was established in 1861 but lasted only a short time. A local German newspaper, the Steubenville Germania, was published by Max Gescheider between 1875 and the outbreak of World War I.

Steubenville had a large number of German social clubs, particularly in relation to the fairly small number of German immigrants. Largest was the Turnverein, established in 1874. This organization built Turner Hall in 1887 on the east side of North Third Street just north of Washington; it was both a social and an athletic club. The Harmonie Society, a choral group, dated from 1878; others included the German Farmer's Club, the German Beneficial Society, the Schutzen (shooting) Club, a chapter of the National German American Alliance, and the Schwabenverein (Bavarian) or Schwaben Beneficial Society. This last was formed in 1886 and was headquartered at the building still standing at 211 North Sixth Street.

## **Steubenville Immigrants and Ethnic Groups**

### **1880-1920**

"An economic and cultural explosion took place in the upper Ohio Valley community of Steubenville, Ohio during the first three decades of the 20th century. The city's population more than doubled, and its economy expanded more rapidly than ever before as it outstripped, for a time, the growth rate of any Ohio city." This was one historian's assessment of the city's phenomenal growth during the period when the greatest number of immigrants were arriving and when the industrial base was undergoing its greatest expansion. Indeed, the population tripled between 1880 and 1930.

However, most of this happened during the early 20th century. The two decades from 1880 to the end of the century did help set the stage, as Steubenville's industrial firms became well established and were integrated into national markets, and as the city's commercial enterprises served an ever wider market area. The population figures do show, however, that growth was fairly modest in the late 19th century: from 12,093 citizens in 1880, the city grew to only 14,349 by 1900, a 19% increase. Clearly, the city was pausing from the rapid pace of growth between 1860 and 1880, when population doubled.

The mix of immigrant and native-born population had not changed much from the 1880 census to the one in 1890; it declined, in fact, both in absolute

and relative numbers. In 1880 there were 1,943 foreign-born residents, in 1890 1,933; the native-born population rose from 10,150 in 1880 to 11,461 in 1890. However, a new figure appeared in the 1890 census: native-born citizens with foreign-born parents; there were 3,651 such people in the city, 32% of the native-born population. This indicates that in fact the numbers of people who could be considered of "foreign extraction" or "foreign background" was quite large, though only some of them actually had come as immigrants. Much of what was seen in Steubenville as a growing foreign population was in fact due to growing families and not to new immigration.

During this same period, the black population was nearly unchanged, numbering 510 in 1880 and 505 in 1890. Without further details, it is impossible to say what combination of factors -- number of arrivals, number of departures, birth rate, and death rate -- caused this stagnation.

Because of inconsistencies in how census results were reported, the country-of-origin figures for Jefferson County in the 1890 census were not found. However, given the small amount of change in total numbers of foreign-born people in Steubenville, it is likely that the figures for country of origin for both the city and the county, had not changed substantially. Because Steubenville was under 25,000 total population, the census did not include country-of-origin figures for the city. These figures first appeared in the 1920 census, when Steubenville's population was 28,508.

The 12th Census in 1900 began to show some shifts from the immigration patterns typical of the earlier decades. Countywide, the foreign-born population had nearly doubled since 1880, to 5,252 people, or 11.8% of the county's population of 44,357; most of this increase probably occurred between 1890 and 1900. Unfortunately, inconsistencies in reporting again make it difficult to make census-to-census comparisons -- for example, no total figure for foreign-born residents in Steubenville was given, though it was in past reports, and the country-of-origin figures for Jefferson County do not add up. However, some general conclusions may be drawn. The numbers of foreign-born residents from Ireland, for example, had declined by more than 50%, and new nationalities had risen by a large amount. The figures for foreign-born Irish and other early immigrant groups probably reflected a decline in arrivals from the mother countries, and the effect of the passage of time, as native-born children of immigrants grew up and the original first-generation arrivals died.

Though their greatest influx was yet to come, eastern European immigrants by 1900 were becoming a dominant trend. Countries of origin sometimes were unclear, due to inconsistent naming of countries by census takers, and to shifting borders and names in Europe. For example, Jefferson County figures were given for "Poland (Austrian)," "Poland (German)," "Poland (Russian)," "Poland (unknown)," and "Russia." It is hard to know where the census-takers perceived these places to be or how it was decided who was assigned to what group but the relatively higher numbers of foreign-born county residents from eastern and southern countries (Italy was well-represented in this census) show clearly how immigration patterns had shifted.

These immigrants generally came from agrarian and unskilled or semi-skilled backgrounds; the steel mills in Mingo Junction, Steubenville, and Weirton, WV represented primary employment opportunities for these groups.

By the Census of 1910, Italians had become by far the largest immigrant group in Jefferson County. Steubenville's population was 22,391, so breakout figures for the city were not published, but the country-of-origin figures for the county again probably fairly represented the mix in Steubenville. County population was 65,423, consisting of 63,768 white residents and 1,647 black; there were six Asians (listed as Chinese), and two were unaccounted for.

Country-of-origin figures for the county were Italy (3,596); Austria (2,718); Hungary (2,327); Russia (1,938); England (920); Germany (818); Ireland (574); Wales (390); Scotland (365); Belgium (196); France (90); Canada (82); Greece (69); Finland (1); and approximately 500 identified as "other." As in the past, Steubenville probably had a large percentage of the total foreign-born population (in the 1910 census there was no summary figure for foreign-born people in Steubenville), and Mingo Junction probably had most of the rest.

The county figures clearly show that the immigration flow from western Europe had largely stopped, and that eastern and southern Europe were providing most of the new arrivals, this local experience matched national trends.

Other figures for Jefferson County in the 1910 census listed native-born residents both of whose parents were foreign-born; that is, the second generation of immigrants was documented. 9,016 native-born Jefferson County residents were shown as having both parents of foreign birth. As might be expected, the western European countries from which the earlier immigrants had come showed fairly high numbers, since these people had had time to become established and have children, but several eastern and southern European countries also showed high numbers, indicating that the immigrants from these countries became established and had families quickly. As examples, there were 1,323 native-born people with German parents; 1,185 with Irish parents; 651 with English parents; 274 with Welsh parents; and 250 with Scottish parents. Native-born people with Austrian parents, however, numbered 1,201; Hungarian 1,084; Italian 1,040; and Russian 1,015. Though the major waves of these latter immigrants had been occurring only since about 1890 or even later, large numbers had come to Jefferson County, and they had had large families by the time of the 1910 census. Once again, it is unfortunate that the census reports do not give specific figures for Steubenville, but it still is reasonable to assume that the majority of the immigrants and their children were in Steubenville and Mingo Junction because of the opportunities for work at the steel mills.

The Census of 1920 included data on country of birth for foreign-born people in cities of over 10,000; Steubenville was one such city, of course, its population now standing at 28,508. It is unfortunate that the census-takers did not decide to do this several decades earlier.

Figures for Jefferson County were included, as before. Total county population was 77,580, so Steubenville had about 37% of the county's

population. The foreign-born population of the county was 15,160, or 19.5% of the total, of these, 7,330, or just under half, were listed as "East European-born." Steubenville had 5,581 foreign-born people (always listed in this and other censuses as "foreign-born white"). Thus Steubenville contained 37% of the foreign-born population of the county, and the foreign-born population of Steubenville was 19.5% of the total city population, the city population exactly mirrored the characteristics of the entire county's population, except that Steubenville had about 1/2 of the county's black population (1,115 out of 2,063).

The census included 21 countries of birth, as well as "all other countries," and showed the number of Steubenville residents from each:

Austria	198
Canada	43
Czechoslovakia	92
England	337
Finland	15
France	9
Germany	308
Greece	176
Hungary	333
Ireland	185
Italy	1,754
Jugo-Slavia	456
Lithuania	57
Poland	723
Rumania	30
Russia	336
Scotland	196
Sweden	42
Switzerland	20
Syria	12
Wales	182
All other	77

Between the 1910 and 1920 censuses, the county population had risen by just over 12,000 (18.5%; Steubenville had grown by 27.3% in the same period), but the foreign-born county population had gone from 14,584 to 15,160, an increase of only 576 people (4%) in ten years. In Steubenville, the peak year of immigration was 1913. The interruption of immigration from 1914 to 1918 because of World War I would have been a primary cause of this sudden slowdown, followed by the American backlash against immigration immediately after the war.

From the 1910 and 1920 census figures, it can be seen that Steubenville had just under half of the county's Italian immigrant population (1,754 of about 3,600 people), but only modest percentages of the Austrian, Hungarian, and Russian people (this assumes that, given the small change in county immigrant population between 1910 and 1920, the country-of-origin figures also stayed largely the same). Thus, while Steubenville and the county had the same proportions of immigrant population, the ethnic mix was not the same. Italians clustered more in Steubenville; eastern Europeans largely went elsewhere in the county, most likely to Mingo Junction.

## **Blacks**

As the census figures showed, the county's black population was small but had grown since 1910 (from 1,647 to 2,063). From about 1/3 of the county's black population, Steubenville had grown to having half by 1920. Much more growth was to come, beginning in the late 1920s.

## **Italians**

This was the largest immigrant group in Steubenville in this period, representing 31.4% of the city's foreign-born population in 1920 (1,754 out of 5,581 people), after immigration had reached its peak. By this time, native-born Steubenville residents of Italian background, which had numbered 1,040 in 1910, would have increased also, so the city of 28,508 in 1920 could easily have been at least 10% to 20% of Italian descent.

Italian immigration to Steubenville began in the late 1880s and was based in part on the "padrone" system, in which the padrone went to Italy to recruit men for jobs, arranged transportation for them to the U.S., contracted with employers for the immigrants' laborer, and made a profit on each immigrant. The men usually came alone and saved as much money as quickly as possible in order to send for their families to join them.

Between 1908 and 1914, almost half of the Italians who arrived in Steubenville (80% male) later returned to Italy. This high turnover, with constant new arrivals replacing departing people, made upward mobility for Italians difficult: a large number at any given time were just "starting out" and had not achieved high skill or seniority levels.

Language, of course, was a major barrier to assimilation and to getting skilled jobs. Some Italian immigrants, eager for a fresh start, made strenuous efforts to assimilate: they anglicized their names or willingly accepted new versions of names imposed by immigration officials; they learned English as quickly as possible; and they moved up from the unskilled and semi-skilled work force into sales, managerial, and entrepreneurial positions as fast as they could. These people were anxious to leave the Old World behind and become part of the New, even to the extent that they avoided extensive social contact with other Italians. Other immigrants, in contrast, were in less of a hurry to "become American": they found and stayed in unskilled or semi-skilled jobs, learned English slowly if at all, were content to live in the city's Italian enclaves, and saw no reason to change their names to more "American-sounding" ones. These differences seem to have been based entirely on the personality of the individual rather than regional or cultural conditions in Italy.

The result was that, while some Italian immigrants moved out into the community and assimilated quickly, others did not, and Italian neighborhoods were a feature of Steubenville life. This was true not only of Italians but also of other immigrant groups, even those of fairly small size, that experienced language, cultural, or social barriers in the United States. There was a natural tendency for people from the same country to cluster in neighborhoods where some semblance of their old lives could be maintained and the social network could provide support and even refuge. This reduced the stress and difficulty of adjustment to life in a new and

sometimes strange country; as time passed and the immigrant community became more assimilated, such neighborhoods became less recognizable as enclaves of particular nationalities.

By far the most popular Italian neighborhoods in Steubenville were the 300 and 400 blocks of South Sixth Street, the 100 and 200 blocks of North Sixth and the 200 block of North Seventh; on the west side of the downtown, on Adams and Market; and to the south on Lincoln Avenue. In these areas were a number of boarding houses catering to single men who had not yet been able to bring their families from Italy or who had not yet married and started a family.

Today most of the old Italian neighborhoods no longer exist, the victims either of extensive clearance or of demolition and new housing construction, such as along South Sixth Street. Some houses formerly occupied by Italians survive, as do some individual commercial and church buildings in the neighborhoods and in the downtown area.

St. Anthony's Church was established to serve the Italian population; its cornerstone was laid in 1910. The city had a few Italian Protestants, who established the Church of Our Savior in 1920; it still stands at 520 South Street. Prior to building their own structure, the congregation met at the Presbyterian Italian Mission at 343 South Sixth Street.

Italian language newspapers also served the city's Italian population. They included Il Telegraphio Marconi, a weekly published by V. Morelli at 120 North Sixth Street, the longest-lived of these papers; Il Gazzetta di Steubenville; Il Corriere di Steubenville, a weekly published by G. Mazzi at 211 Market Street in the 1920s; and Il Messaggero, published in the mid-1920s at 317 South Sixth Street. Social activities for the Italian community were provided by the Sons of Italy, whose Cristoforo Colombo (Christopher Columbus) Lodge met at South Fourth and Slack Streets.

Italian-owned businesses tended to serve mainly the Italian population. These businesses included a large number of grocery and produce shops, saloons (sometimes combined with boarding houses), and foreign banks. These banks were an interesting part of immigrant Italian life, providing foreign exchange and transatlantic steamship tickets to facilitate bringing families to the U.S., returning to Italy on a seasonal basis, or emigrating back to Italy permanently. Joseph Ventolo ran a foreign exchange bank, together with a steamship agency, between 1907 and 1921 at 156 North Sixth Street. Ventolo was superseded in 1921 by the rival Morelli Bank at 118-122 North Sixth, and by the new Antonucci State Bank at South Sixth and South Streets. The Antonucci bank building, the adjacent Testa's Market, and the former Italian Protestant church building, all clustered at South Sixth and South Streets, still survive and represent the former heart of Steubenville's Italian community.

Before 1910, very few Italians had entered professional life. There was one Italian doctor in 1906, and then none until 1911. Other professionals included a druggist in 1909 and an attorney in 1915. By 1915, few Italian-owned saloons survived. By 1920, there had been a significant increase in the number of Italian managers, craftsmen, and skilled manufacturing operatives; these were people who had been in the city and at work long enough to gain new skills and rise in the ranks of the employed;

there was a concomitant decrease in the number of Italians working as laborers. Most of the skilled operatives were employed either at LaBelle Iron Works or Weirton Steel.

### **Greeks**

Greek immigrants came to the U.S. in part by means of the padrone system. Steubenville's Greek population never was very large (fewer than 200 in 1920), since the principal Greek settlement in the area was across the river in Weirton, WV. Most Greek immigrants worked in the steel mills on both sides of the river.

The Greek social organization was the Pan Hellenic Union, located in Weirton.

### **Poles**

Poles were difficult to identify as an immigrant group prior to World War I, as was noted in the discussion of the 1900 census results. The 1920 census, however, listed more than 700 immigrants from Poland living in Steubenville. As did other groups, the Poles clustered in their own neighborhood, generally the hilly part of Steubenville's downtown area above North Seventh Street, on North Eighth, North Ninth, and Highland Avenue.

St. Stanislaus Church, the cornerstone of which was laid in 1915, was the heart of the Polish community and still dominates the former Polish residential area. Prior to construction of St. Stanislaus, Poles attended St. Peter. Services at St. Stanislaus were in Polish, and its school taught the Polish language until 1954.

Polish immigrants generally found industrial employment in the steel mills, coal mines, and on the railroads. The Polish National Alliance was a social organization which still exists today and occupies a modern building on North Seventh Street; but no information about its original location has been found.

### **Russians**

Russian immigration to Steubenville began about 1900. In 1910 there were 830 Russians in Steubenville and nearly 2,000 in Jefferson County. The Russian population in Steubenville had declined by 1920 to 336, so they were not a major component of the city's immigrant population by that time.

Russians in Steubenville lived on South Eighth and South Ninth Streets, with some on North Ninth and North Tenth. Catholics attended St. Stanislaus in the Polish neighborhood, while Orthodox Russians went to a Russian Orthodox Church on North Fourth Street. Holy Transfiguration Church was established in 1914, and the current building at Washington and North Tenth dates from 1915. There was a Russian Orthodox Catholic Aid Society, but no location for it has been found.

Russian Jews attended B'Nai Israel on South Fifth Street and later Temple Beth-El, which organized in 1920 and built on North Fifth Street.

## **Austrians**

Though a fairly sizable immigrant group in Jefferson County (more than 2,700 in 1910), Austrians were relatively few in Steubenville (316 in 1910, 198 in 1920). They did not have their own immigrant "colony" or neighborhood like other groups, but they did generally live in the city's south end. They were generally employed in the steel mills, coal mines, and on the railroads.

## **Serbians**

Prior to World War I, Serbians were listed as coming from Austria-Hungary. One secondary source states that 456 lived in Steubenville in 1920; this matches the figure for "Jugo-Slavia" in the census reports, and these undoubtedly refer to the same people. Serbian immigration began as early as 1890, but the greatest influx was between 1900 and 1912.

Wells Street, in the city's far south end next to the steel mill, was known as "Little Belgrade," and most of the Serbian population worked at the mill. The Serbian and Italian neighborhoods in the south end abutted each other, and there was occasional violence between the two groups. Today the unused Serbian-American club building still stands adjacent to the mill entrance.

## **Slovaks**

92 people from Czechoslovakia (a newly-formed country at the time) were enumerated in Steubenville in the 1920 census. In Steubenville they generally lived in the area around Prospect, Poplar, and Spencer Avenues, south of Lincoln Avenue and west of the Panhandle Railroad main line. Their primary employment was in the steel mills and coal mines.

Slovak immigrants attended the Greek Catholic Church in Mingo Junction, or Holy Name Catholic or Finley Methodist in Steubenville. St. John the Baptist Slovak Church on Lincoln Avenue was dedicated in 1926. The Slovak citizens of Steubenville had no organized social clubs.

## **Other Immigrant Groups**

By 1920, Steubenville had other small groups of recently-arrived immigrants, as well as some of long standing that had declined in size because of cessation of immigration and the deaths of first-generation immigrants. There were, for example, 57 Lithuanians, who lived in the south end but not in a cohesive neighborhood. They generally worked at LaBelle Iron Works and attended Holy Name Church or St. Stanislaus; 30 Romanians lived in the south end and worked at LaBelle Iron Works, there were 42 Swedes; and 48 Canadians, survivors of an immigrant group that never numbered over 100, still lived in the city, mostly employed as glass workers.

308 German-born immigrants lived in Steubenville in 1920 (in 1910 the entire county had 818). Immigration from Germany had dropped off in the 1880s, so German-born Steubenville residents by 1920 were increasing in age and declining in numbers at a gradual pace, unreplenished by new immigration. Among many Germans who had achieved commercial success were Friedl Lambert, who during the 'Teens had a foreign bank at 339 Market Street, and Joseph Niederhuber, who ran the Steubenville Germania Press at

211 Market. His newspaper was discontinued during World War I because of anti-German sentiment.

Chinese immigrants appear to have been the only Asian immigrants in Steubenville in this period, and they never were large in number. Chinese immigration to the U.S., most of which took place on the west coast, was always a controversial issue that seemed to bring out powerful racist attitudes in otherwise liberal-minded people. The United Mine Workers of America, for example, in this period welcomed members of all nationalities but specifically excluded the Chinese and Japanese, the stated reason being these groups' willingness to work for much less than "white men" and thus representing an economic threat. In Steubenville, as early as 1880, Chinese immigrant Hop Wah was listed as having a laundry, and through the years maps of the city showed the locations of several "Chinese laundries" (presumably owned and operated by immigrants). Chinese residents in Steubenville never appeared to number more than just a few at any given time.

## **Steubenville Immigrants and Ethnic Groups**

### **1920-1940**

In 1920, Steubenville's population had reached 28,508, with 5,581 of those people being "foreign-born whites." By 1930, some significant changes had occurred. The city's population had increased 24.3% to 35,422, while the foreign-born white population had declined to 5,167 (down by 7.4%). As has been noted, the decline in foreign-born people meant that older people were dying and not being replaced by new arrivals; immigration, now subject to significant restrictions on the number of people allowed into the U.S., was having less and less effect upon the city's ethnic makeup. At the same time, by 1930 the native-born offspring of foreign-born people, and even the offspring of those offspring (the third generation), had increased greatly. Though no figures were found in the 1930 census, the result was that Steubenville was still a strongly "ethnic" city, with identifiable nationality groups, and neighborhoods identified with specific races and countries of origin.

1930 census figures for Jefferson County showed an increase in total population to 88,307 (thus Steubenville's population was now 40% of the county population), but little change in the number of foreign-born people. This was consistent with Steubenville's experience.

The more significant story was the growth of Steubenville's black population. The increase was much greater between 1920 and 1930 (from about 1,000 to nearly 2,800) than between 1910 and 1920 (the increase for this period was from about 500 to about 1,000).

This was due to an ongoing need for semi-skilled and unskilled labor in the city's industrial plants. With immigration restricted, employers actively recruited blacks in the south (one source referred to blacks being "imported" to work in the city's industries) in order to meet the need for workers.

Census figures for 1940 illustrated the continued effects of limited immigration and, even more importantly, the effect of the Depression of the 1930s. The city's population increase was only 6.3% over the decade, to 37,651. The black population totaled 2,814, a negligible increase over the 1930 figure.

By 1940, foreign-born Steubenville residents had declined in number by 11% to 4,601; this figure showed the continuing effect of the deaths of older immigrants and the lack of new ones. 41% of Jefferson County's 11,233 foreign-born white residents lived in Steubenville at this time. At 1,684, Italians were more than three times the number of those from any other country; and Steubenville was home to just about half of the county's Italian foreign-born population.

In 1940, the countries of birth for foreign-born Steubenville residents included the United Kingdom and Ireland (816, with only 58 from Ireland); Germany (145), Poland (504); Czechoslovakia (122); Austria (154); Hungary (127); Yugoslavia (279); and Russia/USSR (187). The figures for an additional 24 countries were well under 100 for each.

The picture in Steubenville in 1940 was of a city with a strong ethnic background but whose foreign-born population was in decline and whose residents of foreign background were becoming assimilated into the larger society. Blacks had become the largest ethnic group but were all native-born. Poles and Italians were the largest, most cohesive European immigrant groups, with Yugoslavia (whose people were still mainly called Serbian) not far behind. The war years which were immediately to follow, with their emphasis on patriotism and pride in being American, hastened the process of assimilation so that, even for the large immigrant groups, national identity became less and less important and quickly faded away.

### **Blacks**

Steubenville's black population was scattered throughout the downtown area, with some concentration on South Sixth, South Seventh, and South Eighth Streets, intermingled with the Italian population. There was no real segregation of blacks and whites into separate neighborhoods.

Black Masonic and Knights of Pythias lodges met at 216 Market Street. Phillips Chapel and Quinn A.M.E. Church were the primary places of worship.

### **Italians**

Early in the 20 years between 1920 and 1940, Italians and the south end of Steubenville were nearly synonymous. As one reminiscence put it, "The Pottery was in the South End . . . completely fenced in by a boisterous Italian colony, where English was seldom heard and you saw names like Antonucci Brothers, Luigi de Gesualdo and A. DiSimone on grocery windows."

As they became assimilated and achieved economic success, there was a gradual movement of Italians away from the downtown area and to the hilltop area. There was movement within the downtown area, too, as Italians moved from the south end to the more desirable north end. However, this came later; as late as 1931 there were still no Italian homeowners in the 200 to 300 blocks of North Third, Fourth, and Fifth Streets, and the 500 to 800

blocks of North Sixth Street. In 1931 almost 20% of the Italian population in Steubenville owned real estate; this would increase later with the move to the hilltop area.

### **Serbian**

Serbian enclaves formed around the steel mills and mines in the Tri-State area, with concentrations in Steubenville, Weirton, West Virginia, and Mingo Junction. Six hundred Serbian families lived in the area, 400 of them in these three communities.

A church was organized in 1906 and purchased a Presbyterian church in Mingo Junction, which was remodeled into the Eastern Orthodox style. This was followed by purchase of a parish house in 1917. Loss of employment due to mill shutdowns in Mingo Junction led to a reduction in that Serbian population there, and the decision was made to move the church to Steubenville early in the 1940s. Work on a new church was delayed by the war, but began in 1947 at 630 North Fourth Street. The church was completed the next year and remains in use. The Holy Resurrection Serbian Orthodox Church attracted people from Mingo Junction, Toronto, Cadiz, St. Clairsville, Cambridge, Columbus, Barnesville, and other Ohio communities, as well as Weirton, Wheeling, and Follansbee, West Virginia.

Peter Radakovich, religious and social leader of Serbs in the Ohio Valley, was born in 1882 in what became Yugoslavia. He settled in Steubenville in 1913, where he was a boiler engineer for Wheeling Steel for 44 years. He also was president of the Serbian church congregation and was instrumental in the the decision to move to Steubenville. Other prominent Serbians included the owners of Kvocka's grocery on Wells Street in the south end, and homebuilder Malbasa Construction Company.



Quinn Memorial A.M.E. Church symbolizes the strong religious traditions that helped Steubenville's immigrants and ethnic groups adjust to a new life.

## ASSOCIATED PROPERTY TYPES

The property types associated with the downtown residential areas of Steubenville are described in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910." All of the city's ethnically-associated property types are located in these same downtown neighborhoods and are largely indistinguishable, physically and architecturally, from previously-identified downtown neighborhood property types. What does distinguish the property types associated with the city's ethnic history, however, is their association with and use by ethnic groups. The following discussion of these property types and their registration requirements places primary emphasis on this ethnic association.

### Single-Family Residences

Single-family residences associated with Steubenville's ethnic population were largely built between 1890 and 1910; relatively few date from before or after this period. There were no concentrations of building designs typical of any particular groups; that is, no ethnic groups brought indigenous foreign designs with them. The houses they built or moved into in Steubenville were as much a part of the American architectural mainstream as those in neighborhoods without ethnic associations.

If the single-family residences of ethnic neighborhoods could be said to vary at all from the mainstream, it would be in their general modesty. High-style houses were virtually non-existent, and those with high-style design elements or features were fairly rare. The great majority of single-family residences in the ethnic neighborhoods were vernacular structures, largely of frame construction.

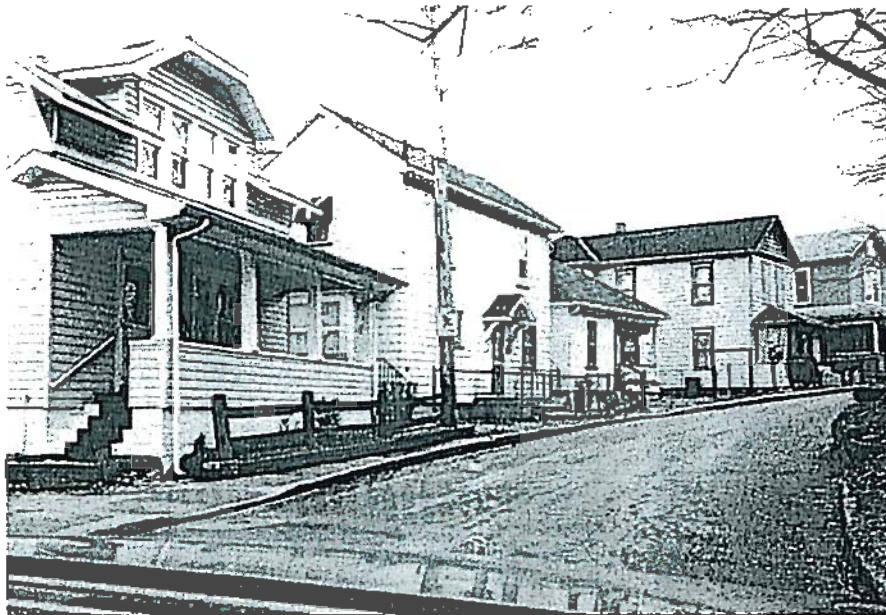
These vernacular structures included one-story, one-and-a-half-story, and two-story houses. They typically were rectangular or square in form, or sometimes L-shaped, and had gable roofs. Window and door openings were plain and unornamented. Windows were almost universally of the double-hung type, typically two-over-two or one-over-one. Doors were plain and usually simply paneled, generally without glazing.

Though these buildings typically were not built in architectural styles or with elements of these styles, their massing, form, and roof shapes sometimes followed those of styles current at the time they were built. Those from the Italianate period, for example, tended to have tall, vertical proportions, with vertically-proportioned windows and fairly low rooflines. Later houses from the Queen Anne period tended to be more square, with shorter, squarer windows, and the rooflines tended to be steeper and more prominent. Building massing and plans likewise followed the conventions of these styles, with Italianate-era houses generally rectangular or L-shaped and Queen Anne-era houses square or irregular in plan.

When houses did have some stylistic elements, these were generally confined to porch ornamentation, window and door surrounds, or gable ornamentation.



The Slovak neighborhood near Lincoln Avenue retains much of its form from the early 20th century, when eastern European immigrants settled here.



The Spencer/Prospect area on the hillside near Lincoln Avenue was another area where eastern European immigrants and families lived.

## Significance

The single-family residences of Steubenville's ethnic neighborhoods may be significant under Criterion A in the field of ETHNIC/IMMIGRATION.

These downtown neighborhoods were located close to the city's commercial heart, but, more importantly, they were located within walking distance of major places of employment at the mills and factories that surrounded and were interspersed throughout the downtown area. These establishments, which are fully discussed in the context entitled "Industrial Development in Steubenville, 1847-1959," were the primary places Steubenville's immigrants found work.

As Steubenville's industrial base and its population grew rapidly in the late 19th and early 20th centuries, many of its streets were built up or rebuilt with new housing. Much of this occurred in the south end, along the west side of the city west of Seventh Street, and along Lincoln Avenue. These were the principal areas to which working-class immigrants gravitated when they arrived in the city, joining others of their nationalities who had come earlier. As in other cities, Steubenville tended to have clusters or enclaves of ethnic groups, as discussed in the context narrative above, especially among people for whom English was not their native language. In many cases, single male boarders stayed with families in single-family houses, living modestly and saving money until their families could join them and they could rent or buy their own homes.

Unfortunately, many of the ethnic neighborhoods of Steubenville no longer exist. Some have been cleared and rebuilt with new housing, as had happened in the old Italian area along South Sixth and South Seventh Streets. Other areas have not been entirely cleared but have experienced considerable demolition and property deterioration. Others, such as the former Polish neighborhood around St. Stanislaus Church, have survived fairly intact.

Single-family homes may be significant under Criterion A through their association with Steubenville's ethnic groups, by having been owned or occupied by members of an ethnic group in an area historically associated with that group. Preference should be given to listing of clusters or districts of such homes over listing of isolated structures, because districts or clusters give more of a sense of neighborhood cohesiveness that was the primary characteristics of the city's ethnic neighborhoods.

## Registration Requirements

To qualify for listing under Criterion A, a single-family residence should have the following aspects of integrity:

### 1. LOCATION

Refer to the discussion of location in the introduction.

### 2. DESIGN

Vernacular buildings should retain their basic form, roof shape, window and door openings, and interior plan. Original porches should be intact,

but loss of a porch should not keep a property from qualifying if its other major features are intact.

If the building was built with some stylistic treatment or ornamentation, this should be largely intact, though loss of some of these features should not keep a property from qualifying.

### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between the property and the ethnic group or groups that occupied and used the property. This relationship is strongest in districts or clusters of properties occupied or used by the same group or groups.

#### **Multi-Family Residences**

##### Description

This property type includes doubles and duplexes. There were some examples of commercial buildings with apartments on the second floor; these are discussed below in the Neighborhood-Scale Commercial Structure property type. Apartment houses were not typical of Steubenville's ethnic neighborhoods.

Double houses were side-by-side structures, where each half of the building was a separate living unit. Duplexes were divided up and down, such that a single building could have either two or, in a double duplex, four single-story apartments. These buildings were almost always of frame construction, though some brick examples are extant, and their designs were almost always vernacular. Rather than being built in architectural styles, these buildings, when ornamented, tended to employ only stylistic elements or ornamentation derived from various styles. These might include Italianate cornice brackets, Queen Anne patterned siding, or ornamented window lintels and sills.

Like most residential buildings associated with Steubenville's ethnic groups, the multi-family structures were built between 1890 and 1910, with few examples from before or after that period. They generally were not much larger than single-family houses, since they typically were built on single lots. In building form and massing, roof shape, and window and door patterns and details, the multi-family residences were much like the single-family.

##### Significance

Steubenville's ethnic neighborhood multi-family structures are significant under Criterion A in the field of ETHNIC/IMMIGRATION.

Under this criterion, these structures may be significant for representing the social life and economic status of Steubenville immigrants and ethnic groups in the late 19th and early 20th centuries. In this period, as large numbers of immigrants flocked to the city in search of unskilled and semi-skilled jobs in the city's mills and factories, the construction of



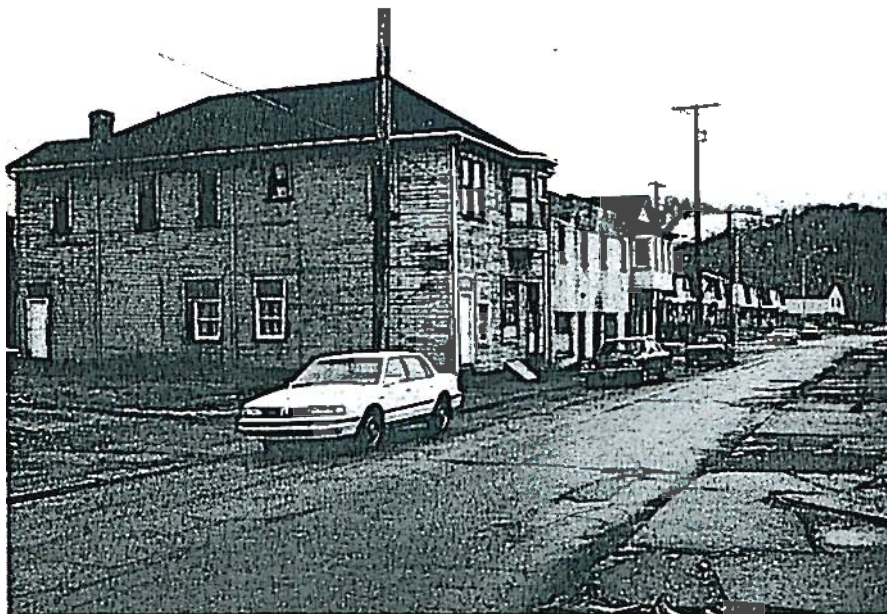
This view on Lincoln Avenue looking east toward the railroad overpass shows some examples of multi-family housing commonly found in immigrant neighborhoods.



On South Sixth Street between Slack and South, there are both single- and multi-family structures in a traditionally Italian neighborhood.



South Street in the Italian neighborhood had numerous doubles and other multi-family houses.



South 6th Street, though it has lost much of its physical fabric, still retains some multi-family housing typical of this early 20th century Italian neighborhood.

housing kept pace. While much housing was single-family, there was a need for multi-family housing as well. Most immigrant workers had to work for some time before they could afford to rent or own a single-family house (many never could achieve this), so they rented rooms or apartments in doubles or duplexes. It was not unknown, in fact, for as many as eight or ten unrelated people to share a single such apartment.

#### Registration Requirements

To qualify for listing under Criterion A, a multi-family residence should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain its basic form, massing, roof shape, window and door openings, and plan. Any decorative or stylistic treatments should remain largely intact, although loss of some of these features should not keep a property from qualifying. Original porches should survive, but loss of a porch or porches should not keep a property from qualifying if other aspects of the design are intact.

##### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between the property and the ethnic group or groups that occupied and used the property. As with single-family residences, this relationship is strongest in districts or clusters of properties occupied or used by the same group or groups.

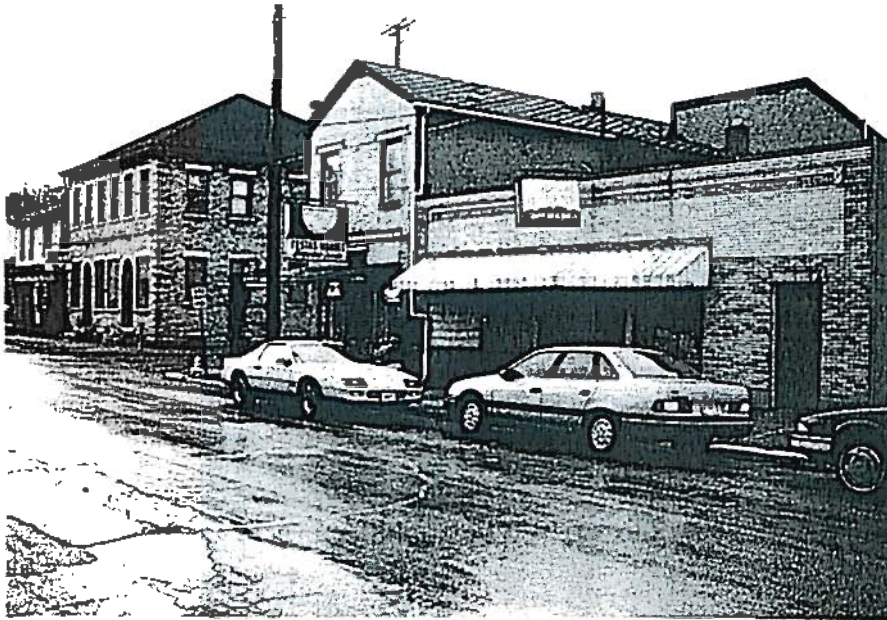
### Neighborhood-Scale Commercial Structures

#### Description

As Steubenville's ethnic neighborhoods grew rapidly in the late 19th and early 20th centuries, they matured into stable residential areas. To meet the day-to-day needs of the residents, small neighborhood-scale commercial enterprises began to appear, and they increased in number as the population grew. Many of these were small retail stores such as bakeries, produce and meat shops, groceries, and saloons, all intended to serve a local population and with a trade area of no more than a few square blocks.

Other commercial enterprises were geared directly to the needs of an immigrant population. These included the foreign banks, steamship agencies, and labor contractors' offices that helped immigrants to save and send money, arrange ship passage to and from Europe for themselves and family members, and find work immediately upon arrival in Steubenville.

The buildings housing these enterprises, unlike most of the housing stock in the ethnic neighborhoods, were primarily of brick construction. Usually located at important street intersections or along well-traveled streets,



The corner of South Sixth and South Streets is still home to Testa's Market, and across the street was the Antonucci bank which served primarily an Italian customer base.



The foreground building, a well-preserved example of a neighborhood-scale commercial structure, is in the traditionally Polish neighborhood above (west of) North Seventh Street near North and Highland.

commercial buildings of this type were typically one or two stories in height; the two-story buildings sometimes had office or apartment spaces on the second floor or beside the first floor commercial space, since it was common for the owner of a business to live in an attached apartment.

Commercial buildings in ethnic neighborhoods generally had the form, massing, and window and door patterns of the late Italianate Commercial style that was typical of Steubenville commercial buildings of this period. This included large first-floor display windows; centered or off-center storefront entrance doors; and residential-size double-hung windows on the second floor. Some had ornamented cornices. However, these buildings typically were not built as full examples of the Italianate Commercial style and were not as heavily ornamented as other examples of downtown commercial buildings. Like much of the building stock of the ethnic neighborhoods, the commercial buildings were well-built and substantial, but they were generally small and plain. The former Antonucci and the existing Testa's Market, in the old Italian neighborhood at South Sixth and South Streets, are excellent examples. An exception is the old Morelli building, across Sixth Street from the site of the Pennsylvania Railroad Station north of Market Street. While not built as an example of an architectural style, this building has an unusual ornamented terra cotta facade.

#### Significance

The neighborhood-scale commercial structures of Steubenville's ethnic neighborhoods are significant under Criterion A in the field of ETHNIC/IMMIGRATION.

These small-scale commercial structures housed the businesses started by enterprising immigrants, usually to serve the needs of members of their own ethnic group, but in some cases to serve the entire community. Such buildings represent the immigrants' efforts to achieve economic success in a new land, taking advantage of the unlimited opportunity and freedom they saw the United States as offering. As points of commercial activity and social interaction, these businesses served as anchors for their neighborhoods and lived in a mutually beneficial relationship with them.

As noted in the "Downtown Neighborhoods" context, the ethnic neighborhood commercial buildings are significant as indicators of the growth, maturity, and stability of those neighborhoods. As part of the fabric of the neighborhoods, the commercial buildings are components of the broad patterns of the city's historical development.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

1. LOCATION

Refer to the discussion of location in the introduction.

## 2. DESIGN

The property should retain sufficient structural, finish, and stylistic features to identify it as a neighborhood-scale commercial structure associated with an ethnic neighborhood in the period between 1890 and 1910. This includes retention of historic storefronts, including display window, bulkhead, transom, and entrance; upper-floor window openings; historic exterior surface materials; and any historic ornamentation. The property's basic form, massing, and roofline should be substantially intact.

## 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between the property and the ethnic group or groups that occupied and used the property. For neighborhood-scale commercial structures, properties may qualify for individual listing because of their association with ethnic group(s), or they may qualify as part of clusters or districts of residential or other structures associated with ethnic group(s).

### **Churches, Rectories/Convents, and Schools**

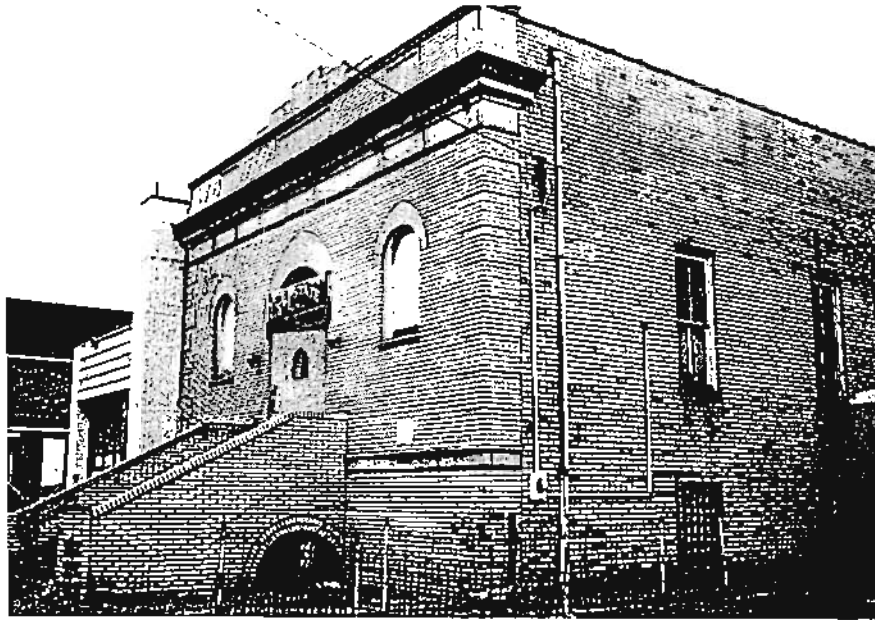
#### Description

Churches and associated structures were essential to Steubenville's ethnic neighborhoods, since virtually every arriving ethnic group brought with it strong religious traditions; establishment of a place of worship was, after finding work and a home, the first order of business for most immigrant groups, even those with a fairly small number of people. Continuation of the religious traditions of their native lands was a primary means by which immigrants coped with the stresses of trying to make a new life in a new country.

Some groups worshipped in churches used by one or more other groups. St. Peter, for example, served both Italian and Irish Roman Catholics, as well as Catholics of other nationalities. Though St. Peter has long been associated with various ethnic groups, this was primarily because it was the oldest Catholic congregation; as immigrant groups grew and became established, they eventually began to form their own congregations and build their own churches. St. Anthony, for example, was established in 1910 as the Italian Catholic church, and St. Stanislaus was built in 1915 as the Polish Catholic Church.

Although extensive description of Steubenville's downtown historic churches can be found in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910," it is the later single-nationality churches that are the focus of this current context. Such churches, located in the midst of former ethnic neighborhoods, today are often the most visible remnants of those neighborhoods and stand as reminders of their ethnic history. For example, it is not obvious from looking at the residential and commercial buildings surrounding St. Stanislaus Church that this was the old Polish neighborhood; however, a visit to the church makes it clear that it was indeed the heart of that group's neighborhood.

These churches, and their related structures such as rectories, convents,



The Church of Our Savior, a rare example of an Italian Protestant church, is in the heart of the old Italian neighborhood around South Sixth and South Streets.



St. Stanislaus, at the northwest corner of North and Highland, consisted of the church, rectory, and school. It was the heart of the surrounding Polish neighborhood.

and schools, were built at various times, but most date from the period between 1890 and 1915. Some, such as the Serbian Orthodox Church on North Fourth Street, date from as late as the 1940s.

Steubenville's ethnic churches were almost entirely of brick construction (the Russian Orthodox Church was frame). Their size, stylistic treatment, and level of ornamentation were indicative of their congregations' economic means at the time of construction. Generally these buildings are plainly designed. Although most are clearly Gothic in character, they are not full-blown examples of one of the Gothic revival styles. Instead, they employ Gothic features such as steeply-pitched gable roofs and pointed-arch window and door openings. Ornamentation, usually in brick or stone, tends to be very restrained and used sparingly. Some churches have unique features symbolic of or traditional among their national groups; examples are the "onion" dome of the Russian church and the "Eastern Orthodox" dome of the Serbian church. Others are so unassuming that they can be overlooked as churches; the Italian Protestant Church of Our Savior on South Street is an example. A feature shared by most of these churches is an elevated main entrance which requires climbing a flight of stairs to enter the building. Often associated with ethnic groups' churches were artwork or religious symbols derived from the groups' homelands; or, as in the case of St. Stanislaus, inscriptions in native languages were sometimes placed in prominent exterior locations.

Sometimes associated with the churches were rectories, convents, or schools. In some cases these were designed to complement each other in materials, style, and overall appearance (St. Stanislaus is a good example); in others, a house adjacent to the church served as the rectory.

### Significance

Steubenville's ethnic neighborhood churches and associated buildings may be considered significant under Criterion A in the field of ETHNIC/IMMIGRATION. They symbolize the stability permanence, and worldly success that the city's immigrants and ethnic groups sought in coming to Steubenville.

Groups only recently arrived in Steubenville usually attended an established church of their denomination, often together with other nationalities and groups. Some groups preferred to establish their own separate congregations, worshiping in rented or borrowed space in other groups' churches or in non-religious buildings. In almost every case, however, it was the goal of each national or ethnic group to have its own church.

Most groups sooner or later built their own churches; others purchased existing church buildings and converted them to their own use. In any case, the city's various nationalities and ethnic groups did achieve their aspirations, sometimes in only a modest way and sometimes more grandly. Their churches became the focal points of their neighborhoods and were a means of maintaining traditional ties to these groups' birthplaces even as the groups became more and more assimilated into the life of the city.

Schools associated with these churches also were symbolic of the goals and aspirations of the groups that built them: there was a strong desire to maintain traditional religious and social links while at the same time providing the education that most immigrant and ethnic groups saw as essential to success. St. Stanislaus School is an example, where

neighborhood children received an education, yet the Polish language was taught until the 1950s.

#### Registration Requirements

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain its basic form, roof shape, massing, window and door openings, and interior plan. The original level of ornamentation should be intact, although loss of ornamentation should not prevent a property from qualifying for listing if the other essential features are intact. Original entrance steps and main entrances should be intact; some may have been altered by the addition of canopies or vestibules, but such alterations should not prevent a property from qualifying for listing if the steps and entrances are substantially intact. Original features such as towers, steeples, or domes should remain intact.

##### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between the property and the ethnic or national group that built or used it. The property should retain the important design features that were in place during the time it saw its greatest use by the related ethnic or national group, even if the actual neighborhood served by the property no longer exists or if the property's primary users no longer live in the immediate neighborhood.

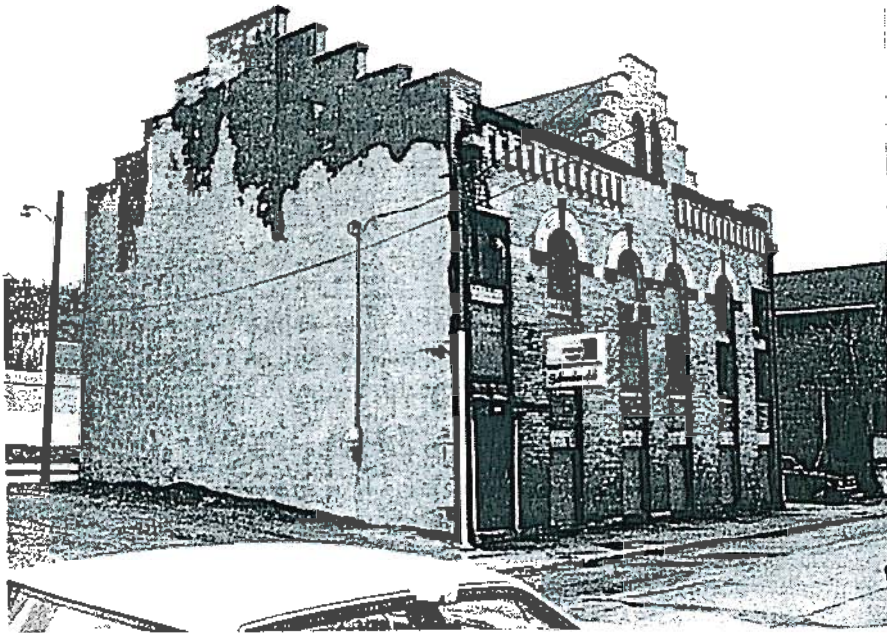
### **Social Clubs**

#### Description, Significance, and Registration Requirements

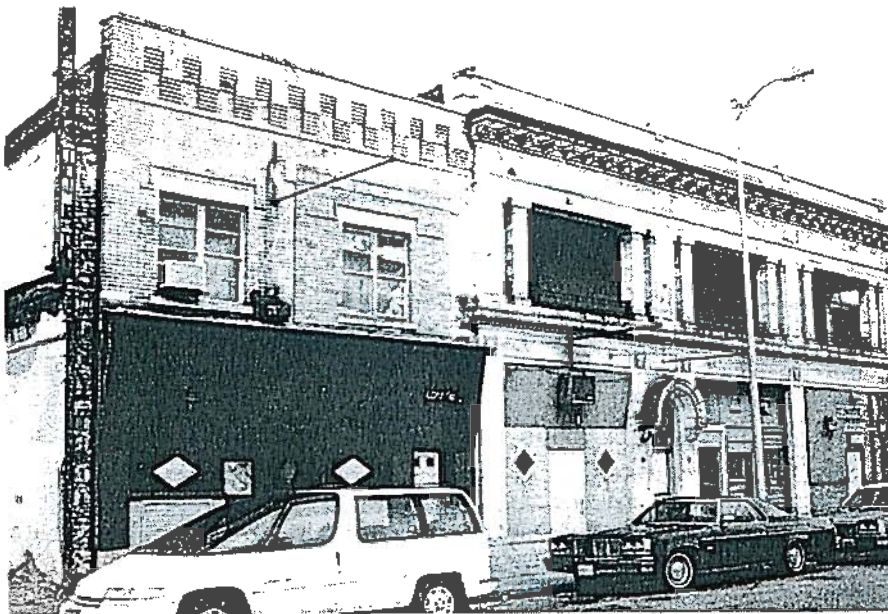
This property type is fully treated in the context entitled "Development of Downtown Neighborhoods in Steubenville, 1870-1910."

Based on current research, the Schwaben Club on North Sixth Street appears to be the only surviving ethnic/national social club building built for that purpose (it was built in 1886 for a German organization). One of the most important and largest in the city, Turner Hall on North Third Street north of Washington, dated from 1888. It served the city's German population but appears to have been demolished in the 1920s.

Many social clubs, as was noted in the "Downtown Neighborhoods" context, met in rented space in commercial buildings or were established in converted residences. There appears to be one surviving example of the latter, a Serbian club located on Wells Street in the south end, adjacent to the Weirton Steel parking lot. The club was in what appears to be a converted residential structure. The building has had some alterations, most notably application of siding, and probably elimination of some window openings. It appears not to be in use, and its interior has so far been inaccessible.



The Schwaben Club, located along the west edge of the downtown area, survives as a reminder of Steubenville's German heritage.



The Morelli Block, housing the Morelli Foreign Bank and Steamship Agent (the building to the right), was strategically located on North Sixth Street opposite the Pennsylvania Railroad station.

## POTENTIAL HISTORIC DISTRICTS AND INDIVIDUAL LISTINGS FOR ARCHITECTURAL SIGNIFICANCE

### Potential Historic Districts

In the former ethnic neighborhoods of Steubenville, large-scale demolition and reconstruction have eliminated many square blocks of properties, but two potential districts in the south end have been identified. Further work must be undertaken in order to define precise boundaries, but these districts can be generally described:

1. South Fourth Street: Both sides of South Fourth from Slack Street to just north of the intersection with Route 7. This area, formerly part of the city's Italian neighborhood, has single- and multi-family homes typical of the late 19th and early 20th century houses occupied by Steubenville's immigrant and ethnic groups. In addition to having integrity of location, design, and association, this district possess integrity of setting -- the close spacing of the houses; their location close to the street with modest if any front yards; and their physical closeness to the steel mills that provided employment all contribute to this aspect of integrity. While few if any individual properties in this potential district could be considered individually significant for their architecture, together these properties make up a significant and distinguishable entity worthy of recognition.
2. South End Commercial District: Centered on the intersection of South Street and South Sixth Street. At or near this location are the former Italian Protestant Church, the former Antonucci Bank, and the still-active Testa's Market. Interspersed among them are residential structures typical of the south end, and demolition in this potential small district so far has not resulted in a major loss of integrity. In addition to integrity of location, design, and association, the three major commercial structures here retain integrity of setting -- their original spatial relationships to each other and to adjacent residential properties; their location at the sidewalk edge; and the residential character of the immediately surrounding area all contribute to this aspect of integrity.

### Individual Listings for Architectural Significance

Some properties associated with Steubenville's immigrant and ethnic history may be significant and qualified for individual listing under Criterion C because of their architecture. There may be candidates for listing in each of the property types, including single- and multi-family residences, commercial structures, religious properties and schools, and social clubs.

In addition to integrity of location, design, and association, in order to qualify for individual listing under Criterion C, a property should possess the following aspects of integrity:

#### 1. MATERIALS

The property should retain a majority of the historic materials of which it was built. This should include most of the original exterior surface materials, windows and doors and associated trim, decorative elements, porch elements, and chimneys.

## 2. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For buildings built in architectural styles, for examples, this might include hoodmolds, cornice brackets, window and door trim, patterned brickwork, frieze and cornice details, or decorative features. For vernacular buildings, items of workmanship should include door and window trim, any decorative features, and any porches.

## 3. FEELING

The totality of a property's components should express a specific point or period in time.

# HILLTOP NEIGHBORHOODS IN STEUBENVILLE, 1890 - 1950

## Introduction

One of the most significant trends in Steubenville's development was the uphill movement of much of its population, beginning in the late 19th century and increasing rapidly early in the 20th. This movement was away from the urban neighborhoods directly adjacent to the downtown commercial and industrial districts, and to the hilltops west of the city that overlooked the downtown area and the Ohio River.

As this context will show, the suburbanization of Steubenville was part of and occurred at the same time as the suburbanization of virtually every American city of any size. Facilitated by technological progress (first by the steam railroad, then the electric street railway and the automobile) by government policies, and by the growth of a middle class able to pay the costs of relocation, the diffusion of population and commercial investment outward from the hearts of the nation's cities was to have permanent and significant effects on those cities.

This context studies the landmarks and the districts that survive as representatives of Steubenville's suburbanization during the last decade of the 19th century and the first half of the 20th, and it discusses, within the framework of national trends, the forces that helped create the city as it exists today.

## The National Context

"In the period between 1888 and 1918, when the automobile was still a novelty and a toy, the electric streetcar represented a revolutionary advance in transportation technology. Radiating outward from the central business districts, the tracks opened up a vast suburban ring. . . . By the turn of the century, a 'new city' segregated by class and economic function and encompassing an area triple the territory of the older walking city, had clearly emerged as the center of the American urban society. The electric streetcar was the key to the shift. . . ." This quotation from Crabgrass Frontier, a study of American suburbanization, hints at what was to be both the blessing and the curse of the suburban phenomenon: the move to the suburbs provided what seemed to be the answer to urban ills and problems faced by city dwellers who wanted a better life, but in leaving behind unsolved problems, they found new and unanticipated ones.

Americans have always had a peculiar attitude toward their cities. It seems that as soon as our cities were established, people began scrambling to get away from them. We have always regarded cities with some suspicion, thinking of them as evils necessary to our economic and political well-being, but to be avoided if possible.

It is understandable that the problems of the cities might have seemed intractable and unsolvable. From crowding and poor sanitation, to crime, vice, and dirty air, the pressures put on American cities by immigration and population growth and by the rise of industrial capitalism and the factory system were tremendous and seemed to bring on problems incapable of solution.

The "Suburban Ideal," the vision of a life away from the city, out in a less congested community of green lawns and clear air, was promoted as early as the 1840s by Andrew Jackson Downing. The roots of this ideal go back much farther, to before the founding of the United States -- consider the custom of the propertied class of England of having both a "town house" and a "country house." However, in the U.S. it was around the middle of the 19th century that people became able to act to achieve the ideal.

The railroad made possible this first significant move out of the city. Before this time, of course, cities had their more desirable and less desirable areas. Various parts of a city, over time, fell into or out of favor as the preferred place to live; more often than not, it was the higher elevations within the city to which people aspired.

The railroad, though, permitted establishment of entire new communities, not just movement within an existing one. One of the earliest was in Ohio: Glendale, a dozen or so miles north of Cincinnati, was established in the early 1850s. Centered around the railroad station that enabled business people to commute to and from work in the city, Glendale had large trees on grassy building lots, an attractive curvilinear street pattern, and a comfortable level of insulation from urban people and problems. By the late 1850s, similar suburban development, also facilitated by the railroad, was occurring in the Chicago area, with some new North Shore commuter communities more than 30 miles outside the city.

Even more influential than the steam railroad, however, was the electric streetcar. Electric power replaced horse power on city car lines very rapidly, starting in the late 1880s. The new power source was cleaner, more reliable, and permitted higher speeds than the horse-powered cars, effectively enlarging the radius within which a population could live and still work in the heart of the city. Relatively inexpensive to build, streetcar systems could be extended into undeveloped areas with the assurance that development of those areas would soon follow, and the "streetcar suburb" was born. Unlike the steam railroads, which mainly facilitated creation of entire new communities, streetcars allowed the movement of population to fringe or close-in areas of a city that were suitable for development but lacked reliable transportation in the era before widespread auto ownership.

Other factors also spurred suburban growth. The publication of house plan and pattern books; the availability of Sears and other manufacturers' mail-order houses; the development of balloon frame construction, which made housing construction efficient and affordable; the creation of building and loan associations to finance housing; and expansion of utilities to suburban areas and undeveloped land all worked in favor of new development outside the core city between 1890 and about 1920.

This was a period of democratization of the suburbs, too. The new communities and housing were not intended just for the rich or the upper and upper-middle classes. "Streetcar" suburbs in particular, which in many ways were often as urban in character as the old city neighborhoods, were home to store clerks, low- and mid-level managers, and other middle class people.

The suburbs, then, were still seen as a panacea for urban problems in the early 20th century -- or at least a way to escape those problems -- providing clean air, large lots with yards, and new homes for everyone, working class as well as rich. It is worth noting, however, that while European immigrants were welcomed as participants in the Suburban Ideal, black Americans typically were not -- some suburban subdivisions prohibited sale of properties to anyone other than Caucasians, and blacks generally could not participate in the American dream of home ownership.

The automobile, perhaps more strongly than any other influence, further encouraged suburban growth. The streetcar started the boom, but the auto kept it going and helped it to spread. Ford's Model T, introduced in 1908 and gradually refined and made less expensive in a conscious effort to make it the car for the masses, by 1925 became affordable for much of the middle class. In this period, the growth in auto ownership was astounding: in 1905 there were in the U.S. 1,078 inhabitants per registered motor vehicle; but by 1920 there were 13 inhabitants per vehicle, and by 1930 this had fallen to five per vehicle. Everyone, it seemed, was driving everywhere.

This was the era of the "Good Roads" movement, when federal, state, and local governments undertook hard-surfaced road construction at the urging of motorists (and others) who wanted better driving conditions. This, in turn, encouraged auto production and sales, improving affordability and putting cars within reach of more people.

In response to the auto as the primary means of traveling to and from suburban areas, alleys, garages, and even driveways became standard features of early 20th century neighborhoods. Older streetcar suburbs typically did not have alleys, and lots were narrower because there were no curb cuts and driveways; so as the auto facilitated housing development beyond the range of the streetcars, it also influenced the physical layout of these new neighborhoods.

The subdivision was the basic unit of the 19th and 20th century suburb. These developments frequently were large rural or vacant properties that, depending on the local market, utilities, and features of the land itself, could be divided in various ways into building lots. The goal was to subdivide and sell land that had been annexed into the city or that would be annexed shortly, to ensure availability of city utilities and services. Cities were only too glad to undertake annexation because of the increased tax revenues they anticipated. The typical suburban plan (notable exceptions were the "planned" suburbs which typically targeted a more upscale market) was for a grid pattern of streets to be developed with narrow, rectangular, regularly-shaped lots of consistent size. This was especially true in the more affordable suburbs. Wealthier suburbs tended to have larger lots, occasional landscaped islands, or curvilinear streets to add visual interest.

The introduction of zoning in the 1920s provided for separation of uses, an effort to protect the new neighborhoods from the chaotic pattern of unrelated or "incompatible" uses that bedeviled the cities. Zoning ensured that suburban neighborhoods would stay residential, perhaps with some small-scale commercial development, churches, schools, and parks to provide

amenities, convenience, and some necessities. Industry, larger-scale commercial districts, and other land uses deemed degrading to the new neighborhoods were restricted to their own designated districts.

The early 20th century house was designed with "modern conveniences" such as bathrooms, functional kitchens with indoor plumbing, and hot and cold water. In contrast to the sometimes deteriorated and obsolete housing stock of city neighborhoods, new suburban homes stressed easy maintenance and an improved quality of life, employing features such as ceramic tile walls and floors; central heating; electricity for lighting and appliances; and porches, including sleeping porches, to expand available square footage for living.

### **Additions to the Plat of Steubenville**

The original 600-acre plat of Steubenville, dating from 1851, covered all of the downtown area from today's Stoney Hollow Boulevard on the north, to below Slack Street on the south. The east boundary, of course, was the river, and the original city extended west to about 10th Street.

In 1871 an additional area of 1,076 acres was annexed to the city, a far-sighted move on the part of city leaders of the time, for this huge area encompassed all of the hills and hilltop land that would be developed until the end of the 1920s. Indeed, the next addition, 1,440 acres covering the Brady Estates area and land to the north and west, was not made until 1926, 55 years later.

As a local history notes, the Pleasant Heights/Poplar Springs addition (both names are on plat maps in the county courthouse) ". . . marked the beginning of a definite movement of population toward the hilltops. . . ." On January 4, 1890, Pleasant Heights (the more commonly used name) was recorded with 241 lots on Grandview, Pine, Elm, Walnut, Cedar, Union, and Adams Streets.

After this initial opening of the hilltop area, there was a lull in development until June 8, 1901, when the 180 lots of the Altamont subdivision were recorded. This was located on a hill south of the city, but it never developed because streetcar service was never expanded to serve the area.

The electric streetcar was the key, of course, to hilltop settlement. Steubenville was one of the first to electrify its street railway, in 1888 when the technology was brand new. However, it was not until 1901 that the streetcar line was extended to the hilltop area, on a steep route that ran down South Seventh Street and climbed on a loop around the south end of the future hospital site to Lawson Avenue and eventually to Market Street. This placed large amounts of buildable land within a few minutes' walk of reliable public transportation.

The next significant steps came on August 12, 1902, when the LaBelle View Land Company recorded 746 lots. All streets in this subdivision had a 25-foot building line, except on Wilkins and on Pennsylvania north of Wilkins (the shallow-setback area was for apartment buildings).

On the same day, the Pleasant Heights Improvement Company recorded 205 lots on State, Plum, Orchard, Wilson, McKee, Henry, Maxwell, Lawson, Union, and Cedar Streets. Thus, on a single day nearly 1,000 lots were recorded. This was by far the largest increase in building lots since the original 236 Steubenville lots were platted. Prior to this, subdivisions typically were between 100 and 200 lots at a time.

In October of 1907 the Beall & Steele Subdivision at the northwestern end of LaBelle View was platted; this area also had a 25-foot building line, intended to ensure large front lawns and to reinforce the subdivision's suburban character.

Simmons & Foster's Addition at the northern edge of LaBelle View was platted on July 11, 1913 and included a 50-foot building line along Woodlawn Avenue.

On July 10, 1919 Ross Park Realty Company's plat was recorded. The property had been sold to the company by the Ohio Valley Hospital Association; the plat specified that no double houses, flats, duplexes or other multi-family structures were to be allowed on Lawson Avenue, Ross Park Boulevard, or Cardinal Street.

In 1929, Brady Estates was platted on the old Brady farm land north of Sunset Boulevard. Considerably west of the early hilltop subdivisions, this was the first of Steubenville's suburbs designed for autos only, with no reliance on public transportation. The plat was complete with circles, landscaped islands, and cul-de-sacs that have become standard subdivision features.

McCauslen Manor, where the McCauslen greenhouses once stood, was recorded on July 15, 1929 on the south side of Market, abutting the west side of the Pleasant Heights area. This small subdivision developed around a single circular street with one cul-de-sac.

In 1910 there were 3,935 lots in the city, a growth of some 3,700 lots in the period between 1890 and 1910.

### **Pleasant Heights**

A driving park, or race course, was developed by the Pleasant Heights investors, and it was considered one of the best in the country. The park was used for driving and bicycle races, for one successful county fair, and for circuses.

The 1902 atlas shows the racetrack in Pleasant Heights, approximately where the Ross Park subdivision is now, the land apparently was sold for that subdivision.

The 1901 streetcar line extension to the hilltop area, which made it truly feasible to buy and build in the already-platted Pleasant Heights, was promoted by George Maxwell and George Henry. It ran from Fourth and Adams Streets to Sixth Street, then on South to Seventh, then up Wells Hill to Pleasant Heights and the racetrack. The racetrack and a ball park were the bulk of the line's trade at this time, and the line lost money for several

years until it was extended to the gates of Union Cemetery. Even then it still was not very profitable until LaBelle View started to develop, bringing sufficient new riders to the line to turn a profit. Grover Robb was a motorman on the Hilltop Line, and he had the honor of driving both the first and the last hilltop cars; the last car ran in 1938. Robb lived in the hilltop area on Euclid Avenue.

The first Pleasant Heights houses were built on Park Street and Grandview Avenue. According to a local historian, "The original folks on Pleasant Heights were mostly Welsh, English, Scots or Germans and were skilled laborers in the mills springing up." Clearly the new residential areas of Steubenville were not intended entirely for the well-to-do businessmen and industrialists, but also for working class people.

D. H. Sinclair, Chairman of the Water Works Board, remembered requiring that land above the Adams Street reservoirs park ground, where the engine house was located, be deeded to city in exchange for water lines in the new subdivision area; this was an example of the quid pro quo that often occurred between government and private interests that facilitated suburban development.

The first school in Pleasant Heights built before 1910, indicating that population there had grown enough to justify provision of education to residents' children, the McKinley School started as a brick building on Maxwell Avenue near Plum Street and was used until a new one was built at Lawson Avenue and West Adams Street. McKinley School built about same time, about 1904, as Roosevelt School in LaBelle View; that school has been enlarged and rebuilt.

The Pleasant Heights Hose Company, providing fire protection for the area, was first listed in the city directory in 1909. The existing building dates from 1922 and is at 1332 Adams Street.

In addition to essentials such as schools and fire protection, Pleasant Heights attracted amenities such as grocery stores, confectionaries, barber shops, and cafes. They were concentrated around Lawson Avenue and State Street. There were a few apartment buildings, such as the Hillcrest Apartments at 260 Lawson Avenue, and there were some on Lawson at its eastern end.

In 1913 the Congregational Church voted to move to Pleasant Heights, and on March 21, 1915, the congregation dedicated its new church at Lawson and Orchard Park Avenues. In 1924 there were extensive repairs and improvements to the church, although its exterior remained virtually unchanged.

Also in 1913, the Ohio Valley Hospital reviewed a number of possible sites for location of a new hospital. The land known as Ross Park, adjacent to Pleasant Heights, was selected because of the hospital's belief in continued hilltop growth for the city; clearly there was a growing general perception that the future was on the hilltop. There was some controversy regarding the new site's distance from the mills and about the steep incline to reach hospital from downtown. The issue was settled when a fast team of horses, hauling a hearse, sped from the mill gates, up Adams Street and to the hospital site, proving that it would not take substantially

longer than reaching a downtown site. The hospital paid \$15,000 for 11 acres next to the Pleasant Heights Driving Park, where 5 more acres were also available and later purchased; it appears that the extra land was not needed and became part of the Ross Park subdivision.

In 1916 the First Baptist Church purchased a building in the downtown area on South Fifth Street, but in 1925 moved to the McKinley School building on the hilltop. In 1926 the congregation occupied a newly-constructed church at Cedar and Plum Streets.

1920 was a busy year for hilltop construction when, according to the Steubenville sesquicentennial history, "work progressed rapidly on 50 modern homes under construction on Pleasant Heights by Ross Park Realty Company." This probably referred to Oakmont, which was in the Ross Park subdivision. The brick homes, all of similar design, were obviously built by a single company as a speculative development; they were unusual for their brick construction, since frame was by far the preferred material on most of the hilltop.

Addition of amenities in the hilltop area continued when in 1938 the city purchased Murphy Field for use by Pleasant Heights children.

### **The Character of Pleasant Heights**

Larger homes in popular early 20th century styles characterized Ross Park Boulevard and the western edge of Lawson Avenue. Many of these homes were occupied by important civic and business leaders, and clearly this was the more upscale, well-to-do area of Pleasant Heights. These houses tended to have large yards, generous setbacks, and wider than common spacing between homes. Examples include 811 Lawson (Colonial Revival style); 620 Ross Park Boulevard (Tudor Revival), and 646 and 658 Ross Park Boulevard (Colonial Revival). Important civic and business leaders occupying these homes included Carl H. Smith, attorney and a partner with Charles Simeral in the Herald-Star Printing Company (811 Lawson); Frank A. King, a Wheeling Steel executive (646 Ross Park), and W.R. Siedenbergl, a Weirton Steel executive.

Oakmont was a distinctive street because of its double row of brick two-story houses, obviously built all at once by a single developer. It stands out in a neighborhood of mostly frame houses and has a very consistent streetscape -- there is uniformity in height, design, setback, materials, and details, imparting a sense of a tidy, controlled environment.

Apartment buildings, stores, and shops were toward the eastern end of Lawson Avenue, while schools and churches were on "scattered" sites chosen for convenience or for particularly site features such as high visibility.

Much of the remainder of the housing in Pleasant Heights was typical of early 20th century neighborhoods, employing frame construction, with styles such as American Foursquare and Bungalow, and some Colonial Revival; many home were simply well-built vernacular designs with no stylistic elements. Most lots were small, resulting in close-spaced houses with uniform setbacks.

## LaBelle View

The hilltops west of downtown Steubenville and north of the break in the hills where Market Street ran were originally farmland owned by the Mooney, Bustard, Hoffman, and Means families. By 1902, just before intensive development began, the land where LaBelle View would rise was held by the heirs of James Means, who was an important early Steubenville industrialist. The Means farm in particular had an excellent site overlooking the river and would become the heart of LaBelle View.

The LaBelle View Land Company bought the Means land and some other holdings and first laid out lots for mill workers. This fact illustrated how the concept of the suburb had indeed been expanded to include working class people as well as those of wealth. While the best lots, those on and close to the brow the hill overlooking the Ohio River valley, were no doubt intended for business owners, upper-level business and industrial managers, and other well-to-do people, from the very start the inner part of LaBelle View was intended for the working class and was spotted with conveniences and necessities of all kinds, including groceries, confectionaries, and meat markets.

As had happened on Pleasant Heights, the City of Steubenville was willing to extend water service to the new subdivision, but at a price: the LaBelle View Land Company was required to deed to the City the entire hillside from Belleview Boulevard down to the rear of the properties along Eighth Street; a piece of woodland over to Franklin Avenue for a new road; three or four lots for a schoolhouse; three lots for a standpipe; and two lots for an engine house.

The LaBelle View Land Company was incorporated in 1902, with W.A. Roberts as president, Daniel Keller as secretary, and J.A. Langfelt as treasurer. J.C. Lashley was the company's manager and lived at 300 Belleview Boulevard, a house which is still standing.

Lashley moved to Steubenville from Pittsburgh in 1902. He had training in architecture and worked in construction and real estate in Pittsburgh. He organized the LaBelle View Land Company, laid out the development, managed the company, and organized the Ohio Valley Building & Trust Company, which helped people build and buy homes in this and other neighborhoods.

Fannie Travis Porter started working for the company in 1908 selling lots and collecting rents. Later she began to build houses; she drew plans, ordered materials, and acted as a general contractor. According to her daughter, Dorothy Cummings, Porter built 75 homes in 20 years. She lived at 1414 Belleview (now demolished) and later at the corner of Maryland Avenue and Pittsburgh Street. In 1919 she built 1112 Oak Grove Avenue and two houses next door to it; the two adjacent houses still survive.

In 1904 a single-room frame schoolhouse, which later became the First Church of Christ, Scientist, was built at 1300 Maryland Avenue; with rapid increase in population in LaBelle View, a brick school building was constructed in 1911 and enlarged in 1929 at the southeast corner of Maryland and Carnegie. This building has since been adapted for use as a nursing home since the Roosevelt School moved to another location.

In 1907 the Steubenville Traction & Light Company, the street railway operator, laid track from the gates of Union Cemetery to Wilkins Street and Wellesley Avenue. A loop track on Oregon Avenue was added in 1925, though by this time the auto was the primary means of travel to, from, and within LaBelle View and other neighborhoods.

The 1907 Centennial Edition of the Steubenville Herald-Star had a section on LaBelle View which provided some insight into how new suburban residential areas were viewed at the time. Referring to the area as "LaBelle View, Steubenville's Classic Residence Section," the paper enthused:

Steubenville's Beautiful Residence Section Above the Fog Line, Handsome Homes, Natural Gas, City Water, Electric Light, Street Car Lines, Paved Streets, Fire Department and Rural Free Delivery.

During the past eight years the valuation of real estate in Steubenville has nearly doubled as is shown by actual sales. . . . With the commencement of operations at LaBelle Mill and other kindred industries, Steubenville's awakening in real estate was to be expected. With the influx of mechanics, tradesmen and laborers, every available space in the old town was soon occupied. . . .

LaBelle View opened up 170 acres of land for development overlooking the old city, with paved streets and sidewalks, natural gas, city sewer and water, and fire protection.

Many of the cultured citizens of Steubenville have already built their homes at LaBelle View, and others are preparing to do so; away from the rattle, dust and confusion of traffic; away from the smoke of the mills; away from the huddled residences; above the fogline, in God's pure air and sunshine. . . .

According to value, these lots are divided into three sections, the most valuable of which is the section overlooking the city with a splendid view of the Ohio. . . . This magnificent location is divided into large lots, 60 feet in width and from 120 to 220 feet in length, and guarded by building restrictions.

The year just past three new industries, employing over 1,000 skilled workmen were completed on the opposite side of the river; at least a thousand homes must be built in Steubenville during the year 1907 to provide for the increase in population.

Where purchasers find it inconvenient to pay cash, the broad and lenient policy of the company make it possible to buy a home on easy installments.

This newspaper piece tells much about how the new area was viewed. One suspects that even if "every available space in the old town" had not been occupied, LaBelle View would have had an irresistible draw for city dwellers anxious to live close to but not in town.

In 1902 the LaBelle View Land Company offered a free lot to the first church to build in the subdivision. In 1903 the LaBelle View Christian

Church took that offer and built a frame structure; in 1905 the congregation built a brick Late Gothic Revival church at the northwest corner of Euclid Avenue and Ohio Street.

The First Church of Christ, Scientist was organized in 1914 and met in people's homes and then in several locations downtown. In 1936 the church purchased the frame building at 1300 Maryland Avenue, the first schoolhouse in LaBelle View, dating from 1904, and remodelled it for church use.

The Second United Presbyterian Church built its present brick building at the northeast corner of Belleview Boulevard and Ohio Street in 1926 to replace a chapel built in 1912.

1911 was the first year the LaBelle View Hose Company was listed in the city directory, at the northwest corner of Maryland Avenue and Carnegie Street. In 1913 the company had 20 volunteers, one engine and one hose cart. The existing fire station building, no longer used as such, was built in 1928.

Innovations in home financing helped to spur suburban growth in Steubenville, making it easier for people to obtain new houses. As early as 1888 the Steubenville Building and Loan Association was formed, and according to the 1947 sesquicentennial history it "helped thousands build or buy their homes."

The Ohio Valley Building & Trust Company had an even greater impact. The 1907 newspaper centennial publication described this enterprise: "The Ohio Valley Building & Trust Company, which gives many advantages to the purchaser over the Building & Loan, as they not only furnish the necessary funds, but the location, and will build a home on a very small cash payment, leaving the balance to be paid the same as rent, and which, although it has been in existence but a little over 6 months, has been responsible for a number of workmen being able to own their homes and little properties. . . ." The manager of the company was J.C. Lashley, who was also manager for the LaBelle View Land Company. These appear to have been related enterprises, indicating how savvy the suburban developers were. Not only were they able to obtain and subdivide large parcels of land, they secured utilities at low cost, and they provided a mechanism for people of even modest means to buy or build houses.

A 1927 newspaper article gave credit to the developer of much of Oregon Avenue: "Oregon Avenue stands as a monument to Neal J. Dunn. When he moved onto the street several years ago there were three or four houses on it. . . . Oregon Avenue is one of the outstanding streets on the hilltop." Dunn sold 144 properties in LaBelle View and had 9 houses under construction at the time the article was written.

### **The Character of LaBelle View**

Belleview Boulevard, the street along the brow of the hill, was the premier residential street in LaBelle View and remains so today. Relatively few homes were located on the east side of the street, in part because the land dropped off so steeply, leaving clear vistas of the Ohio River valley. Belleview Boulevard included some of the best examples of early 20th century residential architecture in Steubenville and was home to some of the city's most influential people.

Examples include 100 Belleview, home of Charles D. Simeral, President of the Herald-Star Printing Company and publisher of the city's daily newspaper; 267 Belleview, home of H.S. Cable, secretary of Jefferson Building and Savings; 339 Belleview, home of Mone Anathan, owner of the Anathan Brothers Store, later The Hub, the city's largest department store during much of 20th century; 349 Belleview, home of C.P. McFadden, secretary of the Ohio Foundry & Manufacturing Company, 415 Belleview, home of J.H. Mosel, president Mosel-Johnson Co., a very large wholesale grocer in Steubenville. 505 Belleview, home of G.W. Thompson, who helped organize the first building and loan association in Steubenville and was owner of Mingo Realty; 508 Belleview, home of F.W. Owesney, president of the Union Savings Bank & Trust and secretary and treasurer of the Eastern Ohio Sewer Pipe Company; 718 Belleview, home of J.C. Williams, president of Weirton Steel Company, and 812 Belleview, home of George J. Yjurjevic, who was in real estate and insurance.

Belleview Boulevard had excellent examples of popular early 20th century residential styles, including the Bungalow style at 267, 601, and 1401 Belleview; Colonial Revival at 339, 405, 417, 800, and 812 Belleview; the Dutch Colonial Revival at 415 Belleview; Tudor Revival at 349, 631, and 718 Belleview, and Italian Renaissance Revival at 508 Belleview.

Other streets with the larger, more architecturally distinctive homes included South Bend, where Number 200 is an example of the Spanish Colonial Revival style, 301 South Bend, a Bungalow; Dutch Colonial Revival designs at 301, 308, and 317 Reserve, and 321 Reserve an example of the Colonial Revival style.

As anticipated by the shallower setbacks in this area, apartment buildings were built on Wilkins/Reserve Street (the name changes at Ridge Avenue). The Wilken, at the northeast corner of Wilkins and Pennsylvania, is a rare example of the International style. The Durban Apartments at 501 Wilkins is Colonial Revival in style, and the Glenstin Apartments at 329-331 Reserve, with their distinctive enclosed porches, were not built in any style but are a solid early 20th century apartment house.

Streets west of Wilkins tend to have a slightly different character than those east of it. There are more frame residences, designs are more vernacular, and there are neighborhood-scaled stores interspersed among the houses. Examples of such stores are at 518 Carnegie, the Tomlinson store, with apartments above; 1249 Oregon, the well-preserved David Stern Market, where the owner lived above the store; the southeast corner of Pittsburgh and Pennsylvania, a combination store and apartment building.

### **Brady Estates**

This subdivision was developed by John Brady's heirs on land that had been in the family for nearly 100 years; the developers laid out a system of parkways, cul-de-sacs and curvilinear streets that actually set aside a large amount of land for open space rather than filling it up with dense construction. Though the eastern leg of Brady Circle has been stubbed off due to highway construction, the subdivision exists today largely as it was first laid out.

Houses here were built later -- sometimes much later -- than those in other hilltop subdivisions, most having been completed between the 1930s and the 1950s. Some were from the 1960s, and even today there are unbuilt lots. There were many large single-family homes; stone was a common building material and was not found elsewhere in Steubenville neighborhoods in similar quantity.

Many steel executives and other business leaders lived in Brady Estates. Included were F.A. Brandt, of the Brandt Motor Car Company, at 123 Brady Circle West; Joseph Echols of Weirton Steel at 606 Granard; Albert Baker, also of Weirton Steel, 702 Granard; A.S. Earp, another Weirton executive, 703 Granard. Erle H. Leathers of Wheeling Steel, at 801 Granard; 1721 Hamilton Place was the home of Charles Cowher of Wheeling Steel; Guy Wehr of Wheeling Steel lived at 1811 Hamilton Place; John W. Unroe, another Wheeling executive, was at 1807 Hamilton Place; at 1711 Oregon lived J.A. Denning, also of Wheeling Steel, and at 1804 Hamilton Place lived Daniel Schiappa of the Schiappa Coal Company. Clearly if one was in steel, one was in Brady Estates.

The houses were good examples of residential design of the period, which tended to picturesque styles such as Tudor Revival, as well as older established styles such as Colonial Revival. Examples of the Colonial Revival survive at 129 Brady Circle East, 702 Granard, 703 Granard, and 1811 Hamilton Place; the Tudor Revival can be found at 123 Brady Circle East, 606 Granard, 801 Granard, and 1721 and 1803 Hamilton Place; and an International style house is at 1804 Hamilton Place. Residences in Brady Estates are almost all single-family, but there are a few apartment houses.

### **Other Suburban Development**

Between 1920 and 1930, the city expanded to the west and south, doubling the land area it covered. Harding Junior High, at one of the entrances to Brady Estates, was built in 1926 at a cost of \$359,000, the stadium was added in 1928 for \$40,000 more. This school obviously was built for more than just the population at the time and was a sign of continued hilltop growth.

Other subdivisions farther west started to develop at this time (1930-50s) along the spine of Sunset Boulevard; these are outside this project's study area and an inventory and an assessment of these areas have not yet been done. The later and more distant subdivisions include Buena Vista, John Spahn's Subdivision, Beverly Hills, and Hollywood; they too are part of the ongoing story of the suburbanization of Steubenville and are worthy of more study.



## ASSOCIATED PROPERTY TYPES

### Single-Family Residences

#### Description

Single-family residences in the hilltop neighborhoods were built later than the downtown area housing stock, due to the later development of the hilltop. Homes in this area are almost entirely early 20th century in date, with most built between 1900 and 1940.

Although Pleasant Heights and LaBelle View, the two largest hilltop subdivisions, developed rapidly over a fairly short time (around 1900 to 1930), there is a good deal of architectural variety in their single-family residential structures. High-style houses tended to be built on the most desirable lots, while vernacular designs were the general rule in the less expensive areas.

In LaBelle View, the most desirable lots were along Belleview Boulevard overlooking the Ohio River valley, and it is here that Steubenville's business, industrial, and political leaders built their large high-style homes. To the west, back from the brow of the hill, there were smaller and more modest residences, mainly vernacular in design, though the Bungalow style was fairly common in these areas, and many buildings had some elements or ornamentation typical of various styles. In the far western portion of LaBelle View, along Woodlawn Road (which had a fifty-foot setback and very large lots), high-style designs again became common.

In Pleasant Heights, most of the single-family homes were vernacular in design but, as in LaBelle View, there were some more desirable areas where larger lots prevailed and high-style houses were built. In Pleasant Heights, these were along Ross Park Boulevard, along the west side of the subdivision overlooking Beatty Park, and not along the east edge overlooking the Ohio River Valley. This was likely due to the fact that downtown development had extended up the hill immediately below Pleasant Heights, and the clear views of the valley typical of LaBelle View were not available here, making the buildings lots less desirable.

The third major hilltop subdivision, Brady Estates, was the farthest west of the major subdivisions and was the latest to be built up. Most homes here date from about 1930 to 1940. Unlike the traditional urban grid that prevailed in Pleasant Heights and LaBelle View, the streets in Brady Estates had a more planned and consciously "suburban" look, with curvilinear roads, landscaped parkways, and cul-de-sacs. The lots were large, as were the setbacks, and the subdivision was popular with business leaders and upper-level industrial managers and officers. Much of the residential architecture was either high-style in design or employed stylistic elements; there were no blocks of vernacular working-class homes typical of the two earlier subdivisions.

The vernacular architecture of the hilltop area was quite varied but shared some common characteristics. Brick was used here and there as a building material, most notably in the two-block section of Oakmont Avenue in Pleasant Heights, but frame construction was far more common. Almost every



This view of Oregon Avenue, along the north side looking west, shows the close spacing, common setback, and mixture of designs typical of the LaBelle View subdivision.



At Belleview Boulevard and Pennsylvania, in LaBelle View, is the former F.W. Owsney house. He was a banker and was associated with a local sewer pipe company. The house is typical of the finer, more expensive homes built by the wealthier LaBelle View residents who could afford the prime lots at the edge of the hill.

home was two stories in height, some Bungalow style homes, and those in the bungalow form, were one-and-a-half stories high, but their proportions were such that the upper floor was nearly a full second story. Compositions usually were asymmetrical, with offset doors and large single living room windows on the first floor, and irregularly-spaced second-floor windows. Wood shingles and beveled siding were the most common exterior materials, though today much of the original siding has been covered with artificial siding. Windows were almost universally one-over-one double-hung sash.

Most homes had open porches extending the full width of the main facade. They were a combination of masonry and wood construction, with a common design being a brick porch foundation and column support piers, with a wood floor, columns, and roof structure.

The vernacular homes achieved visual variety through variations in siting, roof shape, and massing of building forms. Siting varied with topography. On one street, due to level site conditions, all the houses might be at grade level, while on another street the houses on one side might be at grade but sit on elevated sites on the other side of the street. In these cases, each house had its own steps, usually concrete, from sidewalk to front yard level.

There was much variety in building form and massing, as well as roof shape. Many homes were square in plan and in main facade proportions; others, particularly bungalows, had more horizontal proportions. The strongly vertical proportions of the Italianate period typically were not found on the hilltop. Irregular massing was common, creating projecting bays, setbacks in facade walls, and irregular and asymmetrical floor plans. Combined with steep gable roofs, hip roofs, gambrels, cross gables, and wall and roof dormers, all these elements gave a rich variety to the simple, vernacular house designs of the hilltop area.

The high-style homes of the hilltop were far fewer in number than the vernacular designs, but they gave their neighborhoods a great deal of architectural variety. As noted above, they were built in the most desirable areas, tended to be large and placed on large lots, and they employed architectural styles typical of their period. Colonial Revival, Tudor Revival, and Dutch Colonial Revival were the popular styles; the Bungalow style was popular, too, and was the only identifiable style that also extended into the more working-class areas dominated by vernacular designs; a pair of well-executed Bungalow style homes might, for example, appear in a long row of plain vernacular houses.

The hilltop's high-style homes, with the exception of the Bungalow style, were almost all built of masonry: brick, stone, or a combination of the two. The Colonial Revival style houses had that style's hallmark features: rectangular plan, symmetrical facade, gable roof, and entrances flanked by columns, colonnades, or column-supported porch roofs. The Tudor Revival style homes employed the half-timbering, diamond-paned windows of small size, irregular massing, and steep gable roofs typical of that style. The Dutch Colonial Revival style houses were distinguished by their trademark gambrel roofs, shingle siding, and rectangular plan. Vernacular houses with some elements of the style, most commonly the gambrel roof, were fairly common in the working-class areas. The Bungalow style homes had the shallow gable roof, deep full-width front porch, and low roof dormers commonly found in that style.



The early 20th century stone house at 309 Lawson Avenue in Pleasant Heights exemplifies the attention to style, appearance, and details that was important to owners of the more expensive subdivision houses.



The 1400 block of Belleview, where the houses sit high on a bluff overlooking the upper end of Market Street, has a great deal of variety in house designs. This variety is typical of most of the hilltop area housing stock in Steubenville.

## Significance

Steubenville's hilltop area single-family homes may be significant under Criteria A, B or C in the fields of ARCHITECTURE, COMMERCE, INDUSTRY, and POLITICS/GOVERNMENT, depending upon their architectural design or the occupations and significance of their builders, occupants, and owners. All three major hilltop subdivisions, and the smaller additions to them, illustrate the powerful 20th century trend toward the suburbanization of American cities. In the period between 1900 and 1940, the hilltop neighborhoods developed to receive working class, middle class, and upper middle class people of all walks of life, changing forever the character and future prospects of the downtown neighborhoods they left behind; these neighborhoods generally were left to lower-level members of the working class and to racial and ethnic groups whom social and economic success had largely eluded.

The hilltop area's single-family homes may meet Criterion A as the homes of the commercial, industrial, and political leaders of the early 20th century, when Steubenville reached the zenith of its commercial and industrial power; and also as the homes of the city's middle and working class residents who had achieved a level of success and aspired to new homes outside the traditional downtown residential areas. These homes are a physical manifestation of the Suburban Ideal as it was achieved in Steubenville and represent one of the city's most important historical trends during the first half of the 20th century.

Under Criterion B, Steubenville's hilltop area single-family homes may be significant because of their association with prominent individuals in the city's commercial, political, and industrial life. Belleview Boulevard in LaBelle View, Ross Park Boulevard in Pleasant Heights, and Granard and other streets in Brady Estates were home to many of the most important people of early 20th century Steubenville. Their economic success and social position can be read, at least in part, from the location, design, size, and expense of the homes they occupied.

Steubenville's hilltop area single-family homes may also qualify under Criterion C because they represent certain types and methods of construction. Both the high-style and the vernacular homes represent the kinds of residences people aspired to in the early 20th century: large homes with many windows, large setbacks with lawns and back yards, and a residential setting completely removed from commercial, industrial, or transportation land uses.

### Registration Requirements

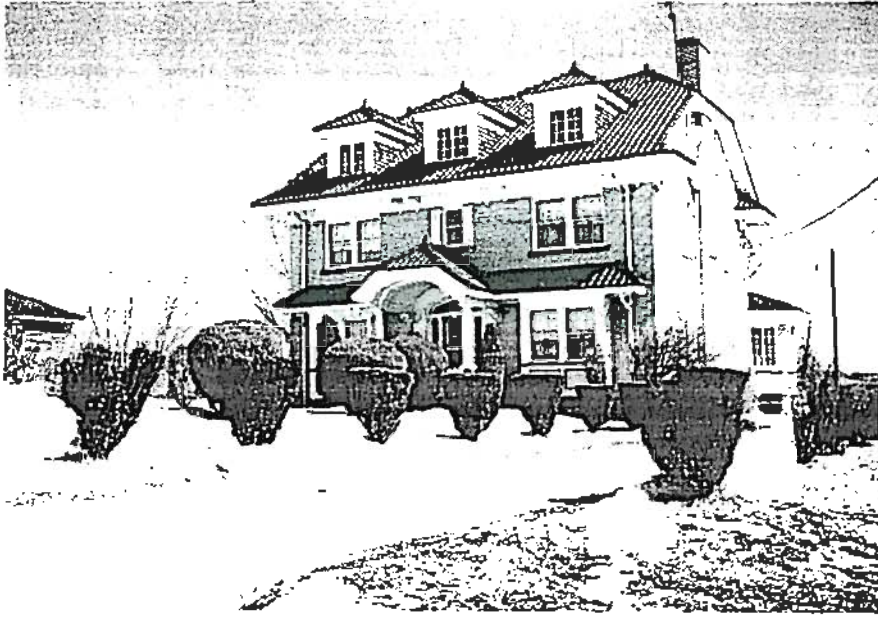
To qualify under Criteria A or B, a single family residence should have the following aspects of integrity:

#### 1. LOCATION

Refer to the discussion of location in the introduction.

#### 2. DESIGN

If a high-style building, the property should have its stylistic features



The brick home at 812 Bellevue was built by George J. Yurjevic, who was involved in real estate, insurance, and contracting. It retains its original clay tile roof and sits on a site with a view of the Ohio River valley.



Two blocks of well-built brick houses on Oakmont in Pleasant Heights were built over a short period of time as a new development early in the 20th century. Today they constitute one of Steubenville's best-preserved and most pleasant neighborhoods.

intact. For example, a Colonial Revival style house should have its gable roof, symmetrical facade composition, and ornamented doorway; a Bungalow should have its full-width porch, low roof pitch, and wall or roof dormers.

If a vernacular building, the property should retain its basic form, roof shape, window and door openings, and interior plan. Original porches should be intact, as should ornamental or decorative features, however, loss of some or all of these elements should not keep a vernacular structure from qualifying if its other major design features are intact.

### 3. ASSOCIATION

The property should be sufficiently intact in the above two aspects of integrity to convey the historical relationship between an event or person and the property, or between the property and the broad trend of suburbanization in Steubenville between about 1900 and 1940.

To qualify under criterion C, a hilltop area single-family residence should have the following aspects of integrity.

#### 1. DESIGN

Refer to the comments above for Criteria A and B.

#### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. In high-style structures, this should include the original exterior surface materials, window and door trim, decorative elements, porch elements, and chimneys. For vernacular buildings, retention of original materials is somewhat less critical, as long as the vernacular building traditions or techniques represented by the structure remain apparent. For example, as long as the basic form, plan, and window and door openings remain intact, replacement of a vernacular building's wood siding with aluminum siding should not keep the structure from qualifying under Criterion C.

#### 3. WORKMANSHIP

The property should retain its major items of workmanship that make it an example of the time and place in which it was constructed. For buildings built in architectural styles, this should include, for example, porch columns and decorative treatments, window and door trim, cornice details, columns and pilasters, or other elements indicative of a particular style. For vernacular buildings, items of workmanship should include door and window trim, various decorative elements, and porches and their details.

#### 4. FEELING

The totality of a property's components, whether the property is vernacular or high-style in design, should express a specific point or period in time.

#### 5. SETTING (for historic districts)

In historic districts, the physical environment into which buildings are

placed should survive with its major features intact. In the hilltop area, for example, the setback of houses behind front lawns, the regular spacing of houses along the street, and the use of sidewalks and curbs all are important aspects of the integrity of setting in Pleasant Heights and LaBelle View. In Brady Estates, the curvilinear streets, broad front lawns and planted parkways are important aspects of integrity of setting.

## **Multi-Family Residences**

### **Description**

The goal of home ownership was a big part of the Suburban Ideal, so doubles and duplexes were virtually non-existent, and apartment buildings were not common in Steubenville's hilltop neighborhoods. These areas were built primarily for owner-occupants.

There was, however, some provision in the planning of the hilltop subdivisions for apartment houses. On Wilkins/Reserve Street in LaBelle View, for example, the setback was made minimal so that apartments could be built on standard-sized lots; indeed, this was where most of the area's apartments were built. In Pleasant Heights, apartments appear to have been limited to major streets such as Lawson Avenue. In Brady Estates, the only apartments were at Margaret Cecilia Manor on Brady Circle East.

Hilltop area apartment houses were varied in design. They were two or three stories in height, boxy and rectangular in shape, and were primarily of masonry construction (either brick or a stuccoed surface), although one large frame apartment house is known. About half of the existing apartment buildings have flat roofs, while the rest have gable roofs. There is one excellent example of the International Style, the Wilkpen Apartments on Wilkins; one nearby has elements of but is not a full-blown example of the Colonial Revival style; and a large apartment building on Lawson Avenue at the north edge of Pleasant Heights, the Hillcrest, is Tudor Revival in style. The rest are vernacular in design, rectangular in shape, with one-over-one windows. Some examples have porches: in one case a small entry porch, in another, a large two-story frame front porch; in a third, a full-width brick-and-frame enclosed porch. One apartment building at the corner of Pittsburgh Street and Pennsylvania Avenue in LaBelle View has a small commercial space in one corner of the first floor, with a commercial storefront in largely original condition.

### **Significance**

Steubenville's hilltop area multi-family structures may be significant under Criteria A and C in the fields of ARCHITECTURE and COMMERCE.

Under Criterion A, hilltop area multi-family structures may be significant as representing one aspect of the move to the suburban hilltops — they could provide temporary quarters for people who aspired to the new neighborhoods away from the older downtown area but who were not yet able to purchase of home. Their relative scarcity in these neighborhoods indicates that these were neighborhoods of owners, not renters.



The Wilkpen apartment building, located in LaBelle View, is an example of the International Style popular in the 1930s.



The Hillcrest, located on Lawson Avenue in Pleasant Heights, is another example of hilltop area multi-family housing. This building employed elements of the English Tudor Revival style.

Under Criterion C such structures may be significant because they represent certain types and methods of construction. In the handful of apartment buildings in the hilltop area, there is great variety in materials and designs, indicating the variety of approaches builders took in apartment design in the early 20th century. Those built in architectural styles, or with elements of those styles, may be significant for representing the use of such styles in the apartment building form. Others built in vernacular designs illustrate the variety of design from the 1900 to 1930 period.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain sufficient structural, finish, and stylistic features to identify it with the 1900-1930 period. This includes retention of window and door patterns, sizes, and trim; floor plans, overall massing, and roof shape; and decorative and stylistic treatments. Though some changes or missing elements will not necessarily keep the property from qualifying, it must retain essential character-defining features.

In order to qualify under Criterion C, a property should have the following aspects of integrity:

##### 1. DESIGN

Refer to the comments above for Criterion A.

##### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. This should include exterior surface materials, window and door trim, decorative elements, chimneys, and porches if they were part of the original design or were added during the period of significance.

##### 3. WORKMANSHIP

The property should retain the major items of workmanship that make it an example of the time and place in which it was built. Multi-family residences should retain ornamentation, original or period light fixtures, doors and windows and their trim, decorative brickwork, porch or stoop elements. If the property originally had a street-level storefront, then elements such as the bulkhead, transom, display window(s), door and associated ornamentation should survive.

##### 4. FEELING

The property's components, taken as a whole, should express a specific point or period in time between 1900 and 1930.

## Neighborhood-Scale Commercial Structures

### Description

Because they developed well outside the older downtown area of Steubenville, which had established retailers and stores to meet the needs of the nearby residents, the hilltop area subdivisions early saw the development of small-scale commercial buildings that housed stores offering groceries and other needed household goods. These usually were on the first floor of a two-story building that also contained residential units.

Both brick and frame construction were used; in some cases the building was residential in character, with massing, roof shape, height, and details similar to those of nearby buildings. In another example, a portion of an apartment building had a commercial storefront in one corner of the first floor.

In all cases, these were small-scale commercial enterprises providing primarily "convenience" goods and services to the immediate neighborhood. They blended well with the neighborhood's character, often occupying a corner lot for maximum visibility. Most were in vernacular buildings not constructed in an architectural style; they followed storefront design practice of the time, employing large display windows with a bulkhead below and transom above, with a modest glazed entrance door. Typically the storefronts and the buildings in which they were located had little ornamentation; in one case, however, a large enamelled metal sign survives (the closed David Stern Grocery on Oregon Avenue), which gives it a distinctive character.

### Significance

The neighborhood-scale commercial structures of the hilltop areas of Steubenville may be significant under Criteria A and C in the fields of ARCHITECTURE and COMMERCE.

Construction of small commercial enterprises was a measure of the growth and maturity of a neighborhood, making them a good means for tracking neighborhood growth and development.

In the case of the hilltop subdivisions, the small commercial stores met a specific need: since the area residents had left behind the more fully-developed retail facilities in the downtown area, there was at least some need, before larger-scale retail development occurred along Sunset Boulevard, for groceries and "convenience" stores so that every little need did not require a streetcar or auto trip downtown.

Under Criterion A, the hilltop area Neighborhood commercial structures may be significant as indicators, over time, of the growth, prosperity, and stability of the hilltop neighborhoods. As part of the fabric of the neighborhoods, these structures are components of the broad patterns of Steubenville's historical development.

Under Criterion C, the hilltop area neighborhood commercial structures may be significant because they represent certain types and methods of construction. They were built to blend into their neighborhoods as part of

the residential building stock, yet followed standard commercial storefront design practice. Unlike earlier Victorian-era commercial buildings typical of the downtown commercial district, the hilltop neighborhood commercial buildings did not employ applied ornamentation, patterned brickwork, or other decorative features. Their distinction lies in the fact that they were specialized structures built to perform a specific function in a residential area without disrupting the area's character.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain sufficient structural, finish, and stylistic features to identify it as a neighborhood-scale commercial structure from the 1900-1930 period. This includes retention of such features as original or historic storefronts (bulkhead, display windows, transoms, signboards and signs, doors and doorways, and associated ornamentation); upper-floor window openings and windows; exterior surface materials; and overall form and massing.

To qualify for listing under Criterion C, a property should have the following aspects of integrity:

##### 1. DESIGN

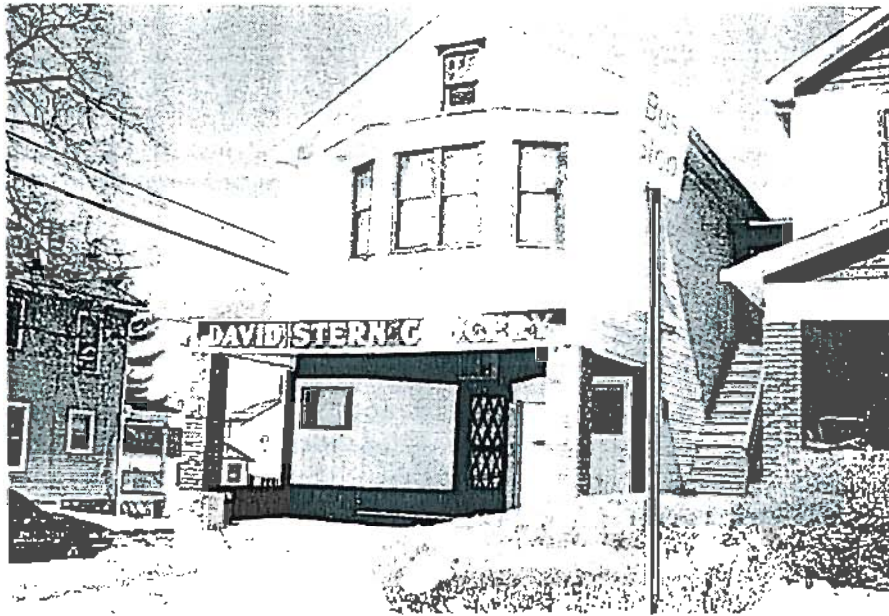
Refer to the comments above for Criterion A.

##### 2. MATERIALS

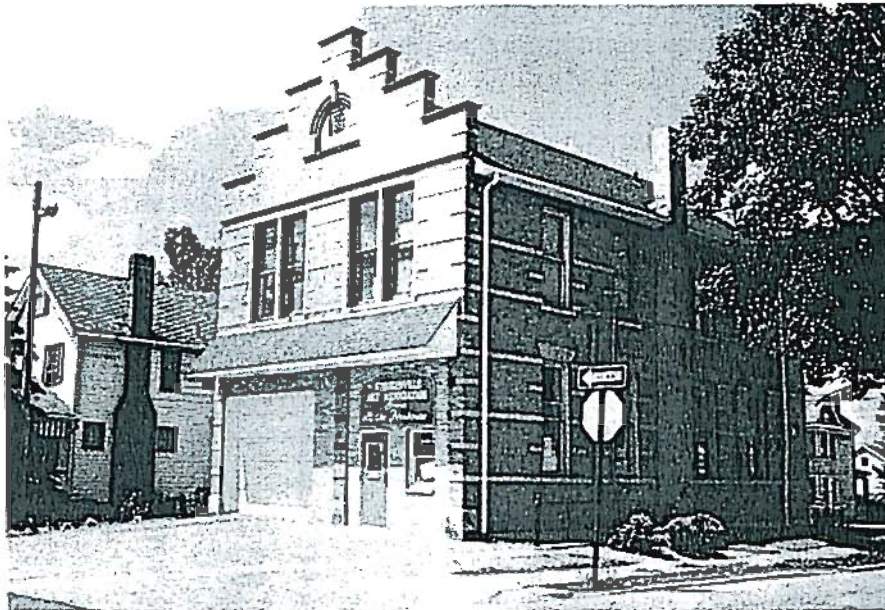
The property should retain a majority of the historic materials of which it was built. These include materials comprising the commercial storefront(s) and entrance door(s); upper-floor wall materials, including window openings, windows, and trim; and any associated ornamentation. Alterations or loss of some of these elements will not prevent the property from being listed as long as the character-defining features and original materials remain substantially intact.

##### 3. WORKMANSHIP

The property should retain the major items of workmanship that make it an example of the time and place in which it was constructed. Neighborhood commercial buildings should retain, for example, display windows, transoms, bulkheads, and other storefront components; entrance doors; trim and ornamentation, upper-floor windows; and signboards or signs. Alteration or loss of some of these elements should not keep a property from qualifying for listing as long as most of the elements of workmanship survive.



The David Stern Grocery at 1249 Oregon Avenue in LaBelle View is no longer operating but is an example of the neighborhood-scale commercial buildings which can be found in the neighborhood.



The former fire station in LaBelle View, now adapted to another use, was built to blend with the neighborhood in form, massing, materials, and scale.

#### 4. FEELING

The totality of a property's components should express a specific point or span of time in the period 1900-1930. This results from a majority of the property's elements of design, materials, and workmanship remaining intact. The specific components of neighborhood commercial buildings that contribute to these aspects of integrity are identified above.

### Churches

#### Description

Steubenville's hilltop neighborhoods did not have the number or density of churches typical of the older downtown neighborhoods. The principal reason was that, while many people moved from the downtown to the hilltop in the 1900-1940 period, they usually continued to attend their traditional downtown churches. However, some congregations decided to leave the downtown and relocate to the hilltop area. This was the case with the First Congregational Church, which was established in 1875, built a downtown church on Washington Street in 1882, and moved to Pleasant Heights in 1913. Others, such as the LaBelle View Christian Church dating from 1905, were new buildings built to serve entirely new congregations.

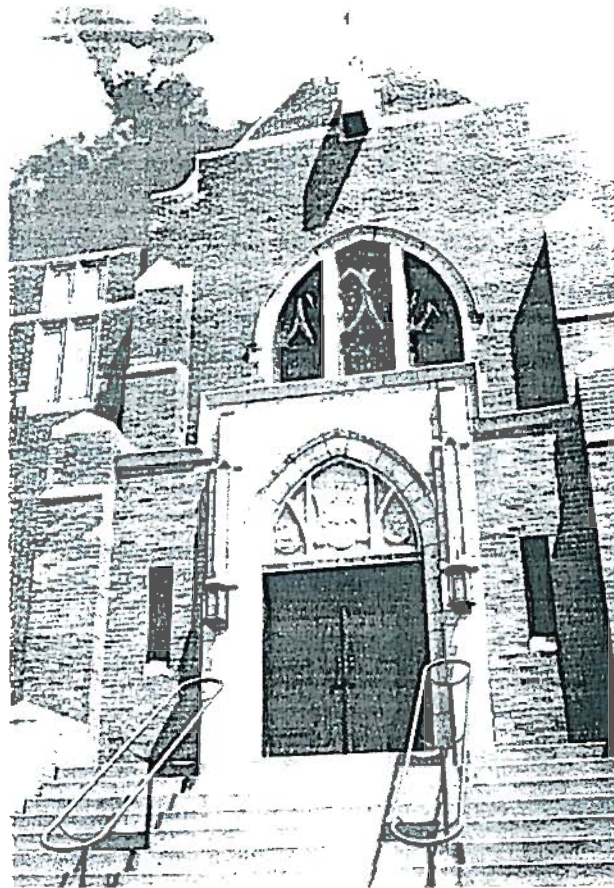
The hilltop area churches were early 20th century in date and were built in less ornate designs than the downtown churches. As always, their designs were affected by specific liturgical requirements and by the congregation's budget, but overall the hilltop churches were plainer and less ornamented than the downtown churches, and they tended not to be built in specific architectural styles.

Brick was the universal church building material, with light-colored cut stone used for trim material and ornamentation. Traditional elements of religious architecture, largely Gothic Revival in derivation, were employed in the hilltop churches: steeples and bell towers; tall, vertical proportions; pointed-arch windows; and extensive stained glass. Some buildings, such as First Congregational, moved away from Gothic origins and employed classical design elements such as columns, round-arched windows, and lower and more square proportions. In contrast, the Second United Presbyterian Church, at the corner of Belleview Avenue and Ohio Street, is a 1926 structure with Late Gothic Revival style elements, though it is not a full-blown example of the style.

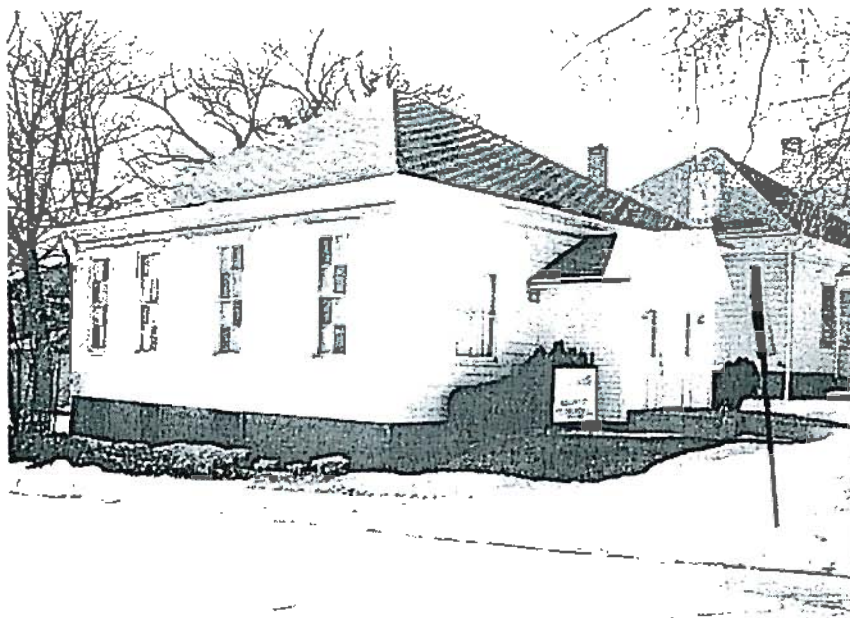
#### Significance

Steubenville's hilltop neighborhood churches may be significant under Criteria A or C, in the fields of ARCHITECTURE and RELIGION.

Under Criterion A, the hilltop churches may be significant as symbols of the shift in population and investment from the city's older neighborhoods to the hilltop area. Places of worship are an essential part of a neighborhood and tend to provide an "anchor" effect for the surrounding residents. As was noted above, the hilltop area churches were built later than the downtown churches and did not always have the same long-time relationship to their neighborhoods as those older churches; often, hilltop



The Covenant (Second United) Presbyterian Church in LaBelle View is a major neighborhood landmark.



This building was the original school in LaBelle View but now serves as the Seventh Day Adventist Church. Located at Pittsburgh and Maryland, it has had some alterations but retains its original form and most of its original materials.

area residents continued to go downtown to worship. Nonetheless, the hilltop churches were important as indicators of the growth, maturation, and stability of the suburban neighborhoods on the hilltop.

Under Criterion C, the hilltop churches may be significant as examples of early 20th century religious architecture. As was noted above, they tended to be less ornate and of simpler designs than the older downtown churches, employing mainly 20th century design conventions and building techniques. In this way the churches mirrored the new neighborhoods' housing stock, which was equally representative of the era in which it was built. The residential and religious architecture of the hilltop area worked in tandem as an expression of the Suburban Ideal as it was achieved in early 20th century Steubenville.

#### Registration Requirements

To qualify for listing under Criterion A, a property should have the following aspects of integrity:

##### 1. LOCATION

Refer to the discussion of location in the introduction.

##### 2. DESIGN

The property should retain sufficient structural, finish, and stylistic features to identify it as a neighborhood religious structure from the 1900-1940 period. This includes retention of such features as original form, plan, and massing; towers or steeples; major window and door openings; and details and ornamentation. If the property is an example of an architectural style or employs elements of a style, sufficient defining characteristics should survive to enable identification of that style.

To qualify for listing under Criterion C, a property should have the following aspects of integrity:

##### 1. DESIGN

Refer to the comments above for Criterion A.

##### 2. MATERIALS

The property should retain a majority of the historic materials of which it was built. These include materials comprising the entrance door(s); windows, including stained glass; wall materials, including trim and ornamental or decorative elements; and ancillary components such as steeples or towers. Alterations or loss of some of these elements should not prevent the property from being listed as long as the character-defining features and original materials remain substantially intact.

##### 3. WORKMANSHIP

The property should retain the major items of workmanship that make it an example of the time and place in which it was constructed. Churches, for example, should retain entrance doors and trim; stained glass and other

windows and trim; architectural trim and ornamentation; and major interior decorative or ornamental features. Alteration or loss of some of these elements should not keep a property from qualifying for listing as long as most of the elements of workmanship survive.

#### 4. FEELING

The totality of a property's components should express a specific point or span of time in the period 1900-1940. This results from a majority of the property's elements of design, materials, and workmanship remaining intact. The specific components of neighborhood churches that contribute to these aspects of integrity are identified above.

### **Public Buildings: Fire Stations, Schools, and Others**

#### Description

Three schools, two fire stations, an armory, and the municipal water filtration plant are located in or adjacent to the hilltop area neighborhoods and represent local government's contribution to the stability and viability of those new suburban areas.

Because of their location at the top of the steep hills west of downtown Steubenville, beyond easy or quick access for firefighting equipment, it was important to Pleasant Heights and LaBelle View that they receive fire protection early on. Accordingly, by 1909 the Pleasant Heights Hose Company was serving that area, followed in 1911 by the LaBelle View Hose Company. Located strategically in the middle of each neighborhood, these companies each built new fire stations in 1928. Both buildings survive; the one in Pleasant Heights still serves as a fire station, while the one in LaBelle View is used for other purposes.

Education of the children born to hilltop area residents was another important government task. Because the steep topography made travel to and from the existing downtown schools difficult and lengthy, schools were built early in the history of these neighborhoods. The earliest was the small frame building at the southwest corner of Pittsburgh Street and Maryland Avenue, in LaBelle View. It was built in 1904 as a plain single-story frame building with a pyramidal roof. This was followed in 1911 by construction of Roosevelt School in LaBelle View at Maryland Avenue and Carnegie Street; by McKinley School in Pleasant Heights in 1914; and by Harding Junior High School in 1926 opposite Brady Estates on Sunset Boulevard. All survive today except for McKinley, which was demolished and replaced by a new building. The original 1904 school is used as a church; Roosevelt School has become a nursing home; and Harding continues in use as a school.

The city's new high-pressure reservoir had been opened in 1895 at what is now the intersection of Brady and Franklin Avenues. In 1915 the existing filtration facilities were added, and today these buildings and the reservoir mark the entrance to Belleview Park. Built of brick in a Classical Revival design, the two filtration plant buildings have arched window openings, low-pitched tiled hip roofs, and a Renaissance character befitting their classical design inspiration.

The National Guard Armory at 1629 Pershing, a few blocks south of the filtration plant, dates from 1926. Its design is typical for its time, consisting of an office block with a large drill hall behind. The office block, which contains the main entrance, communicates the building's military nature through its design, which incorporates massive stepped-back walls and narrow windows.

#### Significance

The schools, fire stations, and other public buildings of the hilltop area are qualified for listing under Criteria A and C in the field of POLITICS/GOVERNMENT. They are significant under Criterion A as physical manifestations of local government efforts to aid the newly-developed suburban areas by providing essential services, as such they are part of the broad history of suburban development in Steubenville. The armory, which could be expected to locate near the center of population, exemplifies the shift of population to the hilltop area which was well underway by the 1920s.

Under Criterion C, these five properties are significant because they represent particular types of buildings devoted to specific, specialized purposes. In the case of the fire stations, the properties represent firefighting technology and building practice of the time in which they were built; in the case of the three schools, one represents the old style of one-room school which was a dominant feature of 19th century education, while the other two represent the large, consolidated multi-room facility intended to serve a large area and provide mass education for large numbers of people. The filtration plant, designed to house large pieces of equipment, still achieved architectural distinction through its choice of style, and the armory has a distinct castle-like quality clearly related to its use.

#### Registration Requirements

Each of the properties above is qualified for listing because each has the following aspects of integrity:

Each property has integrity of location, because each is on its original site and maintains its original spatial relationship to its neighborhood.

Each property has integrity of design, because each retains its original form, massing, roof shape, door and window openings (with some exceptions, noted below), and stylistic elements.

Each property has integrity of association, because of its historical relationship to its suburban neighborhood (LaBelle View, Pleasant Heights, or both; all later served Brady Estates as well). Each can be shown to have contributed to the development of the Steubenville hilltop area subdivisions by providing essential public services to the residents there.

Each property has integrity of materials, because in each a majority of the original materials of which it was built is still intact. Additions or alterations, if any, have had only modest effect upon the integrity of materials for each property.

Each property has integrity of feeling because the totality of the components of each expresses the time it was built and the period in which it served its role in aiding the development of the hilltop neighborhoods.

Finally, each property has integrity of workmanship in the following ways:

The LaBelle View Hose Company fire station retains its character-defining features such as double-hung upper-story windows, decorative masonry, and its unusual stepped-gable design. Its main first-floor doors have been altered (one changed to a modern rolling door, the other infilled and a residential door and window installed), but this did not result in a significant loss of original elements of workmanship.

The Pleasant Heights Hose Company fire station retains its character-defining features such as original window and door openings; original dormer; eave treatment with exposed rafter ends; and brick pilasters. Some original six-over-one windows survive; others have been altered, but these changes did not result in a significant loss of original elements of workmanship.

The 1904 frame schoolhouse in LaBelle View retains its character-defining features such as original window trim and windows; the glass panes in some windows have been changed to colored glass for church use. Though it has been sided with artificial siding and has had an entry vestibule added, its elements of workmanship are intact enough to give the property integrity of workmanship.

Roosevelt School retains its character-defining features such as the high cut-stone foundation; ornamental stone entrance surround; modest cornice; and window sills. Window openings have been downsized for new windows, but the infill material is recessed so that the original size of the window openings is apparent. Despite the window alterations, the school has not experienced a significant loss of elements of workmanship.

Harding Junior High School retains its character-defining features such as its ornamented entrance doorways; a projecting cornice; and an ornamented name board high up in the parapet. The windows have been altered, but the original size of the window openings is apparent; these alterations have not caused a significant loss of elements of workmanship.

The filtration plant retains such character-defining features as original arched window and door openings, low-pitched tiled roofs, and a Renaissance-inspired classical design. The stone columns and decorative brickwork on the main building constitute important original examples of workmanship, credited to the Floto Bros. firm that executed a large number of masonry projects in Steubenville.

The armory retains its character-defining features such as its massive office block with stepped-back walls, buttresses, parapet, and slit windows that impart a distinctive castle-like feel to the building; and its drill hall behind the office block. The building's principal element of workmanship is its very simple scheme of ornament, which is achieved through plain stone banding in the main facade.

## Potential Historic Districts

In a partial history/architecture survey of Steubenville completed in 1990, several potential hilltop area historic districts were identified. Further work will be necessary to define precise boundaries, but the following potential districts are representative of Steubenville's 20th century trends in community planning and development:

1. LaBelle View: Bounded roughly by Belleview Boulevard, Arlington Avenue, South Bend, and Wilkins, this district incorporates both the individually distinctive single-family homes and also the more modest single- and multi-family homes of this neighborhood. It illustrates a cross-section of the kinds of people moving out of the downtown area in the early 20th century.
2. Ross Park: Consisting of several homes along one side of Ross Park Boulevard, this small potential district consists of architecturally distinctive homes built in the 1920s and 1930s. The district represents the Steubenville version of the "Suburban Ideal" available to people of higher socio-economic status such as professionals, business owners, and upper-level managers.
3. Brady Estates: Encompassing both halves of Granard, Brady Circle East, and the western portion of Hamilton Place, this potential district incorporates the latest additions to pre-World War II development in the hilltop area. This was the first of the city's suburban areas to make extensive use of cul-de-sacs and a consciously non-grid, picturesque curvilinear street pattern with significant amounts of open space. Popular with steel industry executives and upper-level managers, the district represents the "high-end" or "upscale" type of suburban development targeted at a specific market of fairly well-to-do people.

In addition, there is a potential district in Pleasant Heights along Oakmont Avenue between Clare Avenue and Cardinal Street. This two-block area of brick homes, obviously built for people of modest means, retains all important aspects of integrity and is representative of middle- and even lower middle-class efforts to move out of the older parts of the city.

# **DEVELOPMENT OF OPEN SPACE AND RECREATIONAL FACILITIES IN STEUBENVILLE, 1854-1945**

## **Introduction**

With the importance placed today on parks, recreational facilities, and urban open space, it may come as a surprise that the City of Steubenville provided no public support of such facilities until the 1920s. Prior to that time, it was only through private efforts that Steubenville had any park and recreational facilities available to the general public.

This context explores the early history of parks and recreation in Steubenville and provides considerable background on the significant public investment made in this area beginning in 1927.

The context concludes with an assessment of the surviving property types representative of Steubenville's parks and recreation history.

## **Early Open Space in Steubenville**

Early development of Steubenville did not include any planned open spaces. Unlike many other cities experiencing rapid growth in the 19th century, the city made no provision for parks in its grid pattern of streets, the commercial center, residential areas, and industrial areas consuming all the flat land on the west bank of the Ohio River.

Rural planned landscapes for urban areas were pioneered at Mt. Auburn Cemetery in Boston in 1831, and, by 1855, at Laurel Hill Cemetery in Philadelphia, Greenwood Cemetery in New York and Spring Grove Cemetery in Cincinnati. These efforts were based on the 19th century idea that cemeteries should be places of recreation much like public parks.

The "lawn plan" of cemetery design evolved in the 1850s. It held that retention of the natural topography and vegetation; development of organized road patterns which respected the topography; controls on the design and number of grave markers; views and vistas; and landscape features controlled to achieve a natural beauty all could be manipulated to provide a pleasant setting not only for the departed but also for city residents generally.

Adolph Strauch, the designer of Spring Grove Cemetery in Cincinnati, wrote, "Diversity, which is the main advantage of free landscape, must be sought in a judicious choice of soil, an alteration of hill and valley, gorges, brooks, and lakes, adorned with tasteful monuments such as temples, columns, and statues, partially concealed in luxuriant vegetation. . . ."

## **Union Cemetery and Beatty Park**

Though Steubenville was not a leader in providing its citizens with public parks for recreation, it was in the forefront of cemetery design. An important function of mid-19th century planned cemeteries was public recreation; local residents were encouraged to use cemeteries for picnics,

walks, and relaxation. Steubenville's earliest planned open space was Union Cemetery, part of which was to become Beatty Park, and it was one of the earliest lawn plan cemeteries in the United States.

The Union Cemetery Association was formed in 1853 by the leaders of several Protestant churches, who were running out of space for interments in their downtown cemeteries. A long search led to the purchase of the original hilltop tract of 48 acres from William Huscroft in 1854.

The Association formed with capital stock of \$10,000, divided into 1,000 shares of \$10 each; some churches and a number of individuals became stockholders. 600 graves were transferred from an old downtown cemetery at South and Fourth Streets, as well as from several other church cemeteries. On July 4, 1854 a picnic was held on the grounds of the new Union Cemetery as an inaugural event.

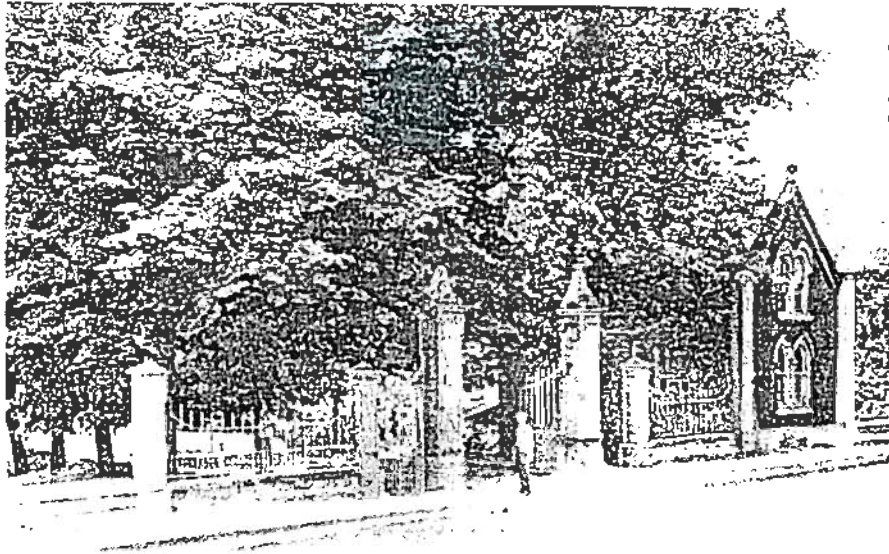
By 1877 there were additional land purchases which brought total cemetery acreage to over 140. An additional 80 acres were acquired in 1905 and 24 acres in 1967. The cemetery and park now occupy approximately 150 and 100 acres respectively; the site is nearly a mile long, with elevations from about 700 feet at the south end to 1,200 feet in the northwest corner of the cemetery.

Some 1855 meeting minutes of the Cemetery Association give place names in the cemetery; they are evidence of the rural ideal that helped to shape the entire concept of the cemetery: Laurel Point, Monument Hill, Spring Dale, Greenwood Ravine, Clover Lawn, Cascade Ravine, Laurel Ravine, Forest Mound, Beech Point, Willow Mound and Prospect Point refer to subdivisions within the cemetery. The principal avenue was called Main Tour (it was 20 feet wide) with narrower roads (13 feet wide) leading from Main Tour; they had names such as White Oak Avenue, Laurel Avenue, Linden Avenue, Willow Avenue, and Crescent Avenue.

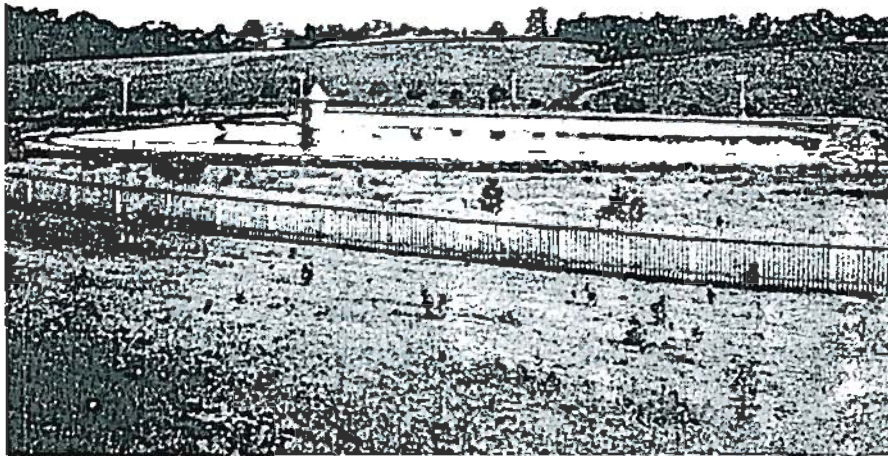
Association minutes from 1854-55 show that the cemetery was thoroughly planned from beginning and that variations from the plan were not common. John D. Slack, "landscape gardener," platted a portion of the grounds; Union Cemetery was referred to in a 1910 Steubenville history as "probably the most beautiful rural cemetery in the country."

Dr. Charles Clinton Beatty, a prominent Steubenville physician and philanthropist with a great interest in Union Cemetery, submitted proposed rules and regulations to the Cemetery Association's board. Among them were provisions that no walls or fences around graves could be over 18 inches high exclusive of railings or posts, and fences could not be over 3 feet high. The rules provided that "all plans of enclosures, monuments, vaults, tombs and other structures must be submitted to and approved by the Board of Trustees." Rules for visitors from 1856 stated that there was to be no admission on the Sabbath except with a special permit.

Union Cemetery's distinctive features include its natural topography, its landscaping, and its large number of flowering shrubs. Its buildings, monuments and bridges, which are of stone and were constructed by the local firm of Floto Bros., have a high quality of architecture and craftsmanship and contribute to the cemetery's sense of quiet permanence. The monuments and buildings are excellent examples of mid- and late-19th century artwork



Union Cemetery, dating from 1854, was one of Steubenville's earliest publicly-accessible recreation areas. This view shows the original fence and gates along Market Street.



The new high-pressure city water reservoir, built near the turn of the century at the head of Stony Hollow, was surrounded by land that would become Belleview Park.

and sculpture and include details such as stone steps, low walls and iron fences enclosing family plots, and elegant small-scale mausoleums.

Significant structures include stone bridges from 1884-85; the original office (now the chapel), built 1892; the receiving vault of 1888; mausoleums from the late 19th and early 20th centuries, including Kelly (1892), Beltz (1894), McClinton (1883), McConville (1896), and Sherrard (1890); the Soldiers & Sailors monument of 1870; and the Market Street entrance and Administration Building built in 1931.

In 1873 the Cemetery Association offered to sell to the city all lands along Glen Brook (also called Copperas Run and later Beatty Creek) from the road up Wells Run (later Lincoln Avenue) to the western edge of the road in the settlement of Jacksonville, containing approximately 100 acres, for \$12,000. The city declined, but this land saw use as a park anyway.

Although now owned by the city, Beatty Park was linked historically with Union Cemetery. The existing park entrance on Lincoln Avenue was the original cemetery entrance until the main gates were built on Market Street later in the century. Early cemetery records refer to Beatty Park, referring to lower portions of the cemetery where graves had not yet been put in. Although public ownership did not occur until 1930, this publicly-used portion of the cemetery property saw regular recreational use and memorialized Dr. Beatty, a moving force in the Cemetery Association from its founding until his death in 1882.

Though the city chose not to purchase the park lands, the Association formalized the land's use as such in a November 2, 1874 resolution: "Dr. Beatty has in different ways and times donated to the Union Cemetery to the amount of six thousand dollars and proposes to make further donations in ground and improvements upon condition that the unoccupied part of the grounds be used improved and embellished as a Park -- therefore resolved, that we guarantee that so much of the Cemetery ground as is not needed for Cemetery purposes shall continue to be used improved and embellished as a park in connection with the Cemetery until so needed for Cemetery purposes or until the City of Steubenville shall take such ground as part of a public park upon such terms as are acceptable to the Cemetery Association."

Improvements continued through the years. Dr. Beatty acquired ground at the southern entrance and prepared plans for "a gateway for carriages and on either side gates for the footways connecting with a fence enclosing the south line the post and pass of stone and the pickets of iron. . . . Dr. Beatty has proposed to pay the expense incurred by this improvement." These improvements cost \$1,510; a lodge house cost \$1450, also the gift of Dr. Beatty.

In January, 1883 a board committee reported that the ground above the second bridge was cleared off for a picnic ground and it would be completed in the spring; the board also adopted a resolution upon the death of Dr. Beatty. In 1891 there were further (unspecified) improvements to the picnic grounds.

According to Steubenville's 1910 history, "Through this ravine have been constructed beautiful walks and driveways, in fact, making a park which has always been free to the public, without any expense to the city."

In 1902 the electric streetcar on the hilltop was extended to the cemetery entrance on Market Street. The existing administration building, including the superintendent's residence, and the Market Street entrance with stone posts, wrought iron gates and fence, were built in 1931.

On August 4, 1930 a tract of 99 acres was sold to the City, leaving (at that time) 120 acres for Union Cemetery. The city formally named the tract Beatty Park in honor of Dr. Beatty; as will be seen, this action occurred during the period before World War II when the City of Steubenville was taking aggressive action to provide park and recreational facilities for its citizens.

### **Facilities of the Steubenville Parks and Recreation Department**

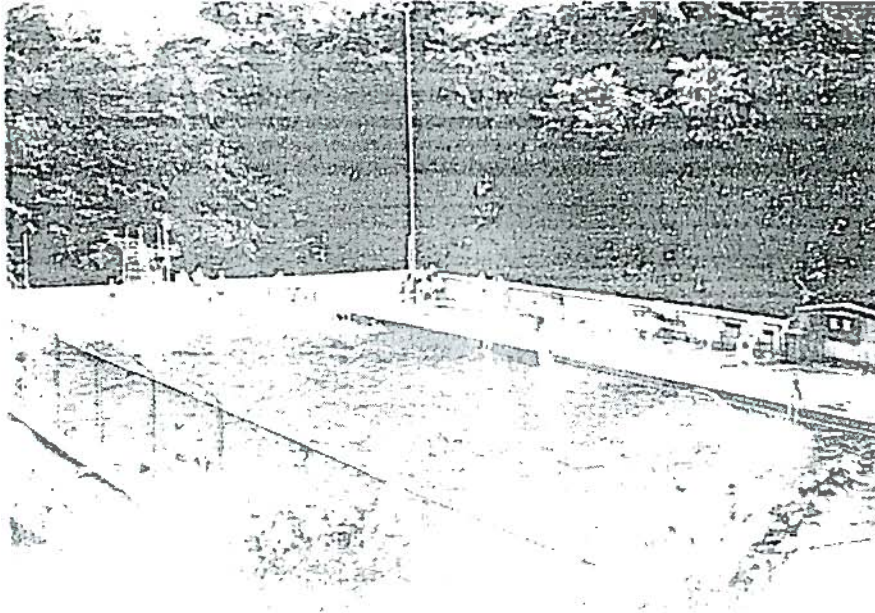
The Recreation Board of the City of Steubenville was authorized by City Council in 1924 and held its first meeting in January, 1925. Prior to creation of this public body, the major parks available to Steubenville residents were Beatty Park, owned and maintained by the Union Cemetery Association; Stanton Park, located north of the city and developed by private interests as a for-profit venture; and playgrounds sponsored by the Steubenville Womens Club.

From 1925 to 1940, under the auspices of the Parks and Recreation Department, there was an ambitious program of developing parks, playgrounds and permanent park facilities for Steubenville.

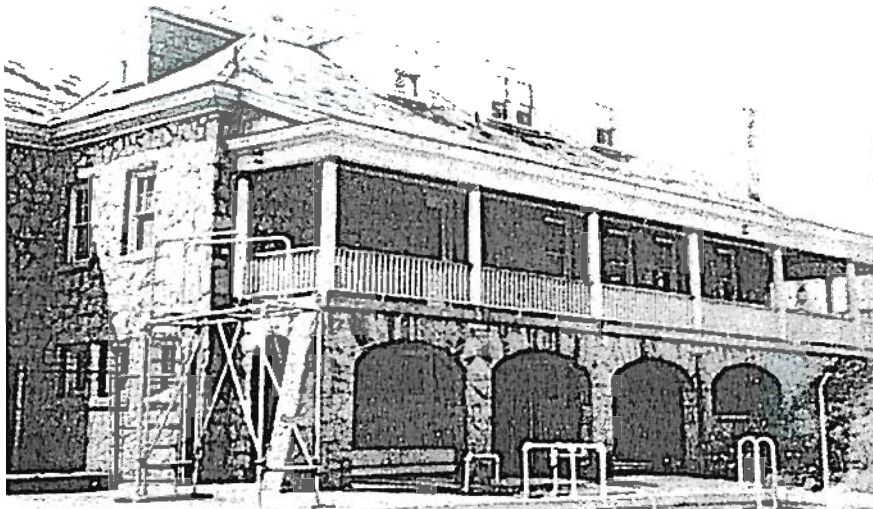
In 1925 the Department hired playground leaders, one man for the North End field and one woman at each of the others -- Lincoln Avenue, McKinley and La Belle View. The Department also approved construction of a shelter house at LaBelle View field (its location is uncertain). The summer playground program offered games, story hour, folk dancing, moving pictures, first aid classes, sewing, and other activities. The facilities were well used, sometimes with problems coming up. Among the comments in the year-end report on the playgrounds were, referring to the North End, "Attendance weakened . . . due to racial prejudice it is very difficult to organize harmonious groups for competitive sports and activities." Average daily attendance at the playgrounds was 136 at McKinley, 112 at LaBelle View, 102 at Lincoln, and 106 at North End, including Industrial League baseball.

In 1926 a committee met with officials of Wheeling Steel to discuss land near the steel plant for a playground; a plan was presented for "Flat Iron Park" on a triangular parcel at the intersection of McDowell & Woodlawn on the hilltop; a contract was made with McCauslen Florist for \$300 for park improvements; \$100 was spent on improvements to Pleasant Heights Park, also known as "Fire House Park" for its location next to the fire station; and several staff positions were created, including supervisors for Lincoln Avenue, Pleasant heights, LaBelle View, North End, and the La Belle Mill grounds.

In 1927 the Union Cemetery Association was approached by the Department about securing Beatty Park grounds (also known as Cemetery Hollow) for a playground. The idea was to combine South End and Lincoln Avenue groups in Beatty Park because there was "plenty of shade" and a site for swimming. A



The Beatty Park swimming pool was a major destination for Steubenville residents in the 1930s.



The clubhouse and pool at Belleview Park date from the 1930s and today remain major city recreational facilities.

resolution was passed calling for a \$100,000 bond issue for a new concrete and steel stadium and to purchase Beatty Park. Average daily attendance at parks was 250 at Beatty, 100 at North End, and 125 at La Belle View.

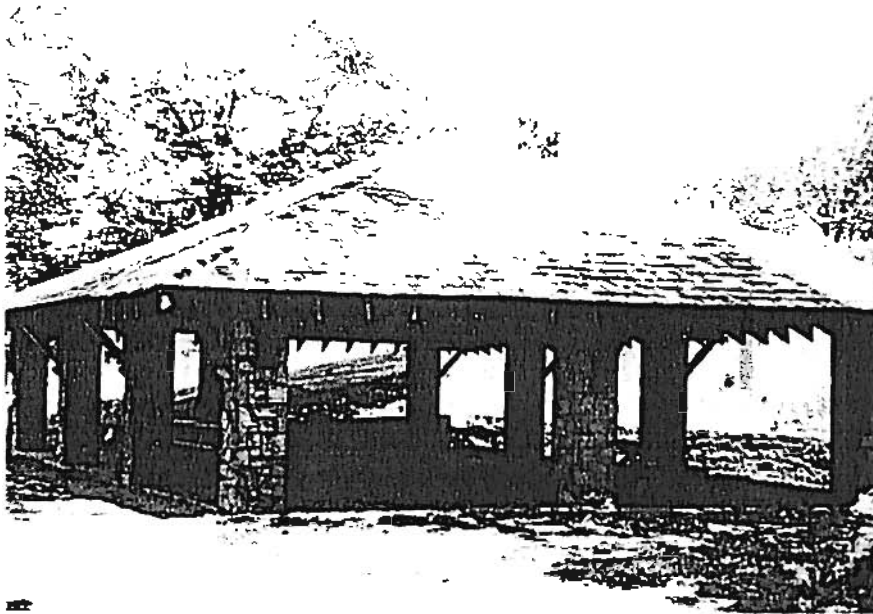
In 1928 bids were received from 6 companies for stadium construction, but the Board of Education had decided to build a stadium on the Harding School grounds, so the Department never built its own. Play equipment was added to the North End, Pleasant Heights, and La Belle View parks; the North End park was popular for ball games among various leagues and clubs in the city. There was discussion of the need for a Municipal Golf Course and a recommendation that it be built on the property known as "City Farm," later to become Belleview Park.

In 1929 City Council authorized plans for construction of a golf course, swimming pools, tennis courts, and associated buildings at Belleview; the Recreation Board decided to build the swimming pools first, the main pool design adapted from one furnished by the Portland Cement Company. Plans for the pool and the adjacent clubhouse were completed; the pool was to be 75 by 150 feet and hold 500,000 gallons of water; \$5,000 was budgeted for the clubhouse. Floto Bros. was awarded the contract for construction of the pool for \$12,716; plans for construction of the clubhouse called for a 2-1/2-story frame building, the first floor for the bath-house, the second floor for golf, and the top 1/2 story for the caretaker. The golf professional at Riverview Country Club was hired to supervise construction of the municipal golf course; 6 fairways were cleared during the year.

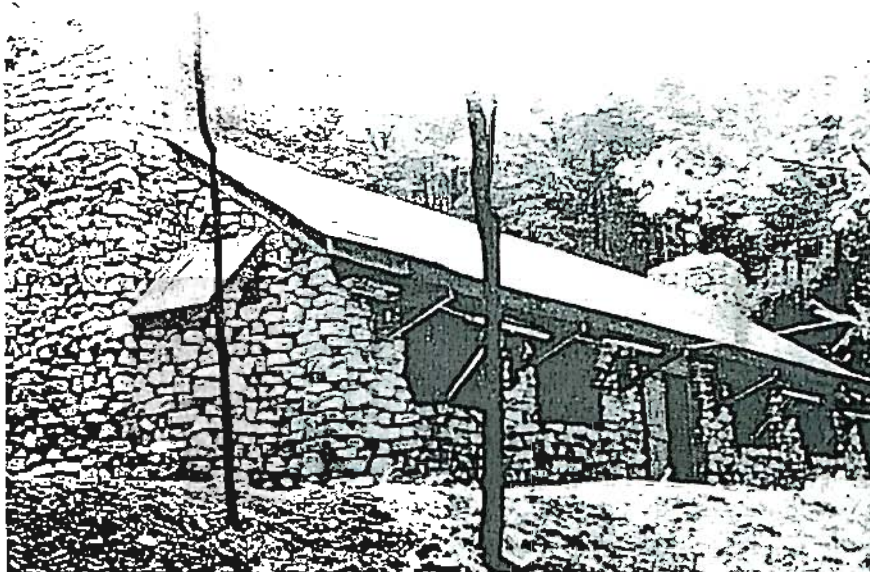
There was a recommendation to City Council to purchase Beatty Park; six city streets were to be set aside for winter "coasting," with barriers for cars, and there was brief board discussion of building a track for skiing.

1930 was another busy year for the Recreation Board and the Parks and Recreation Department. There was discussion of the purchase of Beatty Park and a recommendation that the director of Oglebay Park in Wheeling, West Virginia be consulted regarding improvements and the potential of the park as a "natural park as an aid to nature study"; the contract for a bath-house, for \$6,854, was awarded to Thomas C. Patton; there was a resolution to set aside 1,000 acres in Ross and Salem Townships as a forest and game preserve and to petition the state for funds to fence the land; 6,000 eggs were donated by Butte Laundry Company for an Easter Egg Hunt at Beatty Park (over 2,500 people attended), fees for the year were set at 35 cents for 9 holes of golf, 10cents an hour for use of tennis courts, and five to 20 cents for swimming; at a board meeting "a representation of colored people were present to protest against separation of the two races at the Municipal Swimming Pool"; there was a letter from the Women's Club stating opposition to a pool at Beatty Park because there would be too little sun; the club recommended purchase of a site elsewhere in the south end for a pool, Peterson & Clark and A.G. Whitehouse presented sketches and estimates for a swimming pool and clubhouse at Beatty Park (estimated cost of the pool and bathhouse at Beatty Park was \$26,293); all bids for the pool and clubhouse were rejected as too high and the architects were directed to revise the plans.

In 1931 Floto Bros. received the contract for the Beatty Park pool for a price of \$24,119; architect Whitehouse was asked to prepare plans and spe-



These views show Belleview Park picnic shelters shortly after completion. They were built in a rough "rustic" style considered compatible with, and an outgrowth of, the park's natural character.



cifications for a shelter house and restrooms in Beatty Park, estimated to cost \$8,700. Architects Peterson & Clark were asked to prepare plans and specifications for a swimming pool and building not to exceed \$25,000 for the 10th and Adams center, which was renamed Central Recreation Center later the same year. A contract was awarded to Patton (no other name was given) to construct a Beatty Park shelter house; and Floto Bros. received a contract for the West Adams Recreation Center for \$14,370. 15,000 black locust trees were authorized for planting on the face of LaBelle View hill.

In a report on use of the facilities, some problems were cited -- overcrowding of pools at certain times; a golf league with 9 teams making it hard for others to use the course, and a petition objecting to the "colored swimming pool" on Adams Street. To the latter the Recreation Board responded that none of facilities it controlled was operated for the exclusive use of any one group.

By 1932 the effects of the Depression began to be reflected in some of the policies of the Recreation Board: pools were to be free to children under 12 every weekday morning and to unemployed adults on Monday; golf fees were made 25 cents on weekdays and 35 cents on weekends for 9 holes. At this time, use was still heavy -- Central Recreation Center attendance was approximately 3,500 a month. Library grounds formerly used for recreation were turned over to the Womens Club for a public vegetable garden and were no longer available for playground use; the playground equipment was moved elsewhere. The NAACP requested a drinking fountain and a tennis court at the Central Recreation Center, and a boys' club for ages 17 to 22 was organized at North End Field. The group met monthly, established its own by-laws, and was followed by a boys' club at Beatty Park for 14-to-20-year-olds.

Minutes of the board mentioned reduced receipts, since there were more free admissions to swimming pools and increased use of facilities. For instance, participation in softball increased from 46,000 in 1931 to 86,430 in 1932; baseball increased even more dramatically from 7,500 in 1931 to 67,820 in 1932; playground usage rose from 112,997 in 1931 to 181,168 in 1932; but golf, which required an entrance fee, dropped from 19,530 in 1931 to 11,545 in 1932. Several reasons for this change in statistics were given in the financial report: ". . . unemployment; lack of funds among the people that usually go away on summer vacations; increased leisure, due to lack of business, more facilities; increased activities." The annual report stated, however, that "Recreation is an investment in social progress, which will pay dividends in Americanism."

The Department's budget also reflected the impact of the Depression. From a 1931 budget of \$31,197, funds declined to \$21,324 in 1932, and to \$14,345 in 1933 before beginning to rise slowly. The budget had not quite reached its 1931 level as late as 1937. By 1932, however, only seven years after establishment of the board and the Parks and Recreation Department, the city had 117 acres of parks, four children's playgrounds, three swimming pools, a golf course, three tennis courts, two picnic grounds, and two athletic fields. This was a lot of responsibility, and declining budgets did not help, but the system worked its way through the Depression years substantially intact.

In 1933, reflecting the community's deepening economic distress, rates were

reduced for golf, and cheaper season passes for golf were made available through clubs (season passes also included the pool). The Central Recreation Center offered feeding programs for children, and a recreation center for unemployed adults was established in the St. Paul's Church basement. Beatty Park was closed for the summer, except for picnic use and American Legion band concerts on Sundays. There was proposal, later carried out, for a Boy Scout headquarters in Beatty Park, to be made of logs furnished by the scouts with labor and other materials provided by the Civil Works Administration (CWA), a federal program. Between 1933 and 1936 the CWA built 200 swimming pools, 3,700 playgrounds, and many stadiums, bathhouses, boathouses, paths, trails, and lakes all around the country to provide recreational activities for the public.

In 1934 there was discussion of applying for CWA funds for completion of improvements at Belleview, Beatty and the Central Recreation Center. Architects Whitehouse and Johnson presented plans for altering the gateway to Beatty Park, for a new outdoor theater, and for some new walls; \$3,000 in CWA funds were committed to Belleview, but there was no description of specific work. Plans were developed for a Civilian Conservation Corps camp, and picnic tables were built by Federal Emergency Relief Administration carpenters. The Pennsylvania Railroad turned over the old C&P station (down near the riverfront) for the Department. The station was dismantled and stored at Belleview Park, but nothing is known of what became of it.

By 1935 the Central Recreation Center's year-old choir had 200 members and gave a concert; there was an increase in attendance at Central after the 1934 start of a music institute. Some residents of Kendall Ave organized to oppose wintertime coasting, which was also allowed on Linden, South Bend, Maxwell, and McKinley Avenues. New activities included a baseball school, a marbles tournament, plays, and a boating committee to draw up plans for riverfront boating. A delegation of "colored citizens" attended a board meeting to request more consideration of the Central Recreation Center, which was the one serving the greatest number of black residents. \$200,000 worth of Federal Emergency Relief Administration (FERA) and Works Progress Administration (WPA) projects were requested, including a clubhouse, Beatty Park improvements, a shelter house, an amphitheatre, bridges and tennis courts.

In 1936 the WPA opened a feeding station at the Central Recreation Center; approximately 75 black children were being served, with the local share underwritten by the Colored Ministerial Alliance and the Rotary and Kiwanis Clubs. The bathhouse at Beatty Park was completed. A letter from the WPA in Ohio to the Recreation Board stated, "The major objective of the WPA in Ohio has been to place needy able-bodied men and women to work. We have been guided by a fundamental principle that all work should be useful . . . in the sense that it affords permanent improvement in living conditions and creation of a new wealth for your community and the nation." There was an offer by the Steubenville Pottery Company to let the city use the "pottery lot" (the site of the demolished original pottery, which was vacated when the firm moved to a new facility in 1925) until it was leased or sold. A policy was adopted to prohibit beer and liquor from city parks and recreation centers. In another decision, one hour a day was to be devoted to victims of infantile paralysis for exercise and swimming lessons at Department facilities. Work on a double shelter at Beatty Park

started this year; and work began at the entrance to Belleview Park, including two arched sidewalk entrances. The Central Recreation Center project was never approved by the WPA.

For 1937, the list of activities for the year included a basketball tournament; an Easter egg hunt; kite, model airplane, marble, horseshoe, golf, and swimming tournaments; and a one-act play tournament. Playground activities included a doll show, vehicle week, handicraft week, patriotic week, nature week, and dramatic week. The board received a petition with 400 names requesting ice skating on the tennis courts in Belleview Park, which was approved along with \$600 to implement the activity. In a summary of improvement expenditures, the board reported that \$190,000 in bond issues covered the following work between 1928 and 1937: the purchase of Beatty Park (\$50,000); improvements to Belleview Park (\$40,000); improvements to Beatty Park (\$40,000); North End Field purchase (\$35,000); and construction of the Central Recreation Center (\$25,000).

With the economy improving, expenditures and activity picked up in 1938. Board minutes included the following: a proposal in the budget for four rustic bridges and repair of the Scout Cabin in Beatty Park and to complete additional rooms at the Central Recreation Center; a lease on the Pottery Lot for one dollar a year for five years, the Buena Vista neighborhood was given \$500 for purchase of playground equipment for a school, and \$1,000 was appropriated for improvement of the Pottery Lot.

The mysterious disease polio (infantile paralysis) affected the community, and the Recreation Board voted to offer free swimming passes for crippled children. The WPA continued funding projects, and there was a mention of \$200,000 being available during the year, although no specifics were given about the improvements to be made. The Polish Citizens Club requested a playground for the 3rd Ward because it had one of the largest population of children in the city. The board recommended to city council that \$1,000 be provided for playground equipment and a supervisor in the 3rd Ward on property behind and owned by St. Stanislaus Church.

There was a WPA citywide beautification project, but no details about it were given. A winter program was to be held at the Armory with WPA workers; attendance for nine weeks was 6,878, including 539 adults. There was another request for the Community Recreation Center for WPA funds of \$13,000 for improvements, with mention of it being known as the "Colored Center", citizens petitioned City Council for more money for the CRC, citing improvements already being made at Beatty and Belleview Parks. WPA and National Youth Administration (NYA) workers were at recreation centers consisting of five recreation workers, three penny lunch, one child welfare, and five adult education (all WPA); and three recreation, three penny lunch and one child welfare (all NYA).

A brief discussion of segregation of swimming pools included mention of a Mexican boy swimming at Beatty Park. It should be noted here that, though occasional ethnic or racial conflicts are noted in the written record, there never appear to have been serious incidents, violence, or ongoing tensions. Though the minutes never mention how these occasional issues were dealt with, they indeed appear to be only occasional, isolated complaints from individuals.

In 1939 there was discussion of a proposal from the National Exchange Bank to purchase Murphy Field, located on the hilltop west of Pleasant Heights. Mr. Murphy asked \$50,000 for the property while living; it was now available for \$22,000 and was one of last available pieces of property in Pleasant Heights -- a total of 51 lots in Murphy's Third Subdivison. The board reviewed a proposal from J.S. Williamson and Frank A. King to plant trees and shrubbery along Ross Park Boulevard where the streetcar tracks had been (they had stopped running the previous year). There also was approved a request from Downey Bros. Circus for use of the North End Field for equipment storage. And, this year \$115,000 was in a WPA allocation, but specific projects were not outlined.

In 1940 the board recommended to City Council that a portion of ground from the North Field be sold to the Valley Motor Transit Company for garage and storage space to employ 13 people, but that it should not be sold for less than \$4,500. The Cole Brothers Circus used the lower field at Belleview Park on July 7th and 8th. Attendance at indoor recreation centers was over 170,000 for the season, more than 10,000 above the previous year's attendance.

In 1941 Director McClintock resigned to assume a position with the National Recreation Association, to help set up recreation programs in communities where defense industries and military camps had been established. \$4,600 was received from the sale of a portion of the North End Field to the Valley Motor Transit Company, leading to a lengthy discussion about how to spend the money -- whether on softball fields, lights, bleachers, playgrounds, a community center, and so on. There was mention of NYA workers attending a workshop for training in defense industries.

1942 board minutes mentioned loss of WPA and NYA help and a change of focus to the war effort. There was discussion of a newspaper article on the connection between recreation and the national defense; one board member said, "Since Steubenville is an industrial area, we must do everything possible to build up the morale of the mill workers as well as that of the other citizens." No further WPA or NYA assistance was available for construction, so the board decided to let the \$4,600 from the sale of North End Field land sit until after the war and then make improvements to the balance of the North End Field. This year saw the beginning of resignations of staff members to enlist in the military, and there was mention of a shortage of labor and materials and of a smaller operating budget. There was an announcement that the WPA Recreation Project would be discontinued on January 15, 1943. It had been worth about \$18,000 a year in staff salaries for the Department. The need for a playground in the south end was remedied by means of a "play street" on South Sixth Street, which was quite successful.

1943 brought a much reduced budget and board discussion of a choice between operating the golf course and the swimming pools, or other recreational programs. The need for staff at the Community Recreation Center was discussed, because it had become more than just a recreation center -- it also provided adult education and welfare and health services for black citizens. Part of golf course -- fairway #8 -- was used for mail plane pick-up and drop-off, during the season the golf course was not in use, by All American Aviation, Inc.

In 1944 plans and specifications for improvement of the North End Field were submitted with estimates of \$4,539 plus \$350 for playground equipment, but the decision to proceed was tabled.

In 1945 a playground was approved for the North End Field for cost not to exceed \$1,000. The board agreed to improve Murphy Field in Pleasant Heights Park.

Important individuals associated with recreation and open space in Steubenville included Homer Fish, who in 1930 became the first full-time director of the Parks and Recreation Department. He made the department into a full-scale, staffed municipal department and undertook an aggressive building and capital improvements program with greatly expanded activities available to the public. Fish was followed, between 1935 and 1941, by Ralph McClintock, who continued the aggressive development of the system begun by his predecessor. And, finally, there was A.G. Whitehouse, an architect who worked for the department for several years during the period before World War II.

As can be seen from this record, the City of Steubenville took seriously its responsibility to provide park and recreational facilities to its citizens. Over this 20-year period, the city's efforts increased rapidly during the early years; suffered declines during the early part of the Depression, while still providing innovative services keyed to people's needs; gradually took advantage of funding, construction, and staffing opportunities provided by the alphabet soup of government agencies fighting the effects of the Depression; and saw another decline due to budget and staffing constraints during the war years. By the end of the war, the city's system of parks and recreational facilities had largely assumed the form and scale they have today.

## **Other Recreational Opportunities and Events in Steubenville**

### **Revivals**

According to local newspaper reports, the Billy Sunday Revival held on October 9-30, 1913, was considered one of biggest nights ever in the history of revivals, which were a common form of entertainment/edification during the late 19th and early 20th centuries. Reports described thousands of people, from all walks of life, marching with banners and bands playing. Representatives of most of the Protestant churches attended as delegations, with banners and singing their favorite hymns. The statistics given for the six-week revival included 7,888 conversions and \$11,345 in donations. On the final evening, it was reported that 5,000 people had to be turned away. There was no specific mention of where the Sunday revival was held, but there was mention of crowds on upper Market Street, and it is likely that the site was one of the areas of open ground where circuses were also held.

### **Circuses**

Circuses came to Steubenville on a regular basis, usually once a year, from the late 19th century through at least the first three decades of the 20th century. During this period, the circuses were held in various locations.

They used the railroads for transportation, and the set-up and parade through town were part of the show.

Before the turn of the century, circuses set up in the area where Wheeling Steel is now located, in the south end of the city, and later the location was moved to Pleasant Heights in the Ross Park area. According to local historians, some residents of Park Street sold lemonade to thirsty patrons over their back fences. As houses were developed on some of the lots, the next location for the circus was in the LaBelle View subdivision on a terrace between Roosevelt Avenue and Franklin Avenue.

The Market Street hill was paved with Belgian block, and horses hauling circus wagons had a hard time pulling the wagons up the steep grade. Drivers stopped at the stone watering trough midway up the hill to water the horses and other animals. This trough was constructed by Floto Bros. in 1910 and still survives along a bypassed section of the Market Street hill. The trough and a section of the street, now called "Old Market Street," were listed in the National Register of Historic Places in 1992.

As LaBelle View developed, a later site for the circus was the Brady Farm (later Brady Estates), west of the top of the Market Street hill at the point where Sunset Boulevard began. Development of this subdivision did not occur until the 1930s and 1940s; A 1927 newspaper article mentioned the Hagenbach-Wallace Circus arriving by the Pennsylvania Railroad and making the "long haul" up to Brady Farm.

A 1904 account of the Ringling Brothers Circus described how the circus train arrived in sections, with diners and coaches first and then tents, baggage, sleeping, performers', and other employees' cars. The train arrived from Wheeling and unloaded between Washington and Logan Streets. The circus was held at the Pleasant Heights grounds, the parade went down Adams to Third, north on Third to Clinton, west on Clinton to Fourth, south on Fourth to Market, west on Market to Eighth, and then to the show grounds. According to a local historian, thousands lined the parade route. Streetcar service ran to the grounds throughout the run of the circus.

## **ASSOCIATED PROPERTY TYPES**

### **Description**

Surviving properties associated with this context include only Union Cemetery/Beatty Park and Belleview Park. Union Cemetery/Beatty Park were listed in the National Register in 1987, based on significance both for architecture and for the property's planned landscape features.

Belleview Park consists of the golf course, a large landscaped open space with associated pathways; the clubhouse, swimming pool, and clubhouse outbuilding; and the city's water filtration plant and high-pressure reservoir. The land where the park was built had been known as the City Farm, and the reservoir was built on the flat land at the south edge of the property in 1895; the existing pumping facilities date from 1915. The reservoir property today consists of the reservoir and its round control house; the filtration plant; and the pump house.

The golf course and pool were opened in 1930; the clubhouse dates from 1937. The date of the outbuilding is uncertain, but its built during the 1930-1937 period.

### Significance

Belleview Park is significant under Criterion A in the fields of POLITICS/GOVERNMENT and RECREATION. Because of its physical and functional relationship to Steubenville's hilltop area suburban neighborhoods, the park is significant under Criterion A as evidence of government efforts to provide significant amenities to encourage the development and stabilization of suburban areas in the early 20th century. As such, the park is part of the broad historical development of Steubenville.

Also under Criterion A, Belleview Park is significant as evidence of Steubenville's efforts from the 1920s on to provide municipally-owned, -sponsored, and -managed park and recreational facilities for all of its citizens. This was a major undertaking of the city government, and it operated with significant federal help from the relief programs of the 1930s. The park thus contributes to an understanding of the broad patterns of Steubenville's historical development.

Belleview Park is also significant under Criterion C as a historic district. Its components lack individual distinction, but taken as a whole they constitute a cohesive entity illustrative of early 20th century park and public recreation design. Such a potential district should include not only the clubhouse, pool, and nearby structures, but also the golf course, system of paths, and the reservoir and water filtration plant near the entrance. All of these components work together to create a definable entity.

### Registration Requirements

Belleview Park qualifies for listing under Criterion A because it has the following aspects of integrity:

#### 1. LOCATION

The property and all of its components are on their original sites and in their original spatial relationships to each other and to the hilltop area neighborhoods.

#### 2. DESIGN

The property and its components retain all their essential design features. The park land itself has its golf course and associated pathways intact. The various structures retain their original materials, building forms, massing, window and door openings, porches, and decorative features.

#### 3. ASSOCIATION

The property is sufficiently intact to convey the relationship between itself and the hilltop area neighborhoods and between itself and the rest of the city. No substantial changes have been made in the park's layout or design features; in the spatial relationship between park components and

buildings; or in the buildings themselves that contribute to the park's character.

#### 4. SETTING

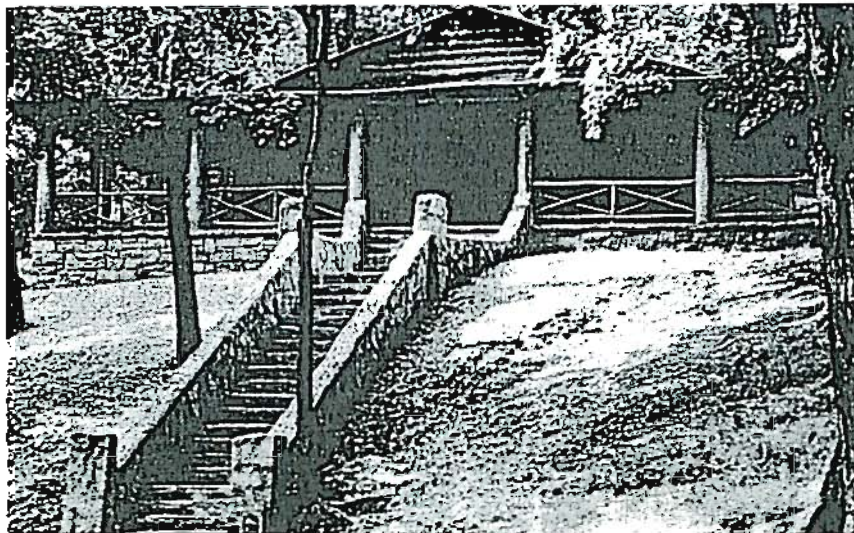
The property's setting is in largely original condition, with major landscape and design features intact and in their original spatial relationships. Spaces between buildings have remained largely unaltered; the golf course and associated pathways and plantings have not been significantly changed; and areas of open space which create vistas between various parts of the park have not been filled in with later construction.

#### WPA-Related Resources

Several buildings or structures were identified during research of this context as having been provided or built by the WPA. Based on the current level of research, all surviving WPA-related resources are in either Beatty or Belleview Parks and would be listed as components of one of these parks.

Outside these parks, it appears that no WPA-related resources have survived. However, should later research reveal the existence of such resources, the following questions may be useful in evaluating their significance:

1. Does the resource retain the necessary level of integrity?
2. Has the resource been definitely linked to the WPA through research in reliable sources?
3. Is the resource the work of an important local architect, or of an architect associated with WPA projects elsewhere?
4. Does the resource possess architectural or design significance, apart from its association with the WPA?



This view shows the Boy Scout Cabin constructed in Beatty Park.

## **Uses of the Contexts**

The City of Steubenville and the Steubenville Historic Landmarks Commission will use the contexts in this publication in four primary ways:

### **1. Planning**

In combination with previously-prepared Ohio Historic Inventory forms and survey reports, and with a historic preservation plan to be prepared by the city, the contexts will aid planning efforts by helping to identify significant historic properties worthy of preservation. Such properties, once identified, can be incorporated into the city's planning process so that important resources are not inadvertently lost.

### **2. Local and National Register Nominations**

The contexts establish a framework for evaluating proposed nominations to the national and local registers of historic properties. National listing can make significant investment tax incentives available to investors in historic property rehabilitation, while local listing can provide legal protection for listed properties when they are threatened with demolition or inappropriate alteration.

### **3. Section 106 Review**

Under Section 106 of the National Historic Preservation Act of 1966, any expenditure of federal funds that may affect a historic property must be reviewed in an effort to avoid adverse effects. The contexts provide a means of making timely evaluations of affected properties to determine whether they may be significant and thus subject to Section 106 protection. The ability to make such an evaluation quickly can save considerable time in the planning of federally-assisted projects.

### **4. Certificate of Appropriateness Review**

The contexts will aid the Steubenville Historic Landmarks Commission when it is reviewing proposed rehabilitation or alteration of properties designated as local landmarks or districts. The contexts provide a means of evaluating the significance of various kinds of properties and of assessing how much change can be borne by specific properties without loss of historic integrity or character.

In addition, these contexts may be used by the Ohio Historic Preservation Office and the National Park Service during the review of applications for federal investment tax credits for the rehabilitation of historic properties. They will also be a valuable educational tool and will be made available to all libraries and educational institutions.

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