

Shelton City Council Study Session Agenda October 24, 2023 – 6:00 p.m. Civic Center & Virtual Platform

- A. Call to Order
- B. Roll Call
- C. Study Agenda
 - 1. Frontage Improvements & Transportation Impact Fees Code Modifications Presented by Public Works Director Jay Harris
 - 2. Well #1 Pipeline Pressurization Project Presented by Capital Projects Manager Aaron Nix
 - 3. Safe Routes to School Project Update Presented by Capital Projects Manager Aaron Nix
- D. New Items for Discussion
- E. Adjourn



2023 Looking Ahead (Items and dates are subject to change)

Tues. 11/7 5:45 p.m.	Special SMPD Meeting	Consent Agenda	Packet Items Due: 10/27 @ 5:00 p.m.
Tues. 11/7 6:00 p.m.	Regular Meeting	 Vouchers/Payroll Warrants/Meeting Minutes Presentation Cruisin' Through Time Car Show LTAC Report Proclamation Operation Green Light Business Agenda Resolution No. 1294-0923 Master Fee Schedule Update Public Hearing Ordinance No. 2013-0923 2024 Preliminary Budget Public Hearing Ordinance No. 2014-0923 2024 Ad Valorem Taxes Resolution No. 1295-1023 Well #1 Pipeline Pressurization Contract Award Action Agenda Designated Crisis Responder Contract for Services LTAC Tourism Grant Recommendations Civic Center Rotating Art Gallery Administration Report 	Packet Items Due: 10/27 @ 5:00 p.m.
Tues. 11/14	Study Session	 Study Agenda Property Maintenance Code Advanced Meter Infrastructure (AMI) Meter	Packet Items Due:
6:00 p.m.		Installation Project (Overview)	11/9 @ noon
Tues. 11/21	Special SMPD	Consent Agenda • Vouchers/Meeting Minutes Business Agenda •	Packet Items Due:
5:45 p.m.	Meeting		11/9 @ 5:00 p.m.

		Action Agenda • Public Hearing Resolution No. SMPD 38-0923 2024 Budget • Public Hearing Resolution No. SMPD 39-0923 Ad Valorem Taxes Administration Report	
Tues. 11/21 6:00 p.m.	Regular Meeting	Consent Agenda	Packet Items Due: 11/9 @ 5:00 p.m.
Tues. 11/28 6:00 p.m.	Study Session	Study Agenda •	Packet Items Due: 11/22 @ noon
Tues. 12/5 6:00 p.m.	Regular Meeting	Consent Agenda	Packet Items Due: 11/22 @ 5:00 p.m.

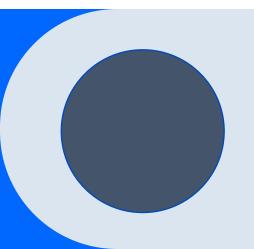
		Public Defense Contract Administration Report	
Tues. 12/12 6:00 p.m.	Study Session	Study Agenda	Packet Items Due: 12/8 @ noon
Tues. 12/19 5:45 p.m.	SMPD Meeting	Consent Agenda • Vouchers/Meeting Minutes Business Agenda • Action Agenda • Administration Report •	Packet Items Due: 12/8 @ 5:00 p.m.
Tues. 12/19 6:00 p.m.	Regular Meeting	Consent Agenda	Packet Items Due: 12/8 @ 5:00 p.m.
Tues. 12/26 6:00 p.m.	Study Session	Study Agenda	Packet Items Due: 12/22 @ noon

Other – TBD

- Public Hearing Ordinance No. 1990-0522 Amending SMC 17.12
- Project and Funding Authorization for Wallace/Shelton Springs Intersection Improvements
- Property Maintenance Code

Frontage Improvements and Transportation Impact Fees (TIF) Code Modifications

Jay Harris, Public Works Director



What is a Transportation Impact Fee and General Facilities Charges?

Impact fees are one-time charges assessed by a local government against a new development project to help pay for new or expanded public capital facilities that will directly address the increased demand for services created by that development.

RCW 82.02.050.110 and WAC 365-196-850 authorize counties, cities, and towns planning under the Growth Management Act (GMA) to impose impact fees for:

- Public streets and roads,
- Publicly owned parks, open space, and recreation facilities,
- School facilities, and
- •Fire protection facilities.



Areas of focus in modifying Shelton Municipal Code pertaining to Frontage Improvements (Chapter 12.36) and Transportation Impact Fees (Chapter 17.12)

- Including an in-lieu of fee program for streets, sidewalks, curbs and sidewalk restoration. Fees to be in Master Fee Schedule.
- Removing In-fill lots definition.
- Aligning the City's deferral program of impact fees with Washington State statutes.
- Utilizing the annual Transportation Improvement Program (TIP)
 update, identifying capacity related projects in which transportation
 impact fees can be dedicated towards, instead of a codified list
 within the code (as currently exists). Fees will remain codified, as
 required under law.

Summary

Staff has taken on the task of revising sections of our transportation impact fee and general facility charge sections of the Municipal Code. The intent of the changes are to ensure clarity of the City's program(s), ensuring compliance with legislative changes that have occurred since inception and providing additional flexibility for payers of transportation and utility impact fees. Staff is proposing future Council meetings and public hearings to modify the current code provisions. Staff has taken on the task of



Thank you

Questions!

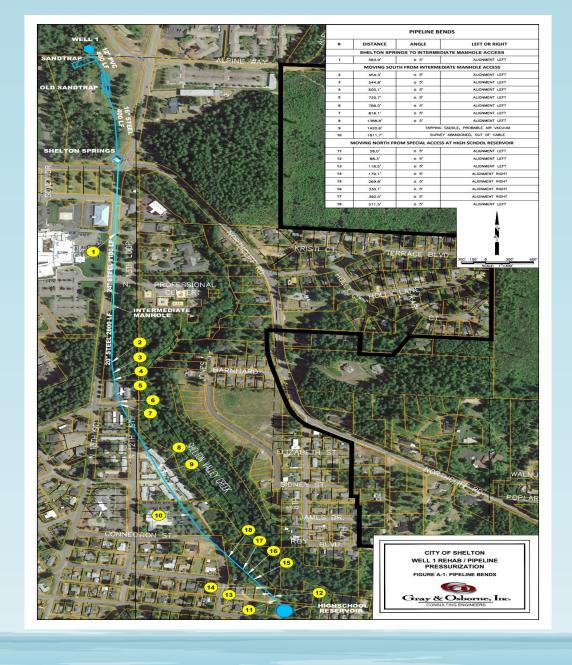


Well #1 Pipeline Pressurization Project

Project Background

- The Well 1 Rehab Project commenced in 2018 with Gray & Osborne, Inc. initiating pre-design efforts. The consultant's progression through design of the project allowed City staff to seek funding assistance through a State legislative appropriation request.
- ☐ In September of 2021, the City was notified of its successful petition, securing a net amount of \$2,000,000 in the 2021-23 State Capital Budget, utilizing the State's Coronavirus State Fiscal Recovery Funds through the federal American Rescue Plan Act (ARPA), administered through the State Department of Commerce.
- Contract provides for the pressurization of the transmission pipeline extending from Well 1 to the High School Reservoir. The work involves modifications at Wells 1 and 4, replacement of approximately 800 LF of pipe from Well 1 to Shelton Springs by open-cut methods, pipe replacement and other modifications to the Shelton Springs site, and replacement of approximately 3,900 LF of pipe from Shelton Springs to the High School Reservoir by sliplining methods. The project will include, but not be limited to, excavation, grading, piping, valves, appurtenances, and all other work specified for successful completion of the project.
- Design and permitting were completed late this Summer and the project is in out to bid, with the bid opening scheduled for 11:30 am on November 1, 2023, at the Civic Center. The formal plan set, and cost estimate have been included as part of the Council's packet materials.

Extent of the Project Area





Well #1 Pumphouse

Sand trap removal and upgrades to the existing facility, better security around the facility and easier access.



Spring House

South end of spring box with overflow from Shelton Creek (Left) and a valve at pipe to manhole chamber (Right).

Replacement of valve and connection to the spring box (emergency water source).



High School Storage Tank

Cast iron pipe running into tank will be replaced with a 12" ductile iron, pressurized water line. Fencing will be improved/expanded, and drought tolerant landscaping planted on steep slopes.

Tank is planned to be repainted in the FY 2025 budget.

Questions/Comments?

Safe Routes to School Project Update

October 24, 2023







Background

Shelton was awarded \$770,103 through a competitive process in Safe Routes to School State funding in July 2021. The grant funds are for design and construction and require a 13.5% city match.

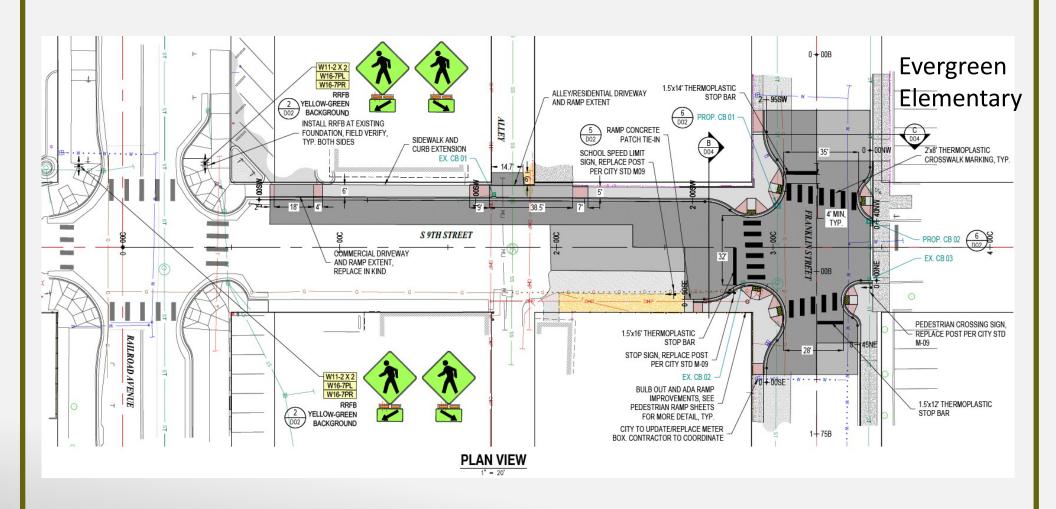
The SRTS scope of work includes crosswalk improvements at 7th and Franklin, near Safeway; 9th and Railroad Ave to Evergreen Elementary; and Shelton Springs Road at the High School and Huff & Puff Park.

RH2 Engineering is completing the construction plans and bid package for the project. The City expects to have the bid results late next month with Council awarding the construction contract in November/December of this year.

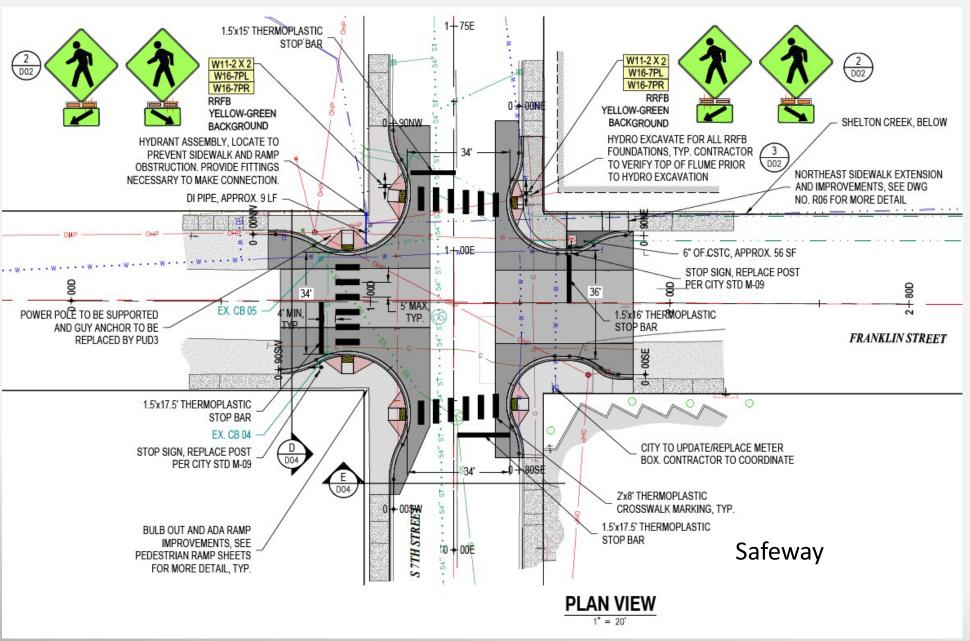
The budget for the project is \$890,300, and the updated engineers estimate is \$889,189 for the improvements that includes a small construction contingency. Staff is requesting an additional \$90,000 in the FY 2024 budget for an additional construction contingency for the project.

Item Number	Total Quantity Uni		Item	City	U	Init Cost	F.	ngr. Est.
Number	Quantity		Tax Schedule - See specification section 1-07.2(1)					gr. Est.
			PREPARATION		- 2	8		
1	1	L.S.	MOBILIZATION	1	5	75,792.00	S	75.79
2	1	L.S.	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	1	S	20,000.00	S	20.00
3	120	S.Y.	REMOVING CEMENT CONC. SIDEWALK	120	\$	30.00	\$	3,60
			SURFACING/PAVING					
4	220		CRUSHED SURFACING TOP COURSE	220	\$	100.00	\$	22,00
5	420	TON	HMA CL. 1/2" PG 64-22	420	\$	150.00	\$	63,00
6	730	TON	CRUSHED SURFACING BASE COURSE	730	\$	110.00	\$	80,30
			GRADING					
7	640	C.Y.	ROADWAY EXCAVATION INCL. HAUL	640	\$	30.00	\$	19,20
			EROSION CONTROL AND PLANTING			- valeral		(Fe)
8	1	L.S.	EROSION CONTROL AND WATER POLLUTION CONTROL	1	\$	5,000.00	\$	5,00
			TRAFFIC					
9	720	L.F.	CEMENT CONC. TRAFFIC CURB AND GUTTER	720	\$	75.00	S	54,00
10	135	L.F.	PLASTIC STOP LINE	135	\$	15.00	S	2,02
11	710	S.F.	PLASTIC CROSSWALK LINE	710	\$	7.00	\$	4,97
12	1	L.S.	PROJECT TEMPORARY TRAFFIC CONTROL	1	\$	50,000.00	S	50,00
13	1	L.S.	RECTANGULAR RAPID FLASHING BEACON SYSTEM	1	\$	80,000.00	\$	80,00
14	1	L.S.	RADAR SPEED DISPLAY SIGN	1	\$	10,000.00	S	10,00
15	1	L.S.	PERMANENT SIGNING	1	\$	10,000.00	\$	10,000
	200		WATER LINES	50			Ž.	-
16	1	EA.	HYDRANT ASSEMBLY	1	\$	11,000.00	\$	11,00
			STORM SEWER					
17	2	EA.	CATCH BASIN TYPE 1	2	\$	6,500.00	\$	13,00
- Ingres			OTHER ITEMS	100				
18	14	EA.	CEMENT CONC. CURB RAMP TYPE PERPENDICULAR	14	\$	5,100.00	\$	71,40
19	560	S.Y.	CEMENT CONC. SIDEWALK	560	\$	150.00	\$	84,00
20	40	S.Y.	CEMENT CONC. DRIVEWAY ENTRANCE TYPE 1	40	\$	200.00	S	8,00
21	1	L.S.	CEMENT CONC. MEDIAN REFUGE ISLAND	1	\$	20,000.00	\$	20,00
22	4	EA.	MAILBOX SUPPORT TYPE 1	4	\$	800.00	S	3,20
23	145	L.F.	CHAIN LINK FENCE TYPE 3	145	\$	50.00	\$	7,25
24	1	EA.	ADJUST MONUMENT	1	\$	1,000.00	\$	1,00
25	1	EA.	ADJUST WATER VALVE BOX	1	\$	1,000.00	\$	1,00
26	1	EA.	ADJUST MANHOLE	1	\$	1,500.00	\$	1,50
27	1	EA.	ADJUST CLEANOUT	1	\$	1,000.00	\$	1,00
28	5	EA.	ADJUST CATCH BASIN	5	\$	1,500.00	\$	7,50
29	1	L.S.	ADA FEATURES SURVEY	11	\$	5,000.00	\$	5,00
30	5000	DOL	UNKNOWN UTILITY REPAIR	5000	\$	1.00	\$	5,00
31	1	L.S.	RECORD DRAWINGS	1	\$	2,000.00	\$	2,000
32	1	L.S.	ROADWAY SURVEYING	1	\$	5,000.00	\$	5,000
33	5000	DOL	MINOR CHANGE	5000	\$	1.00	S	5,0

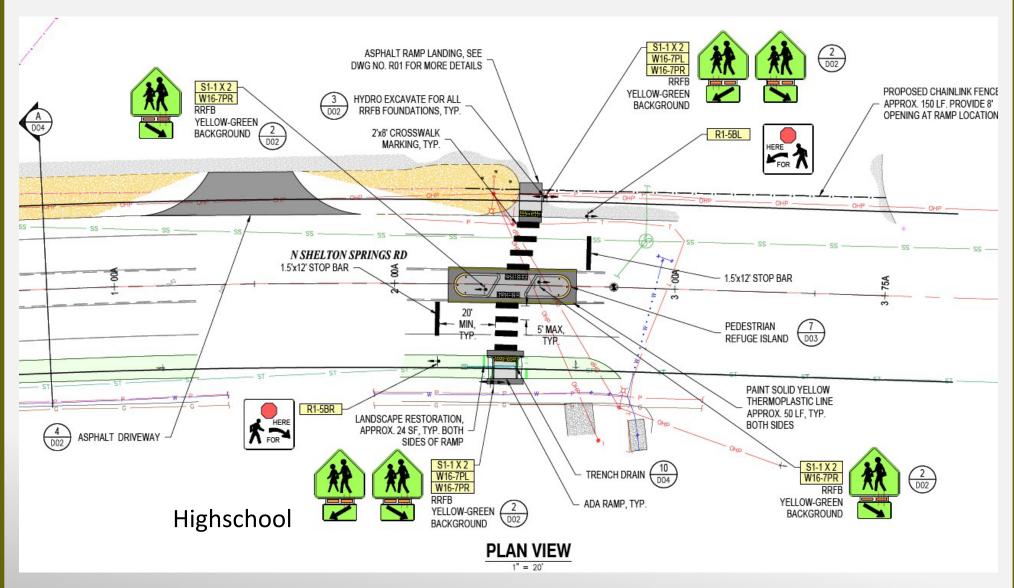
9th and Franklin Plan



7th and Franklin Plan



Shelton Highschool at Shelton Springs Road Plan



QUESTIONS?



For more information, please contact:
Aaron Nix
City of Shelton
360-432-5118
Aaron.nix@sheltonwa.gov

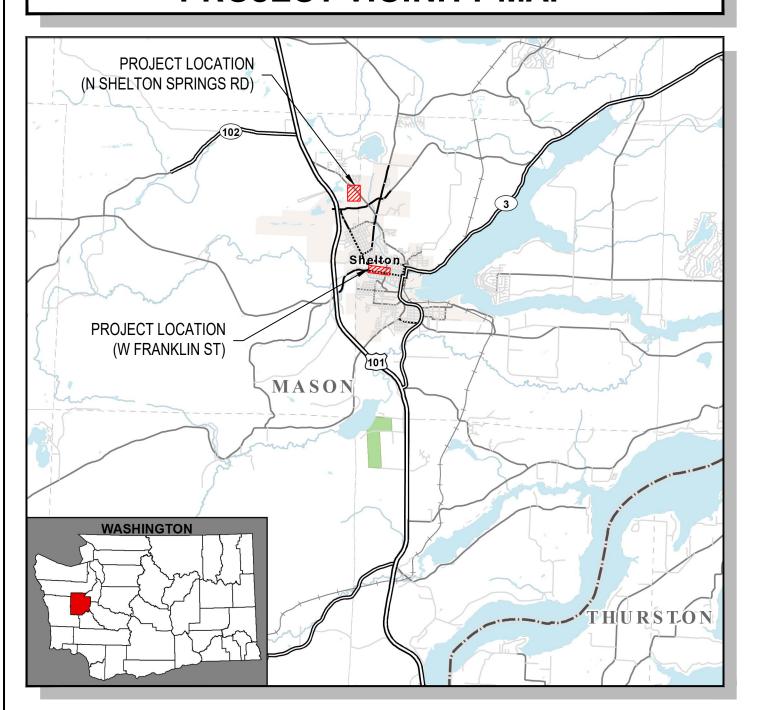


SHELTON SRTS CROSSWALK IMPROVEMENTS



22-0060

PROJECT VICINITY MAP



Item Number

Total Quantity

Tax Schedule - See specification section 1-07.2(1)							
			PREPARATION				
1	1	L.S.	MOBILIZATION				
2	1	L.S.	REMOVAL OF STRUCTURES AND OBSTRUCTIONS				
3	120	S.Y.	REMOVING CEMENT CONC. SIDEWALK				
			SURFACING/PAVING				
4	220	TON	CRUSHED SURFACING TOP COURSE				
5	420	TON	HMA CL. 1/2" PG 64-22				
6	730	TON	CRUSHED SURFACING BASE COURSE				
			OD ADINO				
	0.40	0.1/	GRADING				
7	640	C.Y.	ROADWAY EXCAVATION INCL. HAUL				
			EDOCION CONTROL AND BLANTING				
0	4	1.0	EROSION CONTROL AND PLANTING				
8	1	L.S.	EROSION CONTROL AND WATER POLLUTION CONTROL				
			TRAFFIC				
9	720	L.F.	CEMENT CONC. TRAFFIC CURB AND GUTTER				
10	135	L.F.	PLASTIC STOP LINE				
11	710	S.F.	PLASTIC CROSSWALK LINE				
12	1	L.S.	PROJECT TEMPORARY TRAFFIC CONTROL				
13	1	L.S.	RECTANGULAR RAPID FLASHING BEACON SYSTEM				
14	1	L.S.	RADAR SPEED DISPLAY SIGN				
15	1	L.S.	PERMANENT SIGNING				
	ı	L.S.	PERIMANENT SIGNING				
			WATER LINES				
16	1	EA.	HYDRANT ASSEMBLY				
10	1	LA.	TITUTANT ASSEMBLT				
			STORM SEWER				
17	2	EA.	CATCH BASIN TYPE 1				
	-						
			OTHER ITEMS				
18	14	EA.	CEMENT CONC. CURB RAMP TYPE PERPENDICULAR				
19	560	S.Y.	CEMENT CONC. SIDEWALK				
20	40	S.Y.	CEMENT CONC. DRIVEWAY ENTRANCE TYPE 1				
21	1	L.S.	CEMENT CONC. MEDIAN REFUGE ISLAND				
22	4	EA.	MAILBOX SUPPORT TYPE 1				
23	145	L.F.	CHAIN LINK FENCE TYPE 3				
24	1	EA.	ADJUST MONUMENT				
25	1	EA.	ADJUST WATER VALVE BOX				
26	1	EA.	ADJUST MANHOLE				
27	1	EA.	ADJUST CLEANOUT				
28	5	EA.	ADJUST CATCH BASIN				
29	1	L.S.	ADA FEATURES SURVEY				
30	5000	DOL	UNKNOWN UTILITY REPAIR				
31	1	L.S.	RECORD DRAWINGS				
32	1	L.S.	ROADWAY SURVEYING				
33	5000	DOL	MINOR CHANGE				

SUMMARY OF QUANTITIES

DRAWING INDEX

Sheet Number	Sheet Title	Dwg No
01	COVER	COV
02	GENERAL NOTES	G01
03	SHELTON SPRINGS RD OVERVIEW	G02
04	SITE PREPARATION AND ALIGNMENT CONTROL 1	P01
05	SITE PREPARATION AND ALIGNMENT CONTROL 2	P02
06	SITE PREPARATION AND ALIGNMENT CONTROL 3	P03
07	SHELTON SPRINGS RD PEDESTRIAN IMPROVEMENTS	C01
08	9TH AND FRANKLIN ST PEDESTRIAN IMPROVEMENTS	C02
09	7TH AND FRANKLIN ST PEDESTRIAN IMPROVEMENTS	C03
10	PEDESTRIAN RAMPS 1	R01
11	PEDESTRIAN RAMPS 2	R02
12	PEDESTRIAN RAMPS 3	R03
13	PEDESTRIAN RAMPS 4	R04
14	PEDESTRIAN RAMPS 5	R05
15	PEDESTRIAN RAMPS 6	R06
16	PEDESTRIAN RAMPS 7	R07
17	DETAILS 1	D01
18	DETAILS 2	D02
19	DETAILS 3	D03
20	DETAILS 4	D04
21	TRAFFIC CONTROL PLANS 1	TC01
22	TRAFFIC CONTROL PLANS 2	TC02
23	TRAFFIC CONTROL PLANS 3	TC03

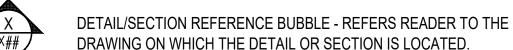
SECTION AND DETAIL REFERENCES

THE FOLLOWING CONVENTIONS HAVE BEEN USED WITHIN THESE DRAWINGS TO REFER THE READER BETWEEN THE SECTION/DETAIL AND THE PLAN FROM WHICH IT IS REFERENCED.

REFERENCE BUBBLES



PLAN REFERENCE BUBBLE - REFERS READER BACK TO THE PLAN FROM WHICH THE DETAIL OR SECTION ORIGINATED.



WHERE, X = SECTION/DETAIL REFERENCE ID*

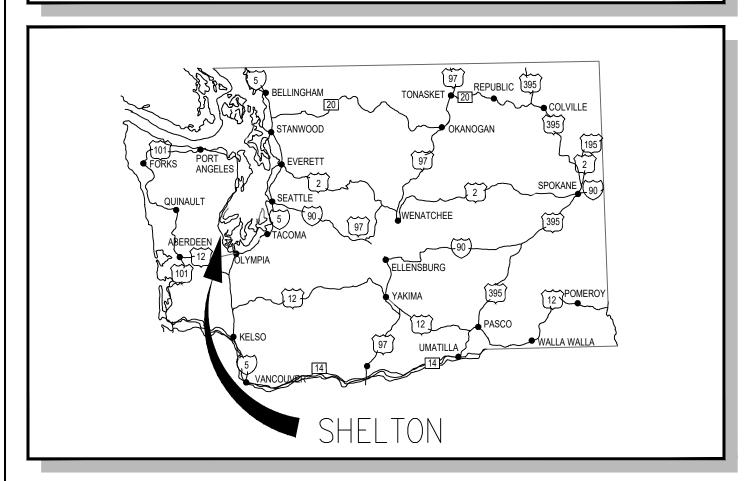
X## = DRAWING NUMBER ON WHICH DETAIL ORIGINATED OR RESIDES.

*SECTION/DETAIL REFERENCE ID CONVENTIONS:
SECTIONS OR ELEVATIONS SHOULD HAVE A LETTER REFERENCE ID
(A - ZZ) AND DETAILS SHOULD HAVE A NUMERICAL REFERENCE ID (0 - 999)

ABBREVIATIONS

СВ	CATCH BASIN	N	NORTHING
CONC	CONCRETE	PE	POLYETHYLENE
CL	CENTERLINE	PROP	PROPOSED
CPEP	CORRUGATED POLYETHYLENE	PVC	POLYVINYL CHLORIDE
CSBC	CRUSHED SURFACING BASE COURSE	R	RIGHT
CSTC	CRUSHED SURFACING TOP COURSE	RT	RIGHT
DIAM	DIAMETER	ROW	RIGHT-OF-WAY
DI	DUCTILE IRON	SPEC	SPECIFICATIONS
DWG	DRAWING	SS	SANITARY SEWER
E	EASTING	SSMH	SANITARY SEWER MANHOLE
ELEV	ELEVATION	ST	STORM
EOP	EDGE OF PAVEMENT	STA	STATION LINE
EX	EXISTING	STD	STANDARD
HMA	HOT MIXED ASPHALT	SY	SQUARE YARDS
L	LEFT	TYP	TYPICAL
LT	LEFT	W	WATER
LF	LINEAR FEET		

PROJECT LOCATION MAP



CALL 48 HOURS BEFORE YOU DIG ONE CALL 811

REPORT ALL SPILLS
DEPT. OF ECOLOGY 1-800-258-5990

SUBSURFACE UTILITY LEGEND

THE CLASSIFICATIONS FOR SUBSURFACE UTILITIES ARE OUTLINED AND EXPLAINED IN THE FOLLOWING LIST:

UTILITY QUALITY LEVEL A.

POTHOLE LOCATION

- PRECISE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES OBTAINED BY THE ACTUAL EXPOSURE OF (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITIES) AND SUBSEQUENT MEASUREMENT OF SUBSURFACE UTILITIES, USUALLY AT A SPECIFIC POINT. UNLESS OTHERWISE NOTED, QUALITY LEVEL A IS ONLY APPLICABLE AT POTHOLED LOCATIONS ON THE PLANS. AT ALL OTHER AREAS, THE UTILITY SHOULD BE ASSUMED TO BE QUALITY LEVEL B.

UTILITY QUALITY LEVEL B.

- INFORMATION OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS TO DETERMINE THE EXISTENCE AND APPROXIMATE HORIZONTAL POSITION OF SUBSURFACE UTILITIES.

UTILITY QUALITY LEVEL C.

- INFORMATION OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES

UTILITY QUALITY LEVEL D. - INFORMATION DE RECOLLECTIONS

- INFORMATION DERIVED FROM EXISTING RECORDS OR ORAL RECOLLECTIONS

NOTE: THE USE OF THE LINE TYPES PROVIDED ABOVE WAS A PRIMARY METHOD FOR INDICATING THE ACCURACY OF THE UTILITIES SHOWN WITHIN THESE PLANS. WHEN THE SOURCE OF THE INFORMATION WAS UNKNOWN OR THE METHOD FOR LOCATING THE UTILITIES WAS UNAVAILABLE, QUALITY LEVEL D WAS USED AS THE DEFAULT.

CONTACT PERSONNEL

CONTACT	AGENCY	PHONE
ERIK HOWE, P.E. (PROJECT MANAGER)	RH2 ENGINEERING	(509) 886-6761
JARED RIBAIL, P.E.	RH2 ENGINEERING	(253) 393-2964









LEGEND

EXISTING LEGEND PROPOSED LEGEND **DEMOLITION LEGEND** MONUMENT ————— SAWCUT LINE MAG NAIL CONTROL POINT ASPHALT REMOVAL IRRIGATION VALVE PLANING BITUMINOUS PAVEMENT WATER METER CONCRETE REMOVAL FIRE HYDRANT (2 PORT) VEGETATION REMOVAL AREA WATER GATE VALVE STORM DRAIN MANHOLE **ROADWAY LEGEND** CATCH BASIN (RECTANGULAR) MAJOR CONTOUR CULVERT DAYLIGHT MINOR CONTOUR SANITARY SEWER MANHOLE LANDSCAPE RESTORE BOUNDARY SANITARY SEWER CLEANOUT SIDEWALK TELEPHONE PEDESTAL **DRIVEWAY 2% SLOPE SECTION** TELEPHONE VAULT DRIVEWAY RAMP AREA 8% SI OPE UTILITY POLE ASPHALT OVERLAY UTILITY GUY ANCHOR COMMERCIAL HMA POWER JUNCTION BOX PEDESTRIAN RAMP WING POWER METER DETECTABLE WARNING/RAMP POWER VAULT WATER PIPE NATURAL GAS METER STORMWATER PIPE \leftrightarrow STREET LIGHT ASSEMBLY TYPE 1 CATCH BASIN TREE (DECIDUOUS) BOLLARD STRIPING LEGEND FENCE GATE POST PAINTED CROSSWALK BAR MAILBOX PAINTED STOP LINE STRIPING SYMBOLS PAINTED EDGE LINE STORM DRAIN CULVERT STORM DRAIN LINE WATER LINE NATURAL GAS UNDERGROUND TELEPHONE UNDERGROUND CABLE OVERHEAD CABLE UNDERGROUND POWER ———— OHP ———— OVERHEAD POWER RIGHT OF WAY CENTERLINE RIGHT OF WAY LINE MAJOR CONTOUR MINOR CONTOUR BUILDING EDGE BUILDING OVERHANG ______ PARKING LOT CURB

ROADWAY CURB

WOOD FENCE

EDGE OF STREAM

ASPHALT AREA

GRAVEL AREA

CONCRETE AREA

UNIMPROVED AREA

LANDSCAPING/SOD AREA

GENERAL NOTES

- 1. ALL WORKMANSHIP, CONSTRUCTION AND MATERIALS SHALL BE PERFORMED OR SUPPLIED IN ACCORDANCE WITH THESE SPECIAL PROVISIONS, PLANS, CITY OF SHELTON STANDARD DETAILS, AND THE WSDOT STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, 2023 EDITION, AS ISSUED BY THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION AND THE AMERICAN PUBLIC WORKS ASSOCIATION, WHICH IS HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS.
- 2. A PRECONSTRUCTION CONFERENCE IS REQUIRED PRIOR TO CONSTRUCTION, AND 48 HOURS ADVANCE NOTIFICATION PRIOR TO ACTUAL START OF WORK IS REQUIRED.
- 3. THE EXISTING TOPOGRAPHIC AND PHYSICAL FEATURES SHOWN ON THESE PLANS ARE BASED ON A FIELD SURVEY BY SITTS AND HILL, RECORD DRAWINGS, AND FIELD RECONNAISSANCE BY RH2 ENGINEERING.
- 4. THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN ON THE PLANS WERE OBTAINED FROM AVAILABLE RECORDS BUT HAVE NOT BEEN EXPOSED AND MEASURED. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING WORK TO AVOID DAMAGE OR DISTURBANCE, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UNDERGROUND UTILITIES. IT IS UNDERSTOOD THAT OTHER ABOVE GROUND AND UNDERGROUND FACILITIES NOT SHOWN ON THE PLANS MAY BE ENCOUNTERED DURING THE COURSE OF THE WORK.
- 5. THE CONTRACTOR SHALL PROTECT BUILDINGS, FENCES, APPURTENANCES, ABOVE GROUND UTILITIES, AND OTHER PROPERTY ADJACENT TO ALL CONSTRUCTION AREAS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR REPAIRING ALL DAMAGE CAUSED BY CONSTRUCTION ACTIVITIES.
- IN ACCORDANCE WITH THE DEPARTMENT OF ECOLOGY AIR QUALITY STANDARDS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ALL FUGITIVE DUST THAT MAY BE GENERATED BY THE CONSTRUCTION PROJECT.
- 7. THE CONTRACTOR SHALL CONTAIN WORK TO WITHIN RIGHT-OF-WAY OR THE CONSTRUCTION LIMITS AS ILLUSTRATED IN THE PLANS.
- 3. THE CONTRACTOR SHALL SECURE NECESSARY PERMITS PRIOR TO STARTING CONSTRUCTION. SEE SPECIAL PROVISIONS FOR FURTHER INFORMATION REGARDING PERMITS.
- 9. ONSITE EROSION CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND BE IN PLACE PRIOR TO CONSTRUCTION. ANY PROBLEMS OCCURRING BEFORE FINAL ACCEPTANCE BY THE CITY OF SHELTON SHALL BE CORRECTED BY THE CONTRACTOR. UPON FINAL ACCEPTANCE BY THE CITY OF SHELTON, OR AS OTHERWISE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE ALL TEMPORARY, NON-DEGRADABLE EROSION CONTROL MEASURES.
- 10. ANY REVISIONS TO PLANS MUST BE MADE BY THE ENGINEER AND APPROVED BY THE CITY OF SHELTON PRIOR TO ANY IMPLEMENTATION IN THE FIELD.
- 11. ALL PAVEMENT MARKINGS AND SIGNING SHALL CONFORM TO THE REQUIREMENTS OF THE MUTCD.
- 12. A COPY OF THE APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- 13. WHERE NEWLY CONSTRUCTED PAVING MEETS EXISTING PAVING, THE CONTRACTOR SHALL SAW CUT OR OVERLAY AND FEATHER NEW PAVEMENT TO PROVIDE A SMOOTH TRANSITION FROM EXISTING TO PROPOSED PAVING. APPLICATION OF A THIN TACK COAT OF EMULSIFIED ASPHALT SHALL BE APPLIED TO INSURE PROPER BONDING.
- 14. THE COMPLETED SURFACE OF ALL COURSES SHALL BE OF UNIFORM TEXTURE, SMOOTH, UNIFORM AS TO CROWN AND GRADE, AND FREE FROM DEFECTS OF ALL KINDS. THE COMPLETED SURFACE OF THE WEARING COURSE SHALL NOT VARY MORE THAN 1/8 INCH FROM THE LOWER EDGE OF A 10-FOOT STRAIGHTEDGE PLACED ON THE SURFACE PARALLEL TO THE CENTERLINE. THE TRANSVERSE SLOPE OF THE COMPLETED SURFACE OF THE WEARING COURSE SHALL VARY NOT MORE THAN 1/4 INCH IN 10- FEET FROM THE RATE OF TRANSVERSE SLOPE SHOWN ON THE PLANS.
- 15. MATERIALS SAMPLING AND TESTING SHALL BE AT A FREQUENCY AND MAGNITUDE AS SPECIFIED IN THE STANDARD SPECIFICATIONS OR DETERMINED BY THE ENGINEER. A PRIVATE AND INDEPENDENT TESTING LABORATORY SHALL PERFORM TESTING AND SAMPLING. CERTIFIED TEST REPORTS SHALL BE FURNISHED FOR ALL TESTS PERFORMED BY PRIVATE TESTING LABORATORIES. THE CITY OF SHELTON WILL BE RESPONSIBLE FOR ACCEPTANCE OF TESTING.

SURVEY NOTES

- . EQUIPMENT USED: TOPCON QS ROBOTIC TOTAL STATION AND TOPCON GR3 GPS SYSTEM
- 2. THIS SURVEY WAS PERFORMED BY FIELD TRAVERSE WITH THE FINAL RESULTS MEETING OR EXCEEDING THE CURRENT TRAVERSE STANDARDS CONTAINED IN W.A.C. 332-130-090. ALL MEASUREMENTS WERE MADE WITH A TOPCON QS ROBOTIC TOTAL STATION IN ACCORDANCE WITH THE EQUIPMENT MANUFACTURER'S SPECIFICATIONS.
- 3. IN ACCORDANCE WITH THE REVISED CODE OF WASHINGTON (R.C.W.) 58.09 AND THE WASHINGTON ADMINISTRATIVE CODE (W.A.C.) 332-130, THIS SURVEY MAY DEPICT OCCUPATIONAL INDICATORS THAT DIFFER FROM THE DEEDED LOT LINES. THESE INDICATORS, IF AT ALL PRESENT, MAY REPRESENT A POTENTIAL FOR CLAIMS OF UNWRITTEN TITLE. THIS SURVEY DOES NOT PURPORT TO RESOLVE SUCH ITEMS.
- 4. FIELD WORK PERFORMED IN FEBRUARY 2023, UNDER SITTS & HILL JOB NUMBER 20002.
- 5. UTILITIES AS SHOWN HEREON ARE BASED ON FIELD SURVEY OBSERVATION OF UTILITY LOCATE SERVICES PERFORMED BY C-N-I LOCATING SERVICES LLC., FOR THIS SURVEY. UTILITIES OTHER THAN SHOWN MAY EXIST ON THE SITE. THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED. THE SURVEYOR DOES CERTIFY THAT THEY ARE SHOWN AS ACCURATELY AS POSSIBLE FROM FIELD SURVEY AND PAINTED UTILITY LOCATE LINES. COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA IS CONSISTENT WITH QUALITY LEVEL (B) OF THE ASCE STANDARD GUIDELINES 38-02.

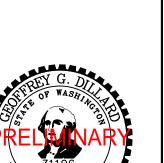
HORIZONTAL DATUM:
WASHINGTON STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NAD 83/11
BASED ON GPS OBSERVATION WITH A TOPCON GR3 GPS SYSTEM UTILIZING THE WASHINGTON STATE REFERENCE NETWORK

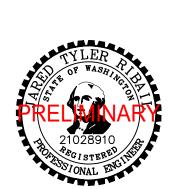
<u>VERTICAL DATUM:</u>
NAVD 88 ESTABLISHED BY GPS OBSERVATION WITH GEOID 2012B LOADED, AS PUBLISHED BY NATIONAL GEODETIC SURVEY

CONSTRUCTION NOTES

- ALL WORK IN THE CITY REQUIRES A CITY OF SHELTON BUSINESS LICENSE.
- 2. PRIOR TO WORKING WITH THE CITY RIGHT-OF-WAY OR ON CITY PROPERTY, THE CONTRACTOR MUST OBTAIN A CITY OF SHELTON RIGHT-OF-WAY PERMIT. ALL CONTRACTORS SHALL BE LICENSE AND BONDED IN THE STATE OF WASHINGTON. PROPONENT SHALL COMPLY WITH ALL OTHER PERMITS AND OTHER REQUIREMENTS OF THE GOVERNING AUTHORITY OR AGENCY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF RIGHT-OF-WAY.
- ALL CURB AND GUTTER, STREET GRADES, SIDEWALK GRADES, AND ANY OTHER VERTICAL AND/OR HORIZONTAL ALIGNMENT SHALL BE STAKED BY A LICENSED ENGINEERING OR SURVEYING FIRM CAPABLE OF PERFORMING SUCH WORK.
- WHERE NEW ASPHALT JOINS EXISTING ASPHALT, THE EXISTING ASPHALT SHALL BE CUT TO A NEAT VERTICAL EDGE AND TACKED WITH ASPHALT EMULSION TYPE CSS-1 PER SECTION 9-02.1(6) OF THE WSDOT/APWA STANDARD SPECIFICATIONS. TACK COAT SHALL BE APPLIED PER SECTION 5-04.3(5)A. THE NEW ASPHALT SHALL BE FEATHERED BACK OVER EXISTING TO PROVIDE FOR A SEAL AT THE SAW CUT LOCATION AND THE JOINT SEALED, IN ACCORDANCE WITH SECTION 5-04.3(5)C OF THE WSDOT/APWA STANDARD SPECIFICATIONS.
- 5. COMPACTION OF SUBGRADE, ROCK AND ASPHALT SHALL BE IN ACCORDANCE WITH THE MOST CURRENT ADOPTED VERSION OF THE WSDOT/APWA STANDARD SPECIFICATIONS.
- 6. FORM AND SUBGRADE INSPECTION BY THE CITY IS REQUIRED BEFORE PLACING ASPHALT OR CONCRETE. 24-HOUR NOTICE IS REQUIRED FOR FORM INSPECTION.
- TESTING AND SAMPLING FREQUENCIES WILL BE AS DESCRIBED IN THE CITY OF SHELTON DESIGN AND CONSTRUCTION STANDARDS.
- 8. DURING CONSTRUCTION, EXISTING MAILBOXES SHALL BE ACCESSIBLE FOR THE DELIVERY OF MAIL, OR, IF NECESSARY, MOVED TO A TEMPORARY LOCATION. TEMPORARY RELOCATION SHALL BE COORDINATED WITH THE U.S. POSTAL SERVICE BY THE CONTRACTOR. THE MAILBOXES SHALL BE REINSTALLED AT THE ORIGINAL LOCATION AT THE COMPLETION OF CONSTRUCTION OR, IF CONSTRUCTION HAS MADE IT IMPOSSIBLE, TO A LOCATION APPROVED BY THE U.S. POSTAL SERVICE AND THE CITY. NEW MAILBOXES SHALL BE LOCATED AS APPROVED BY THE U.S. POSTAL SERVICE AND THE CITY OF SHELTON. STRUCTURES SET IN CONCRETE SHALL ADHERE TO THE REQUIREMENTS OF DETAILS M-05, M-06, M-10, AND M-10A.
- 9. NOTIFICATION REQUIRED AT LEAST 1-WEEK IN ADVANCE OF FIRST PLANNED CONSTRUCTION WORKING DAYS THROUGH THE MEANS OF DOOR HANGERS AND READER BOARDS.
- 10. COMPACTION OF SUBGRADE, SUB-BASE, BASE, SURFACING, PAVEMENT, OR LAYERS OF SIMILAR MATERIAL SHALL BE IN ACCORDANCE WITH SECTION 2-06 AND SECTION 4.04.3(5) OF THE WSDOT/APWA STANDARD SPECIFICATIONS. FILL SHALL BE PROVIDED IN 6-INCH MAXIMUM LIFTS UNLESS OTHERWISE APPROVED BY THE CITY, AND SHALL BE COMPACTED TO 95 PERCENT OF ITS MAXIMUM RELATIVE DENSITY.
- 11. FILL SHALL BE PROVIDED IN 6-INCH MAXIMUM LIFTS UNLESS OTHERWISE APPROVED BY THE CITY, AND SHALL BE COMPACTED TO 95 PERCENT OF ITS MAXIMUM RELATIVE DENSITY.
- 12. INSPECTION BY THE CITY IS REQUIRED DURING ALL PHASES OF CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE CITY OF HIS/HER SCHEDULE. INSPECTION BY CITY PERSONNEL SHALL BE REQUIRED DURING CITY WORK HOURS. CITY WORK HOURS ARE 7:00 AM TO 6:00 PM MONDAY THROUGH THURSDAY, EXCEPT HOLIDAYS. A MINIMUM 24 HOUR NOTICE IS REQUIRED FOR INSPECTION OF ANY WORK. NO TRENCHING ON PUBLIC RIGHT OF WAY AFTER 4:00 PM UNLESS APPROVED BY THE CITY. OTHER WORK SUCH AS BACKFILLING, CLEANUP, ETC., OR AS APPROVED BY THE CITY IS ACCEPTABLE IF 24 HOURS' PRIOR NOTICE IS PROVIDED TO THE CITY.







FLTON

RTS CROSSWALK IMPR



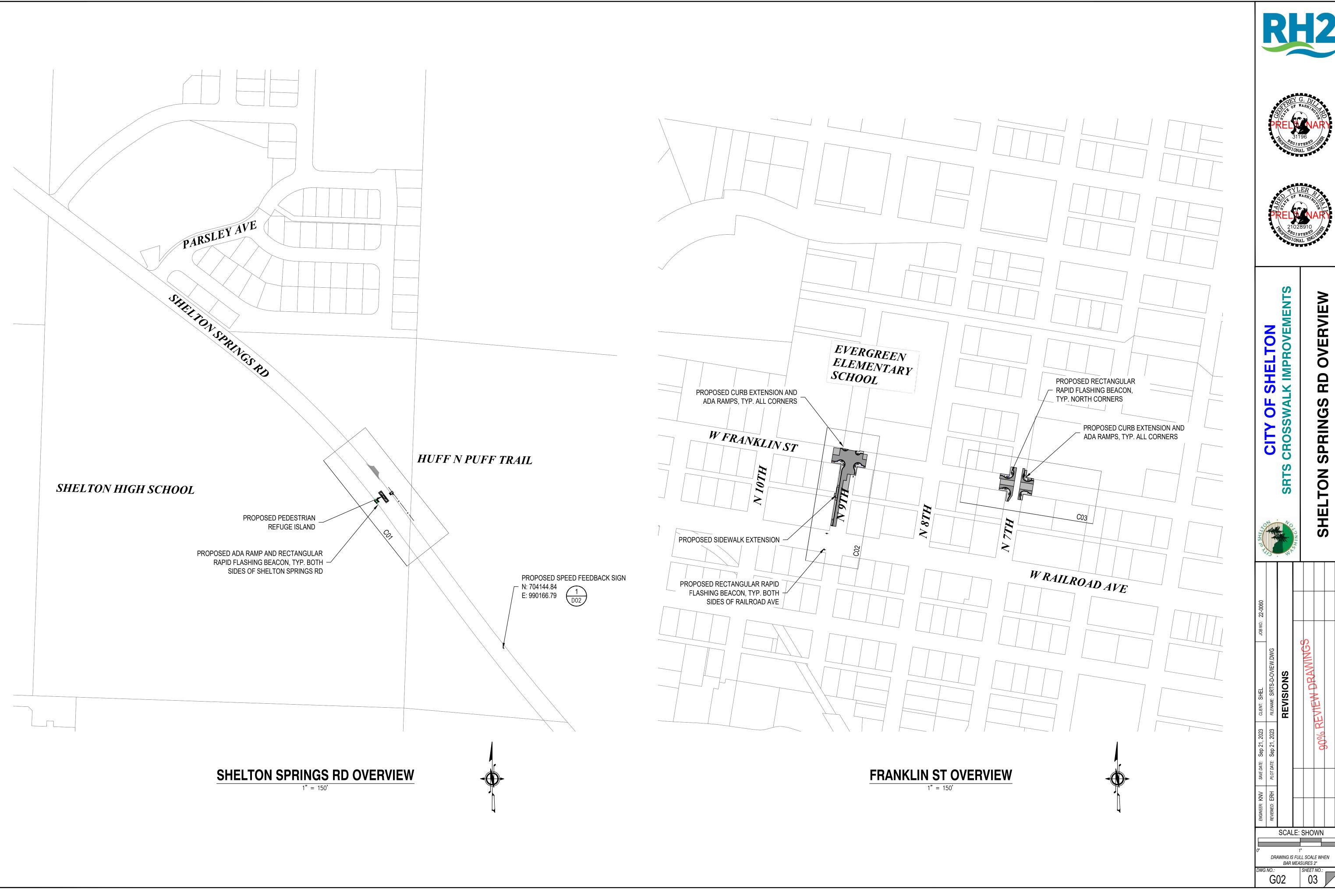
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							REVIE	
JOB NO.: 22-0000							BY	
200 200				•				
CLIENT: STEL	FILENAME: SRTS-P-COV.DWG	REVISIONS	.	CONINGAMINGS	EVIEW D			
SAVE DATE: SEP 21, 2023	Р <i>LOT DATE</i> : Sep 21, 2023				A %UO		DESCRIPTION	
SAVE							DATE	
ENGINEER: NINV	REVIEWED: ERH						NO.	
SCALE: SHOWN								
							2"	
			1"				2"	

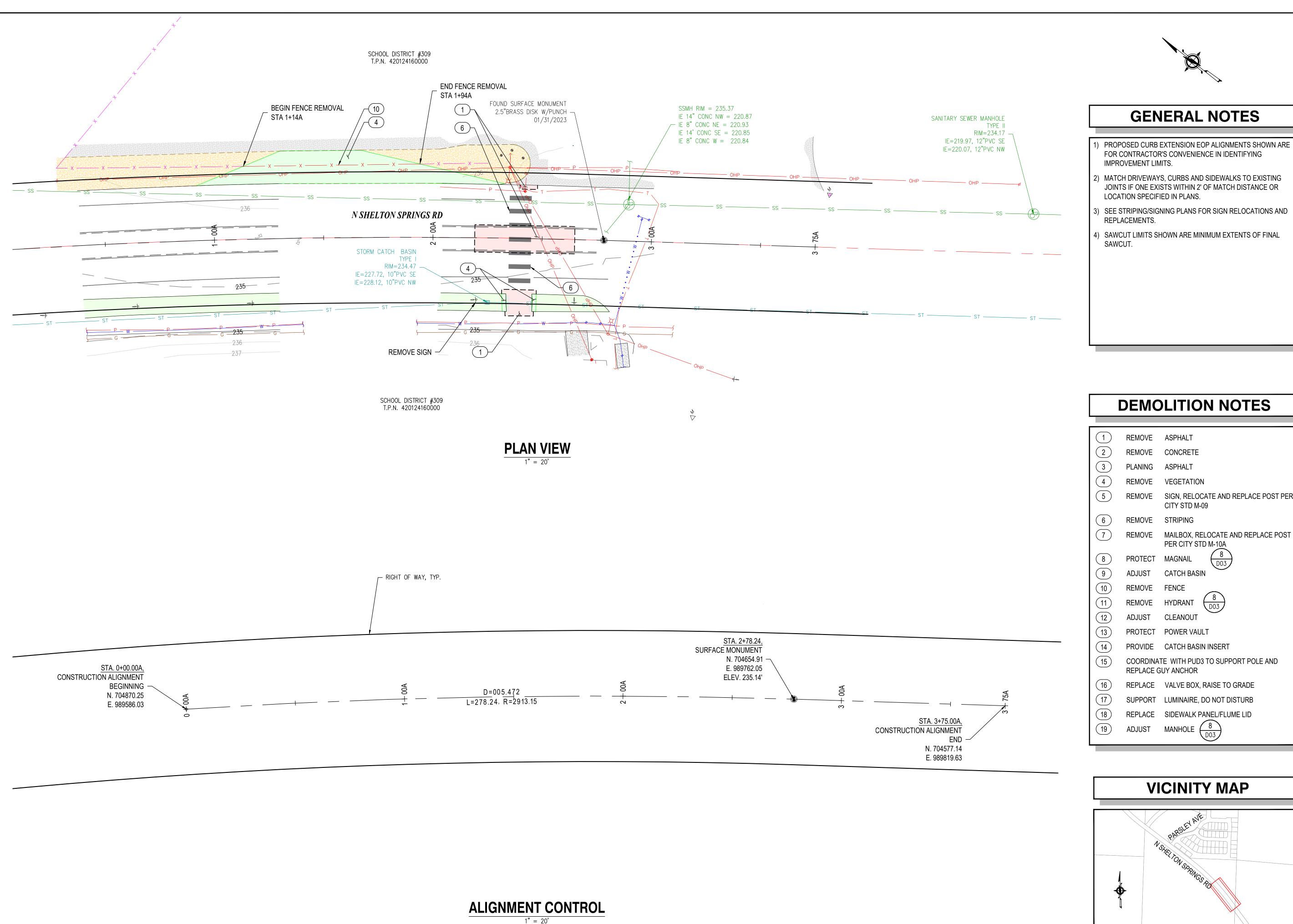
BAR MEASURES 2"
WG NO.: SHEET NO
COLUMN 102

DRAWING IS FULL SCALE WHEN

)2 22















OF SHELTON SWALK IMPROVEMENTS

CONTROL 1

PRE

DEMOLITION NOTES

PLANING ASPHALT REMOVE VEGETATION REMOVE SIGN, RELOCATE AND REPLACE POST PER CITY STD M-09 REMOVE STRIPING REMOVE MAILBOX, RELOCATE AND REPLACE POST PER CITY STD M-10A

ADJUST CATCH BASIN

REMOVE FENCE ADJUST CLEANOUT

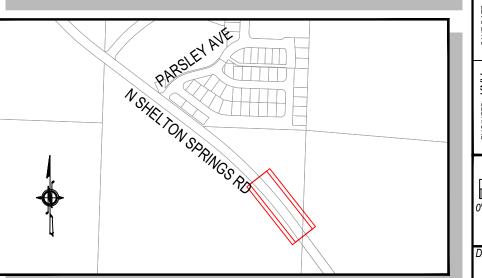
PROTECT POWER VAULT PROVIDE CATCH BASIN INSERT

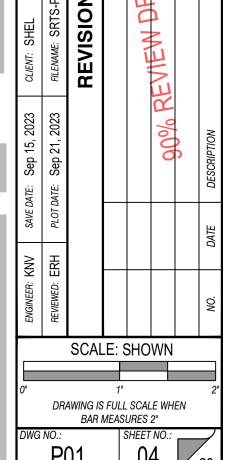
COORDINATE WITH PUD3 TO SUPPORT POLE AND REPLACE GUY ANCHOR REPLACE VALVE BOX, RAISE TO GRADE

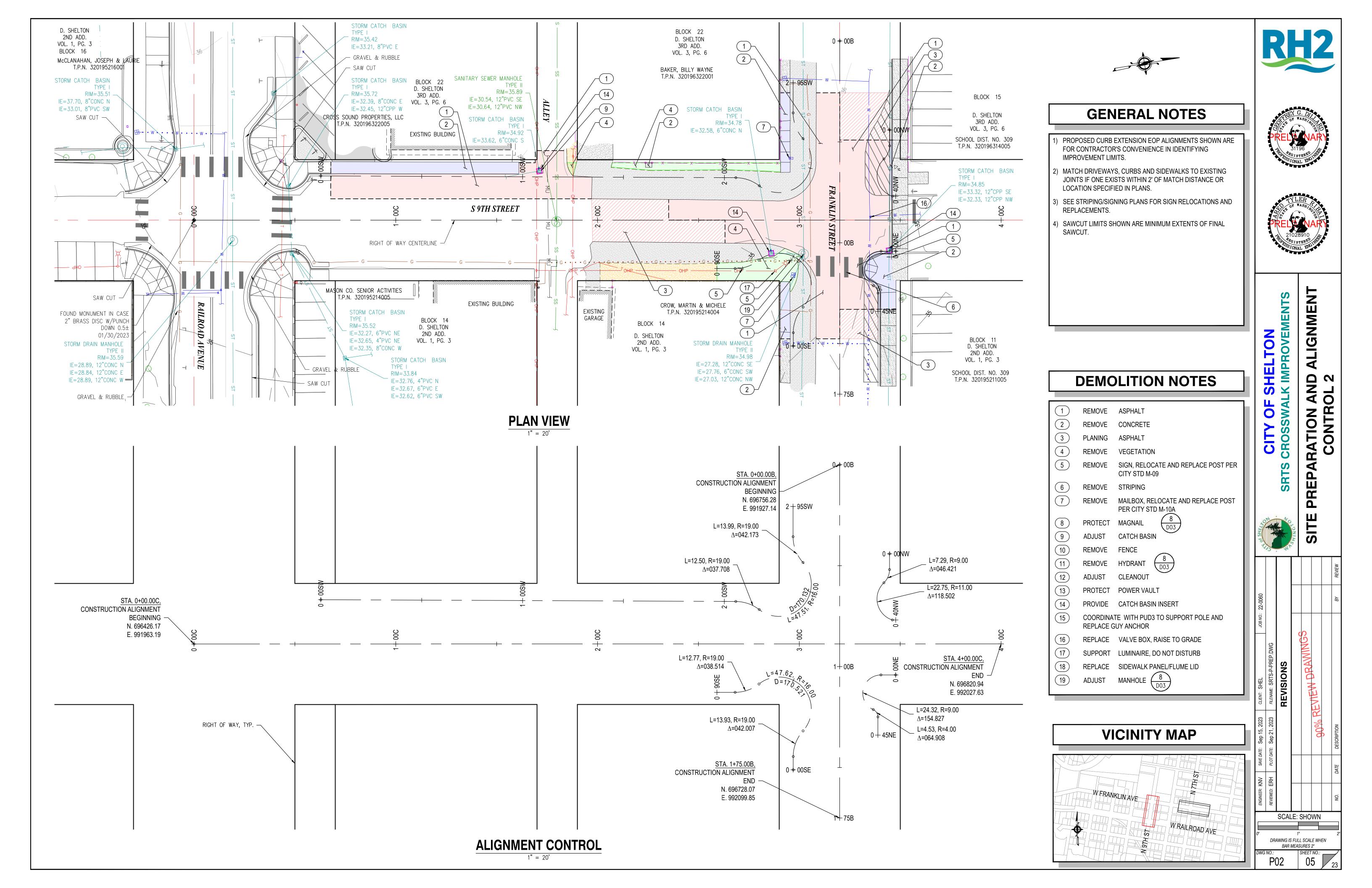
SUPPORT LUMINAIRE, DO NOT DISTURB

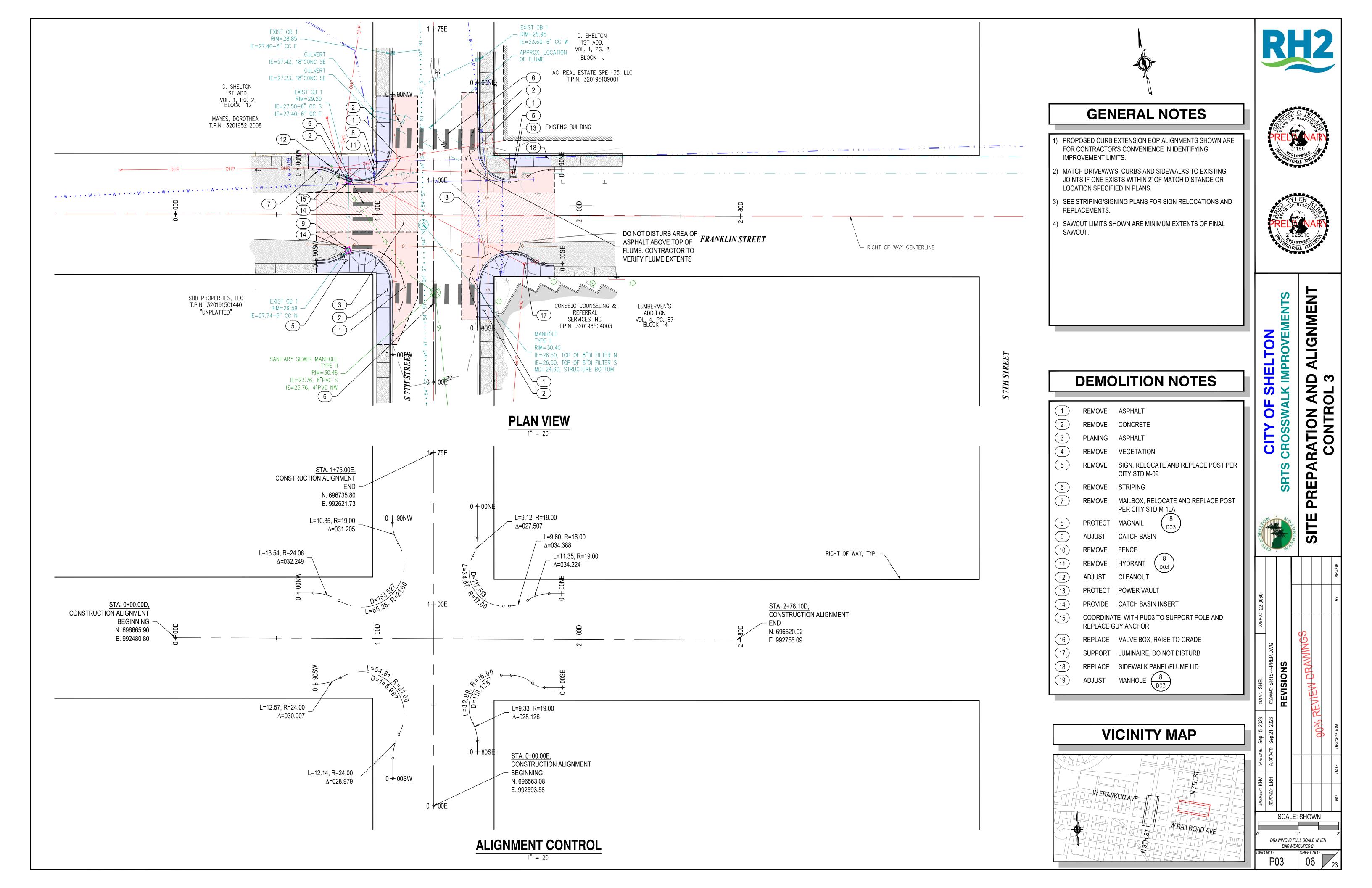
REPLACE SIDEWALK PANEL/FLUME LID ADJUST MANHOLE $\binom{8}{D03}$

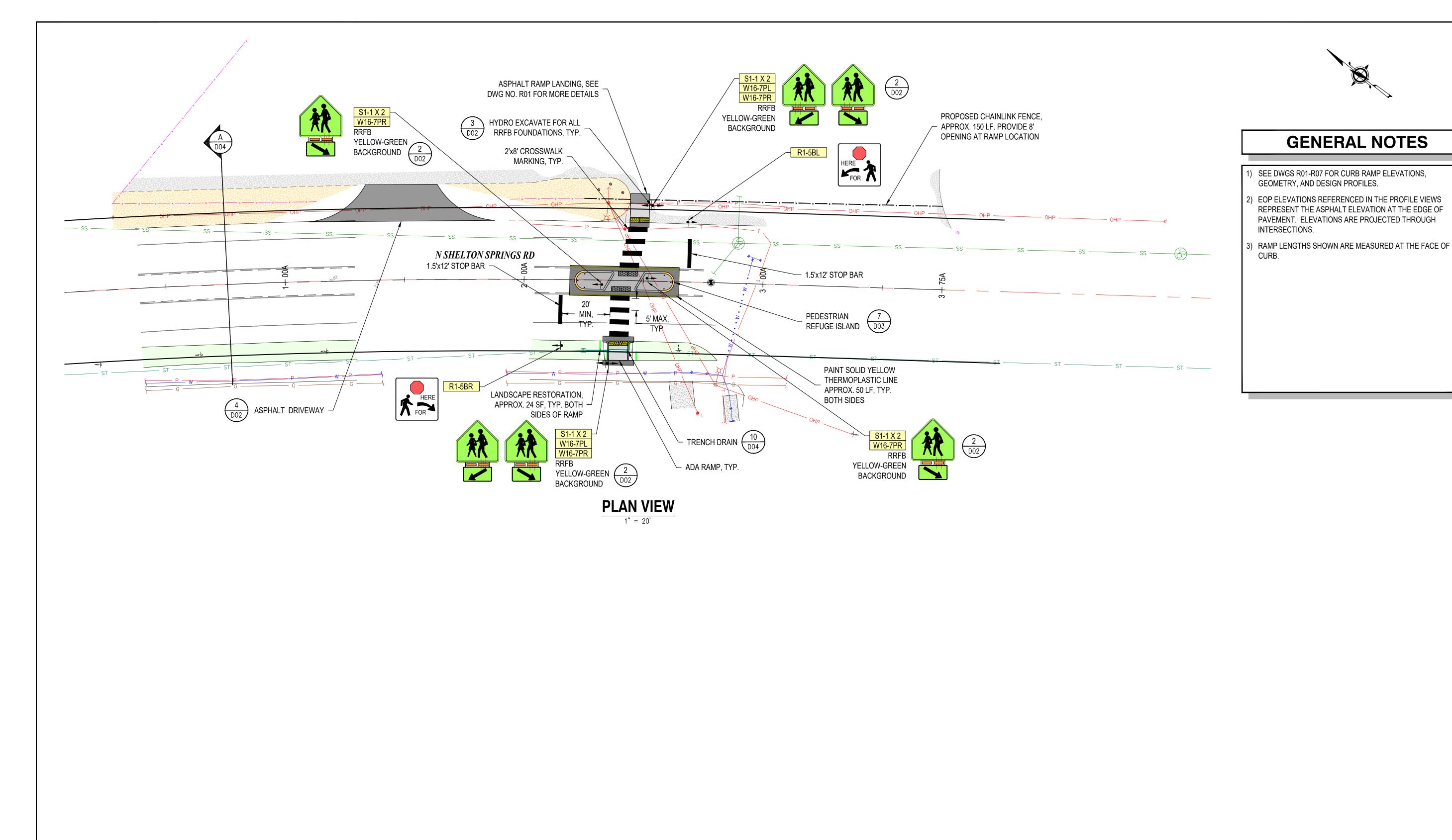
VICINITY MAP





















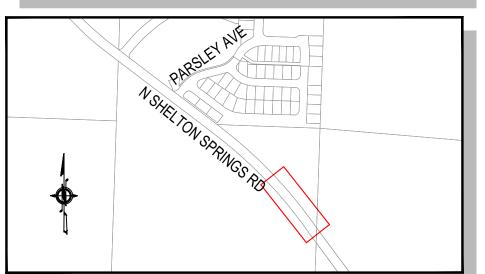
SPRINGS RD PEDESTRIAN IMPROVEMENTS CITY OF SHELTON
CROSSWALK IMPROVEMENTS SRTS NOL

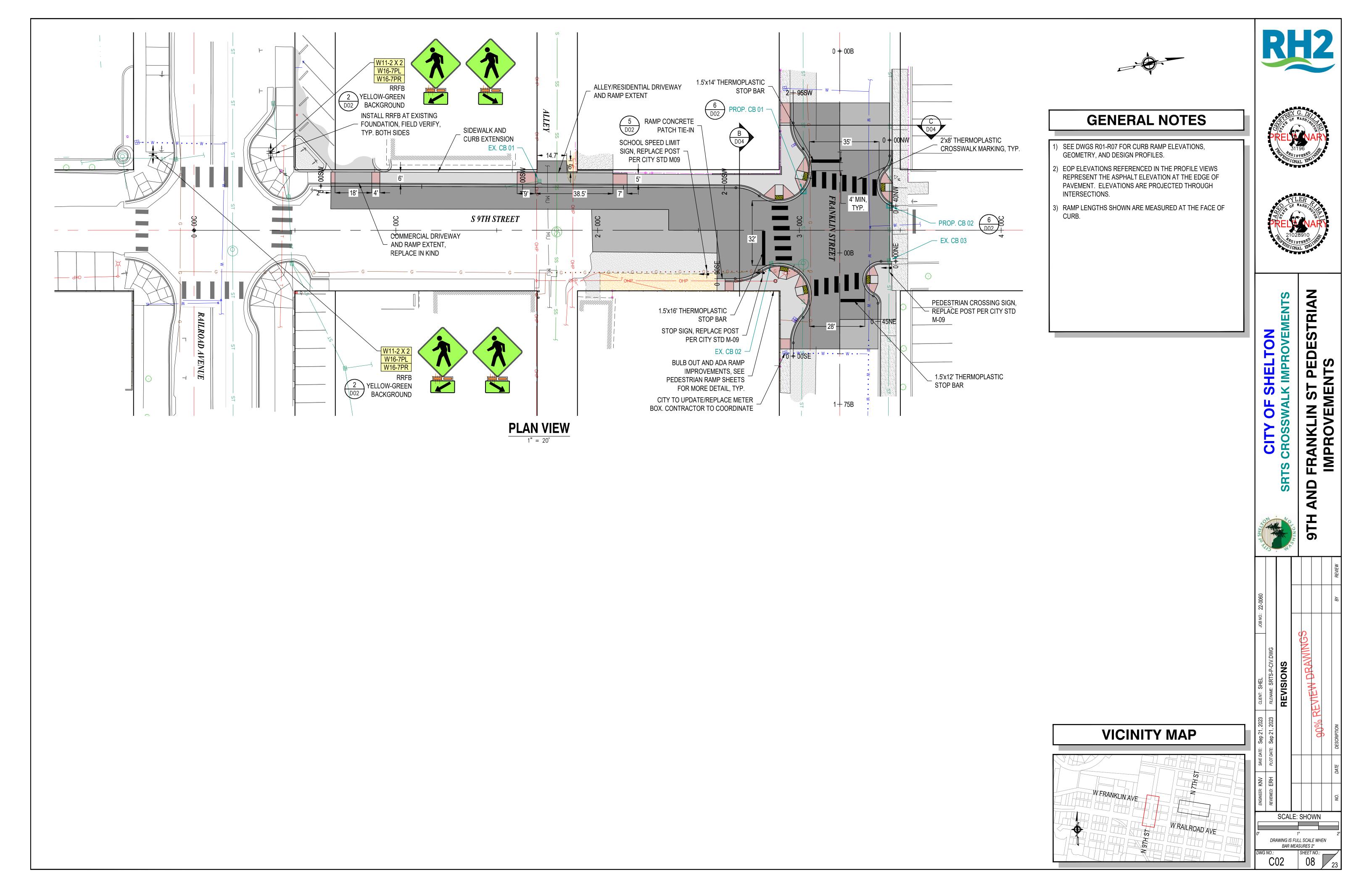


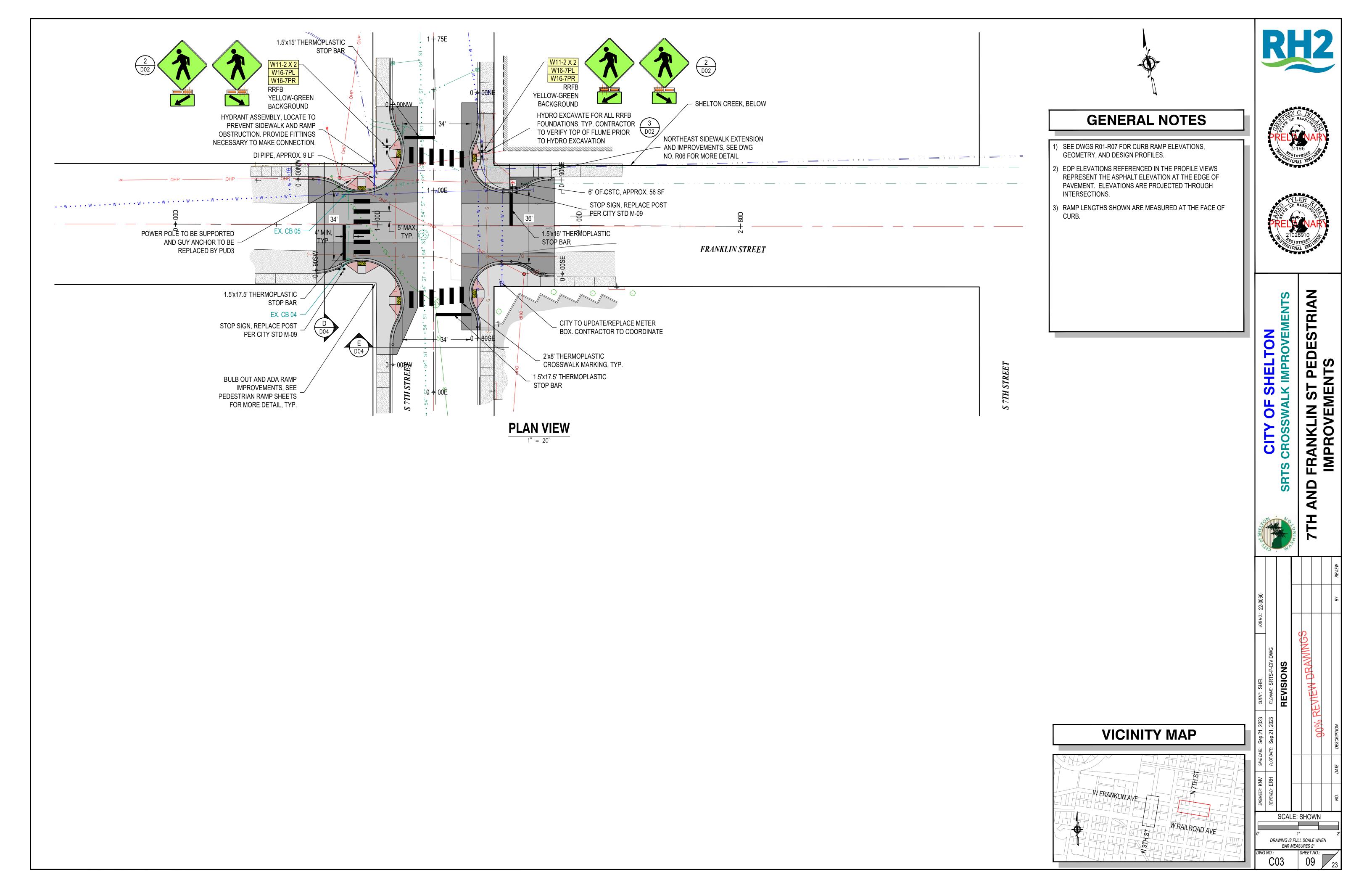
SCALE: SHOWN

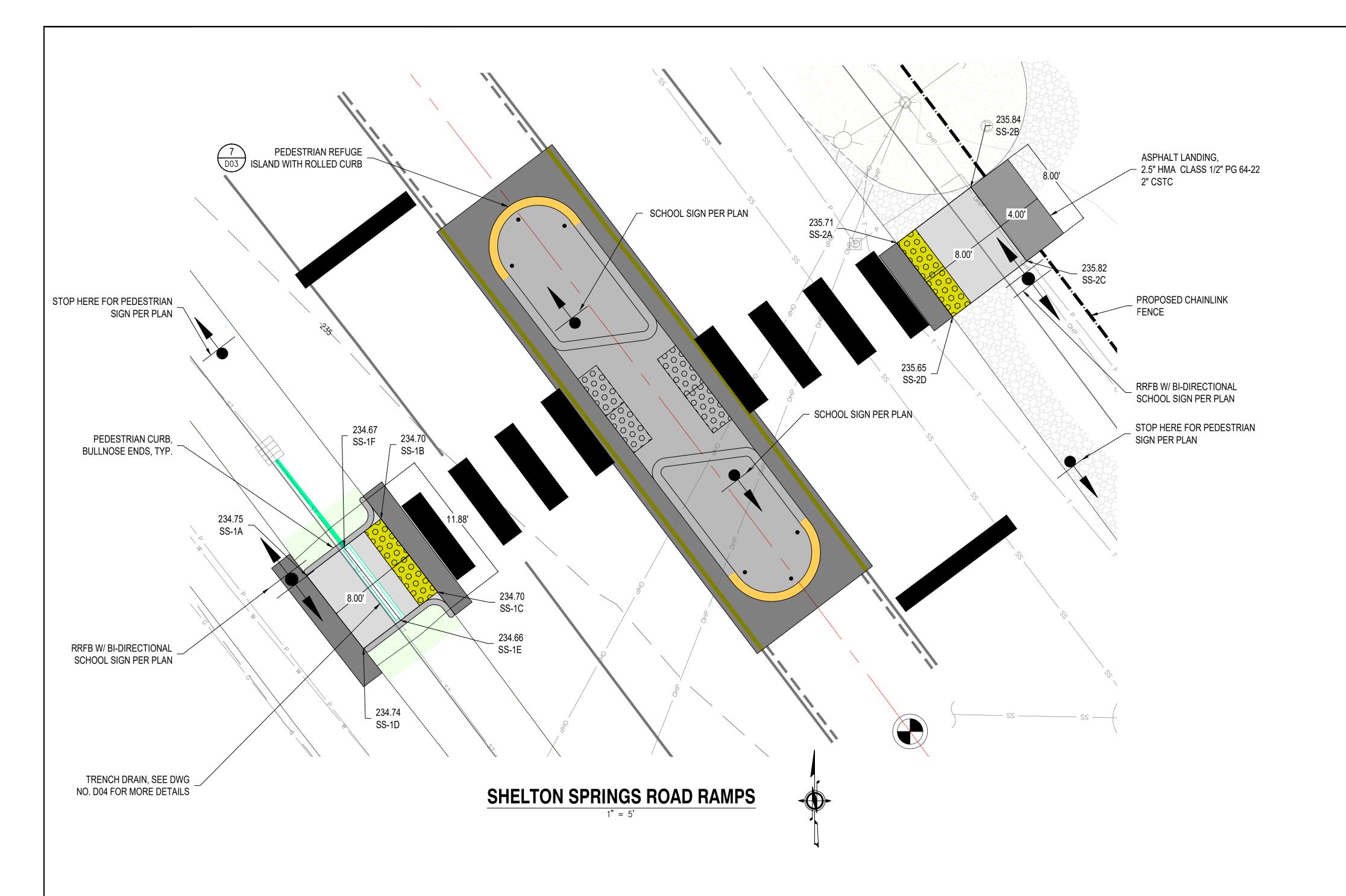
DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

VICINITY MAP







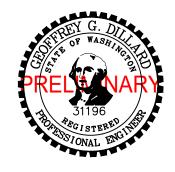


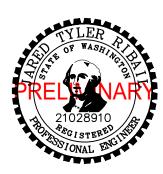
GENERAL NOTES

- 1) EOP ELEVATIONS REFERENCED IN THE PROFILE VIEWS REPRESENT THE ASPHALT ELEVATION AT THE EDGE OF PAVEMENT. ELEVATIONS ARE PROJECTED THROUGH INTERSECTIONS.
- RAMP LENGTHS SHOWN ARE MEASURED AT THE FACE OF CURB.
- 3) POINT ELEVATIONS ARE MEASURED AT THE CURB FLOW LINE.

ADA RAMP ELEVATION TABLE							
POINT	ELEV.	NORTHING	EASTING				
SS-1A	234.75	704668.38	989709.91				
SS-1B	234.70	704673.29	989716.35				
SS-1C	234.70	704666.97	989721.24				
SS-1D	234.74	704662.07	989714.83				
SS-1E	234.66	704664.51	989718.03				
SS-1F	234.67	704670.87	989713.17				
SS-2A	235.71	704697.13	989760.92				
SS-2B	235.84	704701.95	989767.30				
SS-2C	235.82	704695.57	989772.13				
SS-2D	235.65	704690.74	989765.74				







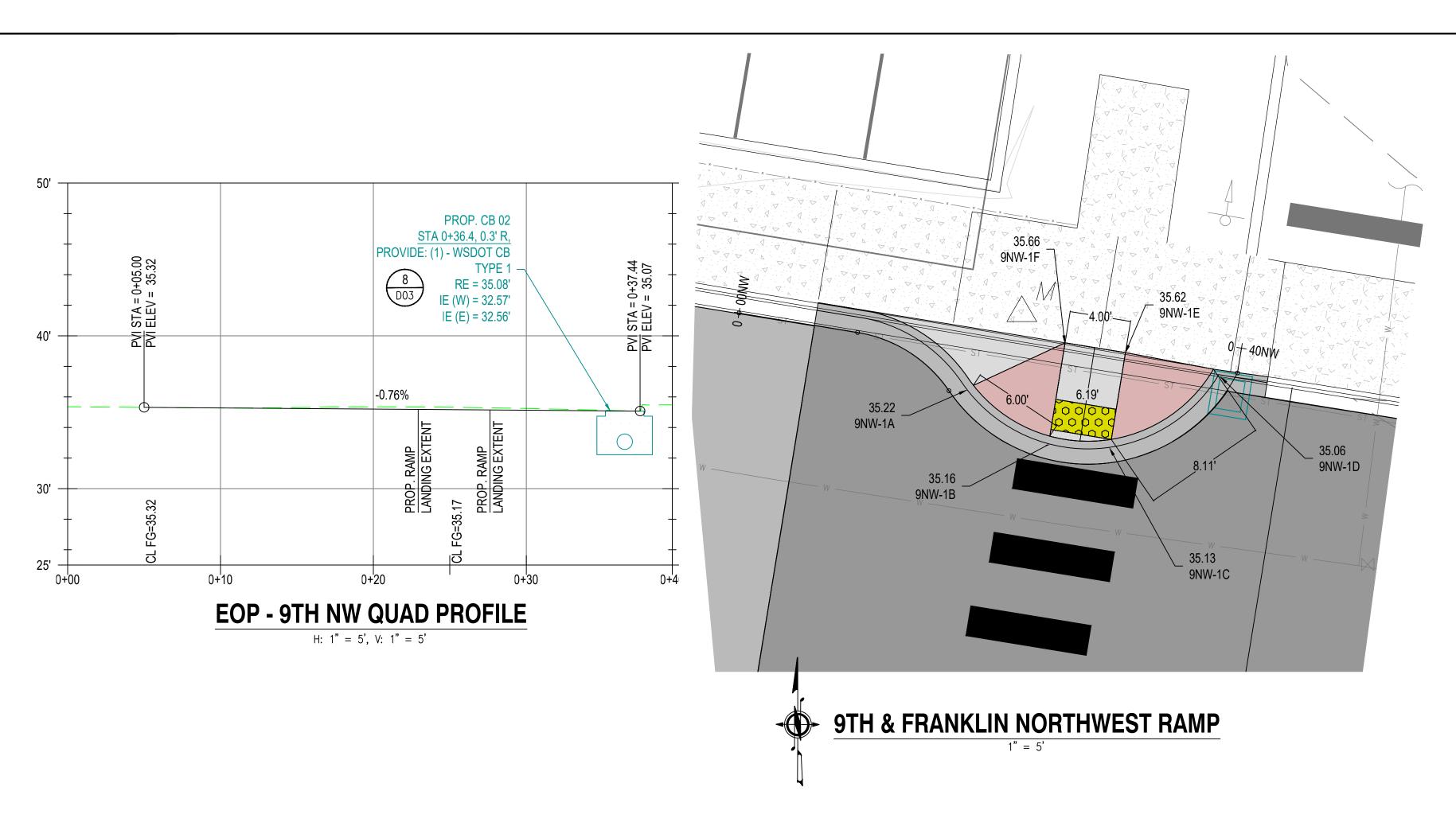
CITY OF SHELTON
CROSSWALK IMPROVEMENTS

PEDESTRIAN RAMPS



	E: Sep 21, 2023 <i>CLIENT</i> : SHEL <i>JOB NO:</i> 22-0060	E: Sep 21, 2023 FILENAME: SRTS-D-CURB.DWG	REVISIONS		HEAVINGS	and REVIEW P. S.	DESCRIPTION BY REVIEW
	SAVE DATE: Sep 21, 2023	PLOT DATE: Sep 21, 2023 FILEN	RE		,	ON REVI	TE DESCRIPTION
	R: KNV						DATE
8	ENGINEE	REVIEWEL	SCAL	E. G	SHO	\\/\NI	NO.
	ENGINEER: KNV	REVIEWED: ERH	SCAL	-E: §	SHO	WN	

DRAWING IS FULL SCALE WHEN BAR MEASURES 2"



ADA RAMP ELEVATION TABLE							
POINT	ELEV.	NORTHING	EASTING				
9NW-1A	35.22	696768.53	991989.53				
9NW-1B	35.16	696764.97	991994.89				
9NW-1C	35.13	696764.74	991998.89				
9NW-1D	35.06	696769.39	992006.15				
9NW-1E	35.62	696770.99	991999.92				
9NW-1F	35.66	696771.61	991995.96				

GENERAL NOTES

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CITY OF SHELTON
CROSSWALK IMPROVEMENTS

PEDESTRIAN RAMPS



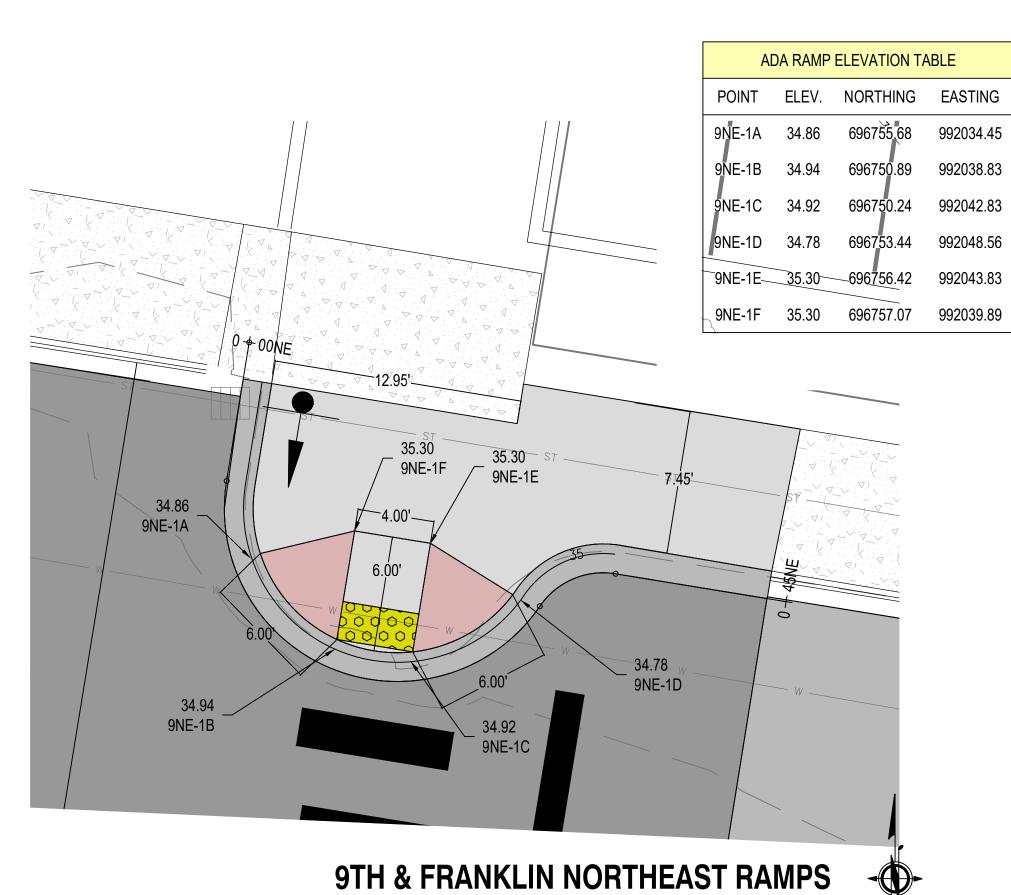
						BEVIE	
JOB NO.: 22-0060						BY	
ON HOP				0			
CLIENI: SHEL	FILENAME: SRTS-D-CURB.DWG	REVISIONS) ()	CONING DRAWINGS	EVIEW D		
SAVE DATE: Sep Z1, ZUZ3	Р <i>LOT DATE</i> : Sep 21, 2023				A %UO	 DESCRIPTION	
						DATE	
ilNEEH: KNV	IEWED: ERH					10.	

SCALE: SHOWN

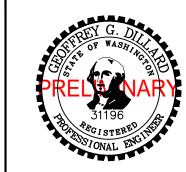
DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

	40'	BVCS: 0+03.98 PVI ELEV:34.80 K:1.04 LVC:4.00 BNCE: 34.80 ENCS: 0+02.98 ENCS: 0402.98		PVI	TA:0+21.26 ELEV:34.98 K:1.79 VC:5.00		BVCS: 0+34.69 R:7.72 LVC:5.00 R:7.72 LVC:5.00 R:7.72 CE: 34.69 R:7.72 LVC:5.00	9
PVI STA = 0+01.92 PVI ELEV = 34.86		-2.85%	1.00%		ш	-1.80%		-1.15%
	30'	CL FG=34.86	EX. CB 03 STA 0+03 RE = 34.8 IE (W) = 3 IE (E) = 3	3.3, 0.5' R, 37'	PROP. RAMP LANDING EXTENT \$\overline{1}{2}\$L FG=34.91			
	25' 0+(0+10	0+2	20	0+30	0.	+40
			EC	<u> </u>	NE QUAD	PROFILE		

H: 1" = 5', V: 1" = 5'









OF SHELTON SWALK IMPROVEMENTS CITY

PEDESTRIAN RAMPS



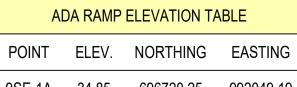


SCALE: SHOWN

DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

GENERAL NOTES

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- 2) RAMP LENGTHS SHOWN ARE MEASURED AT THE FACE OF
- 3) POINT ELEVATIONS ARE MEASURED AT THE CURB FLOW LINE.



9SE-1A 34.85 696720.25 992049.19 9SE-1B 34.78 696722.12 992042.94 9SE-1C 34.74 696721.85 992038.94 9SE-1D 34.69 696719.34 992033.10

9SE-1E 35.25 696715.48 992039.34 9SE-1F 35.30 696715.72 992043.34

9SE-2A 34.67 696719.06 992032.70 9SE-2B 34.60 696714.04 992028.49

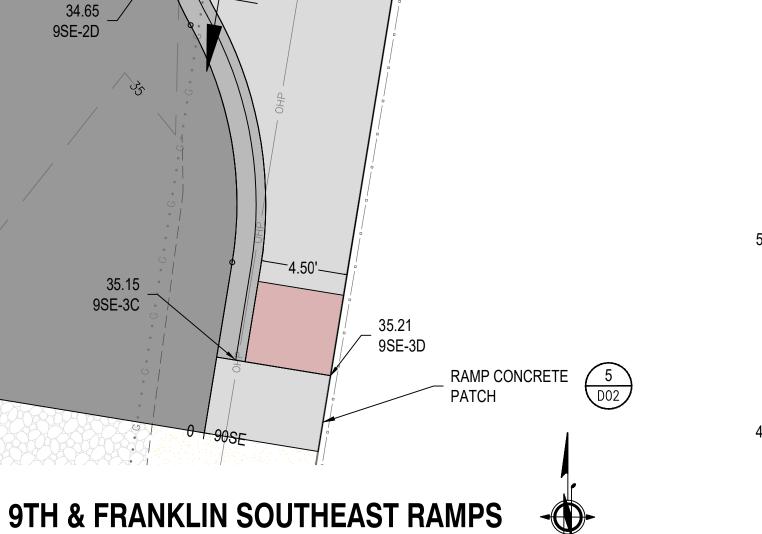
9SE-2C 34.55 696710.25 992027.14

9SE-2D 34.65 696703.86 992027.18

9SE-2E 35.05 696708.12 992033.18 9SE-2F 35.10 696711.88 992034.54

9SE-3A 35.08 696708.83 992068.91 9SE-3B 35.15 696703.48 992068.03

9SE-3C 35.15 696681.91 992030.20 9SE-3D 35.21 696681.05 992035.18



34.74 9SE-1C

34.69 9SE-1D

34.67

9SE-2A

34.60 9SE-2B

34.55 9SE-2C

34.78 9SE-1B

35.30

PROVIDE EXPANSION

JOINT, MIN. 4" OFFSET

35.25

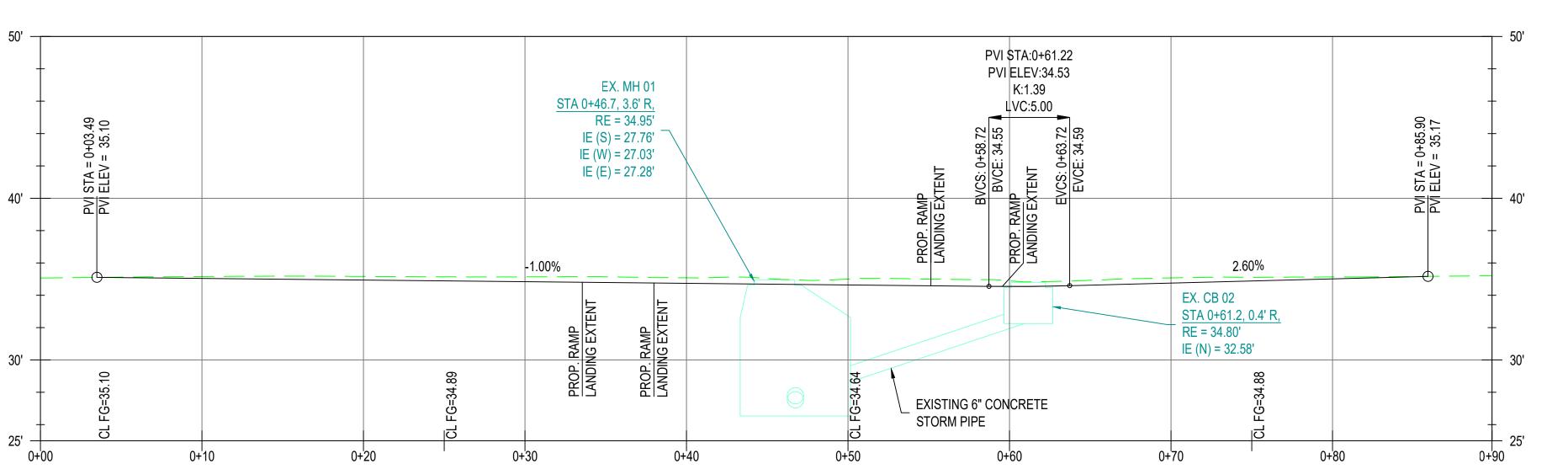
35.10 9SE-2F

35.05

9SE-3A

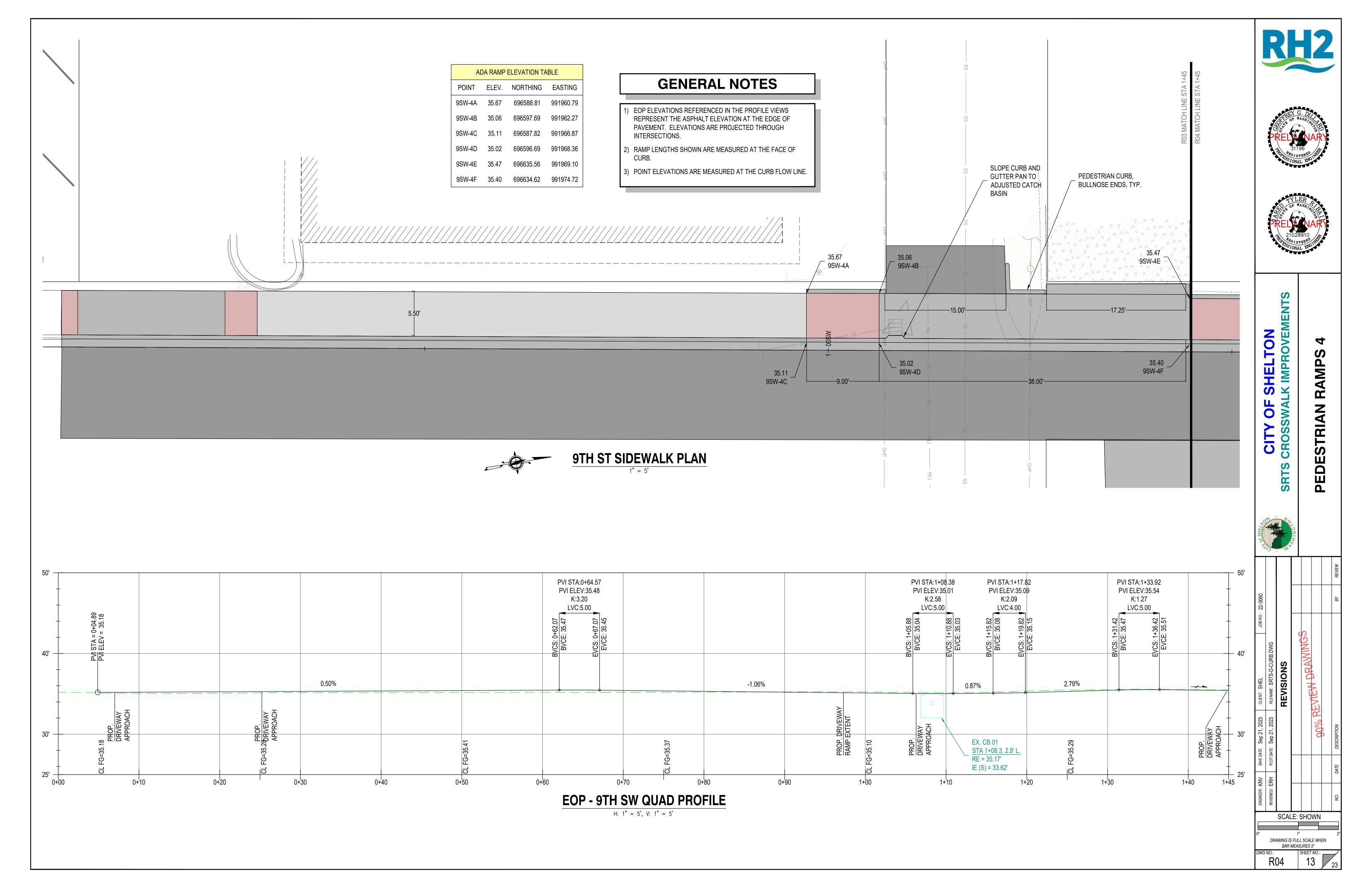
5.00'

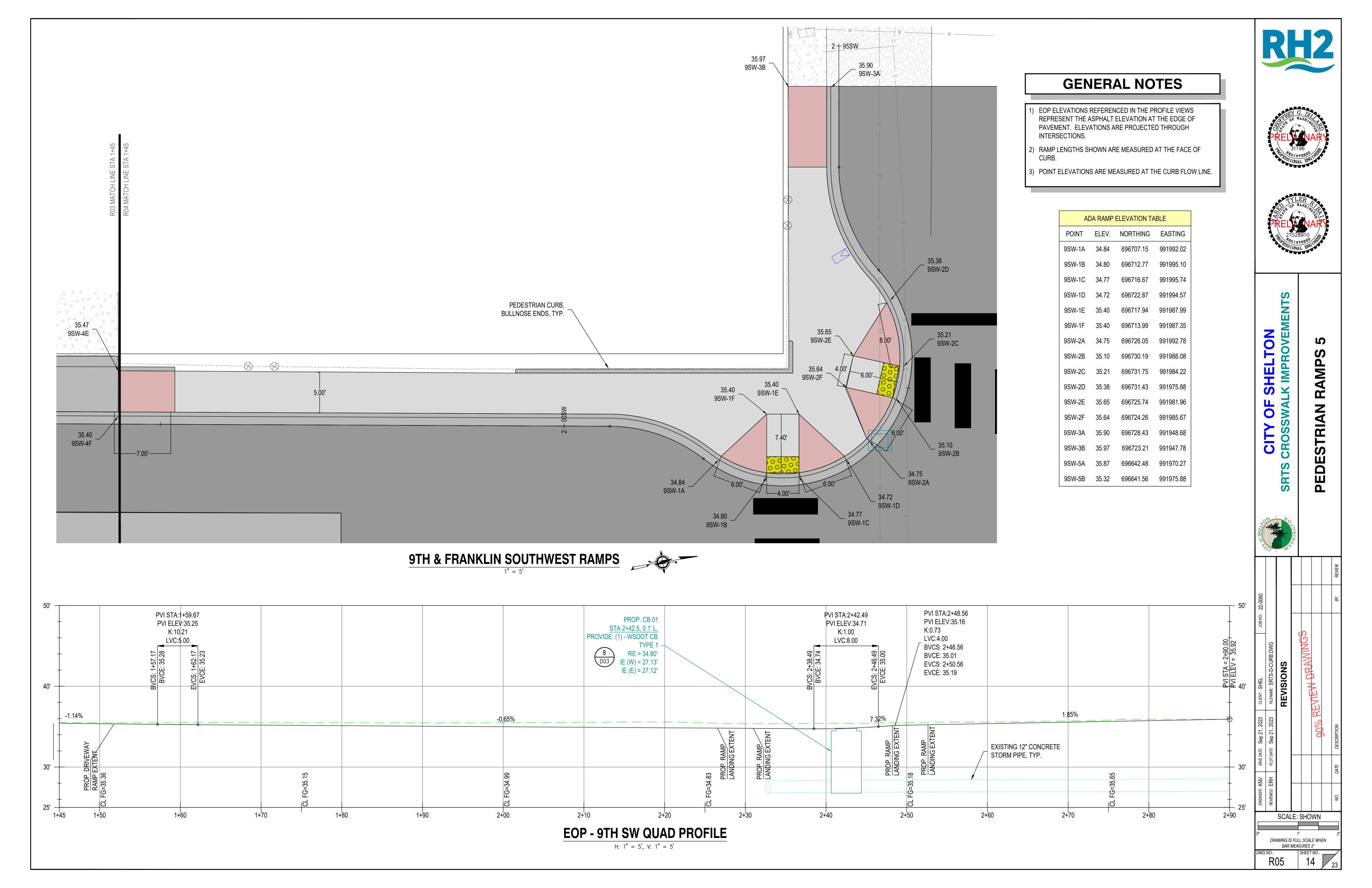
35.15 9SE-3B

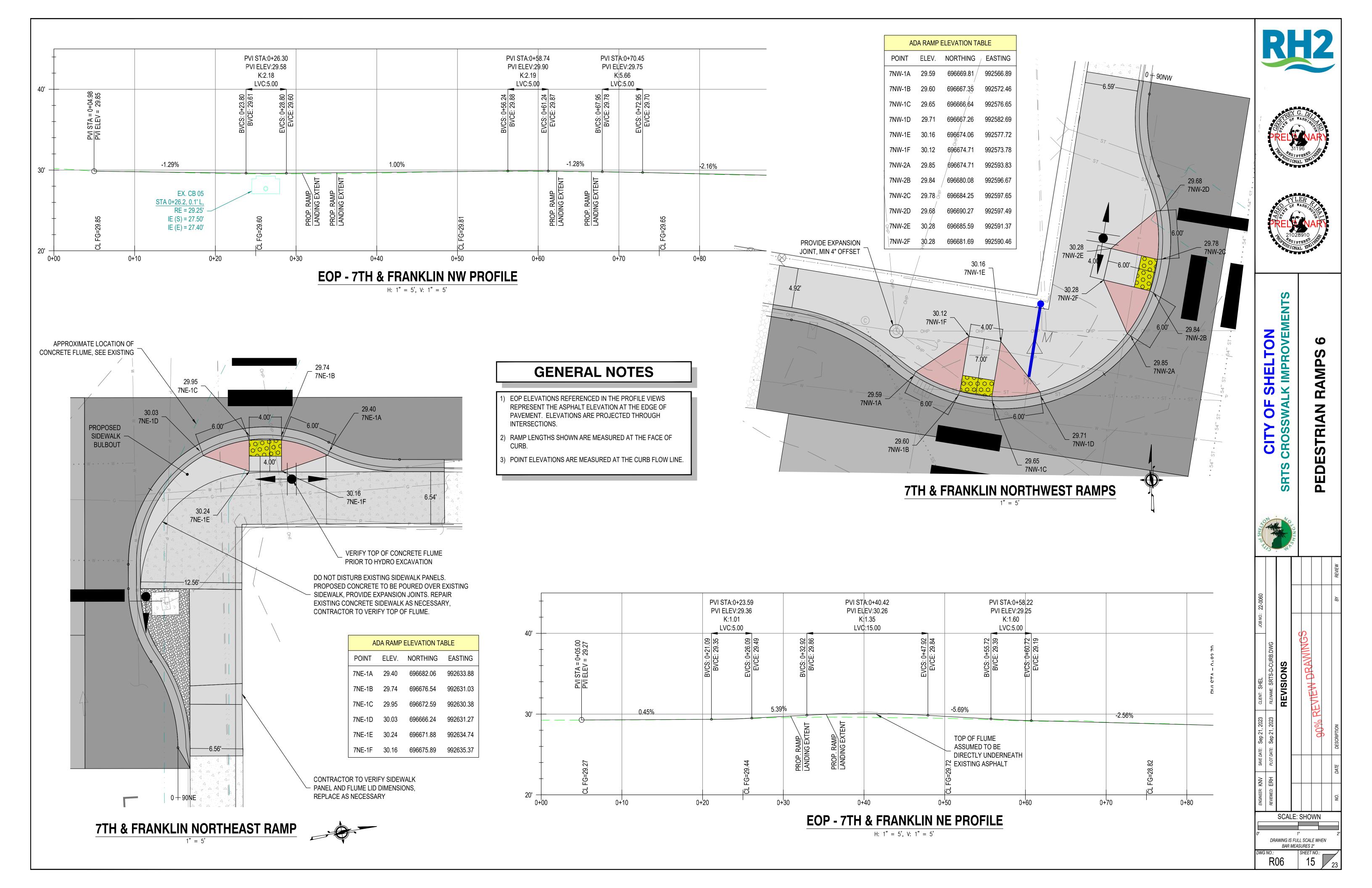


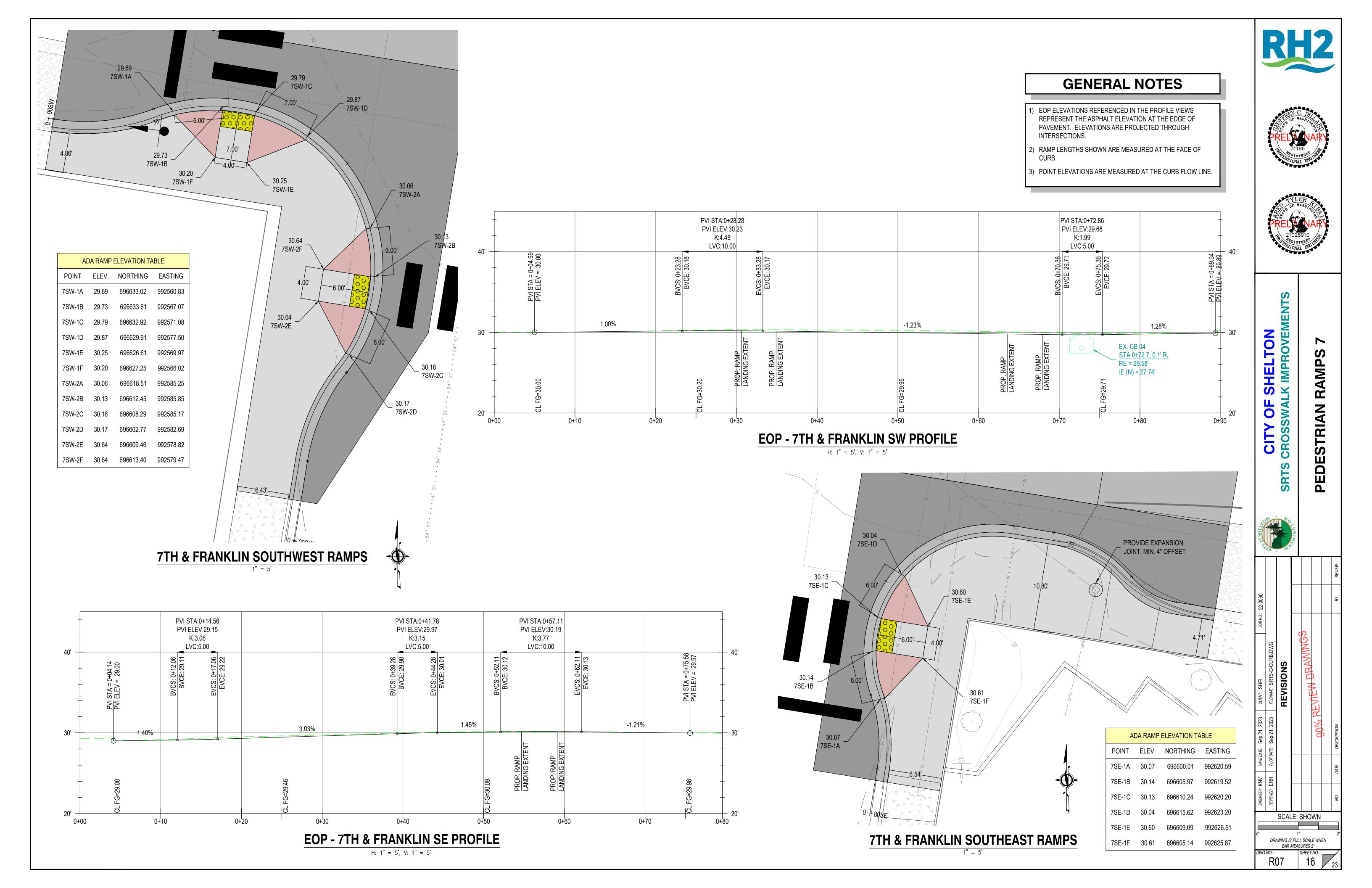
EOP - 9TH SE QUAD PROFILE

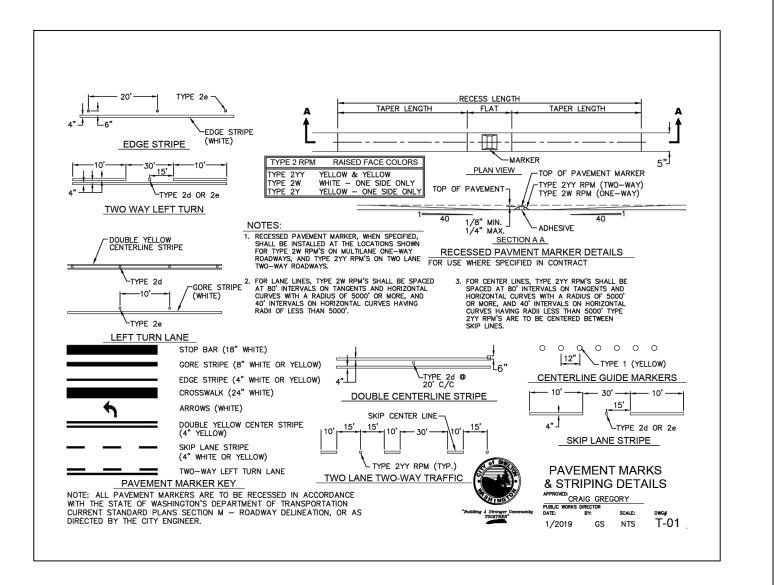
H: 1" = 5', V: 1" = 5'

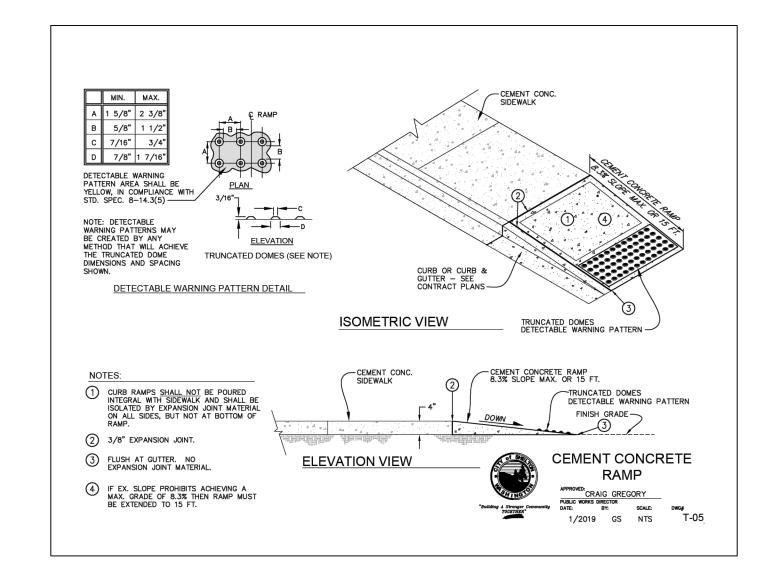


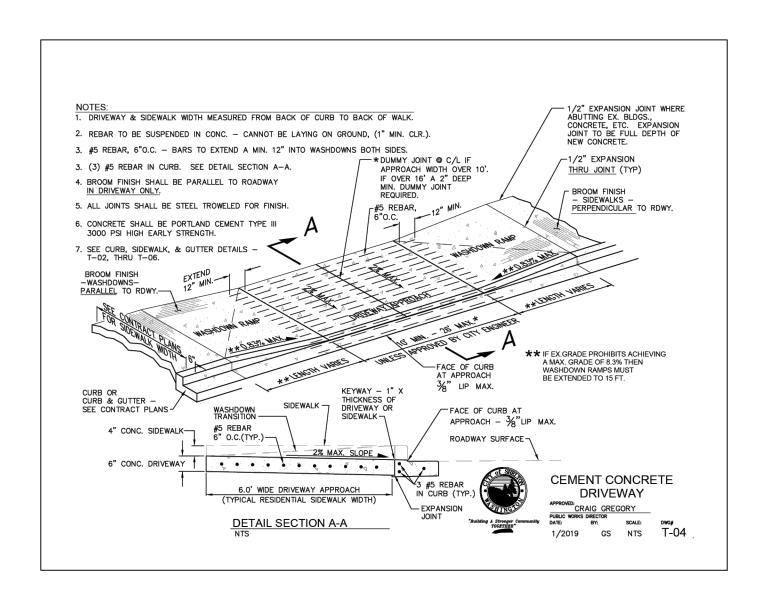


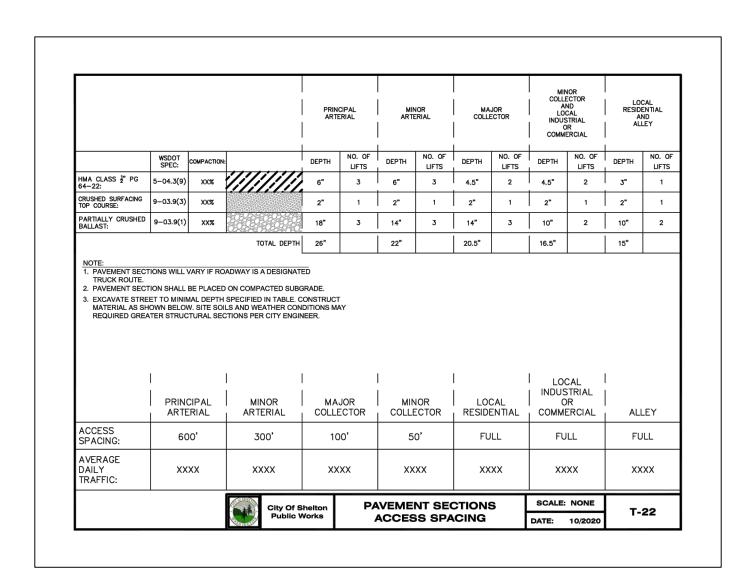


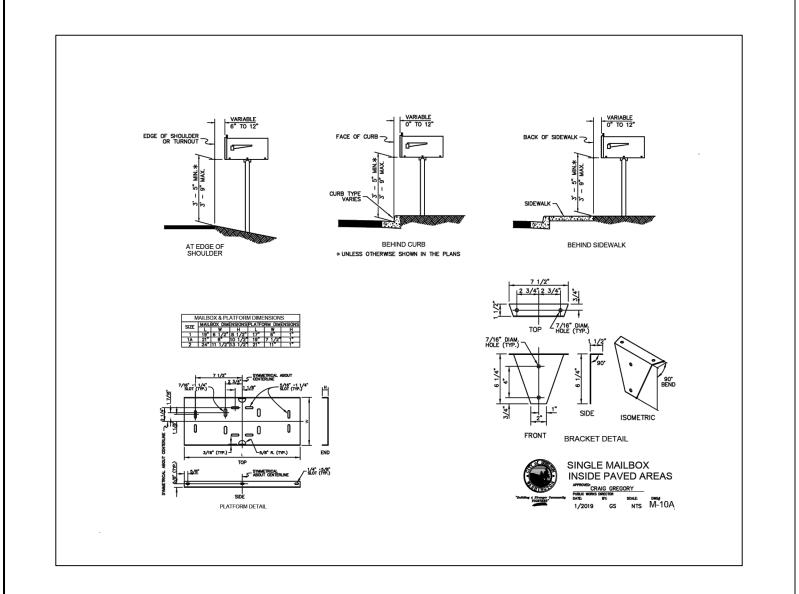


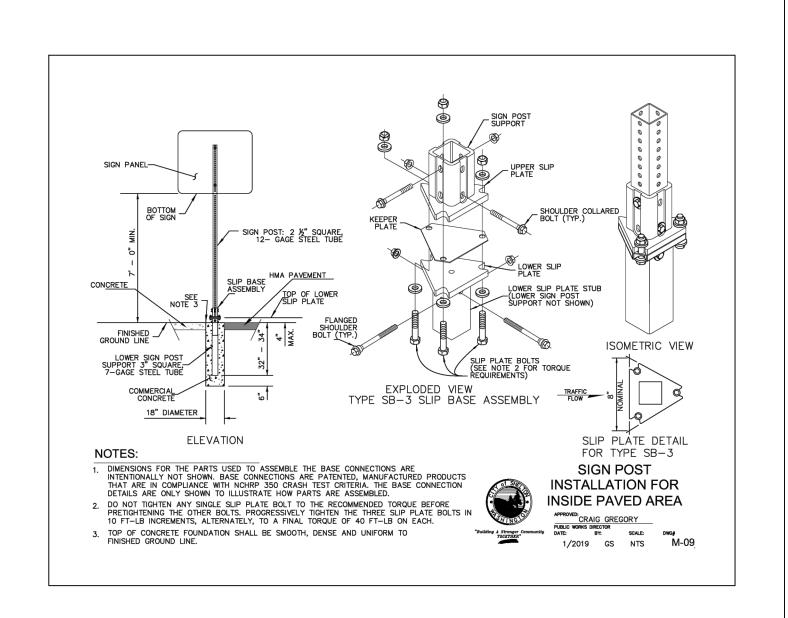


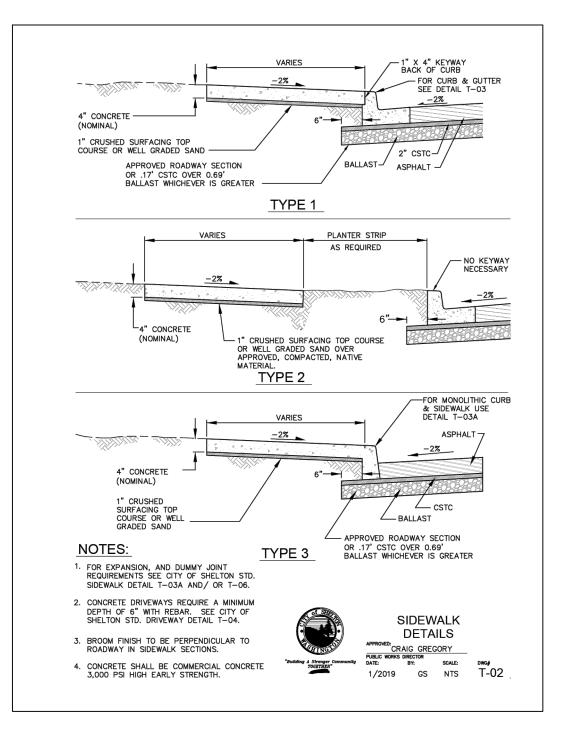


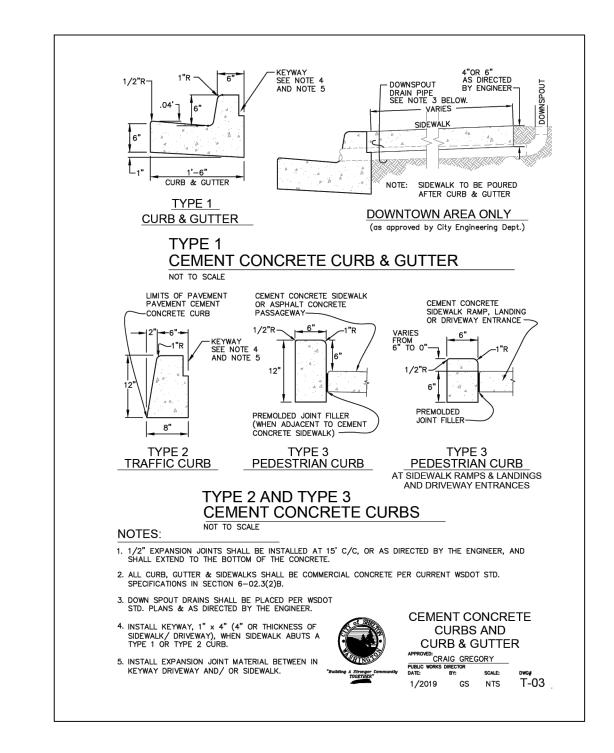






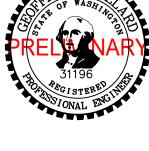
















IMPROVEMENT

SHELTON WALK OF CITY CRO

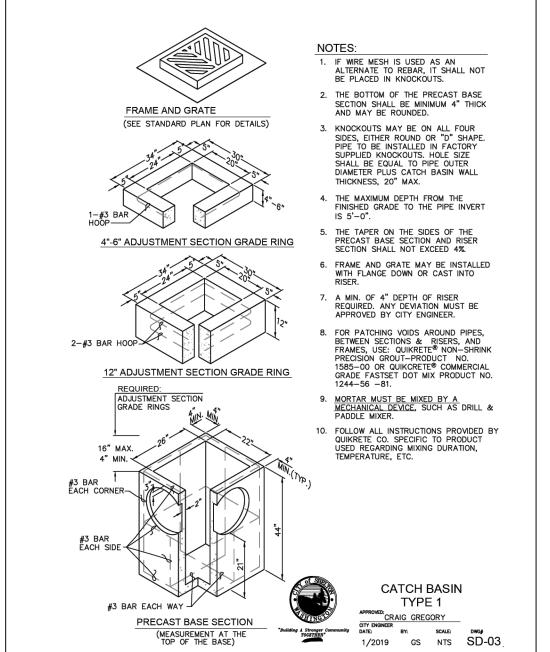
AIL

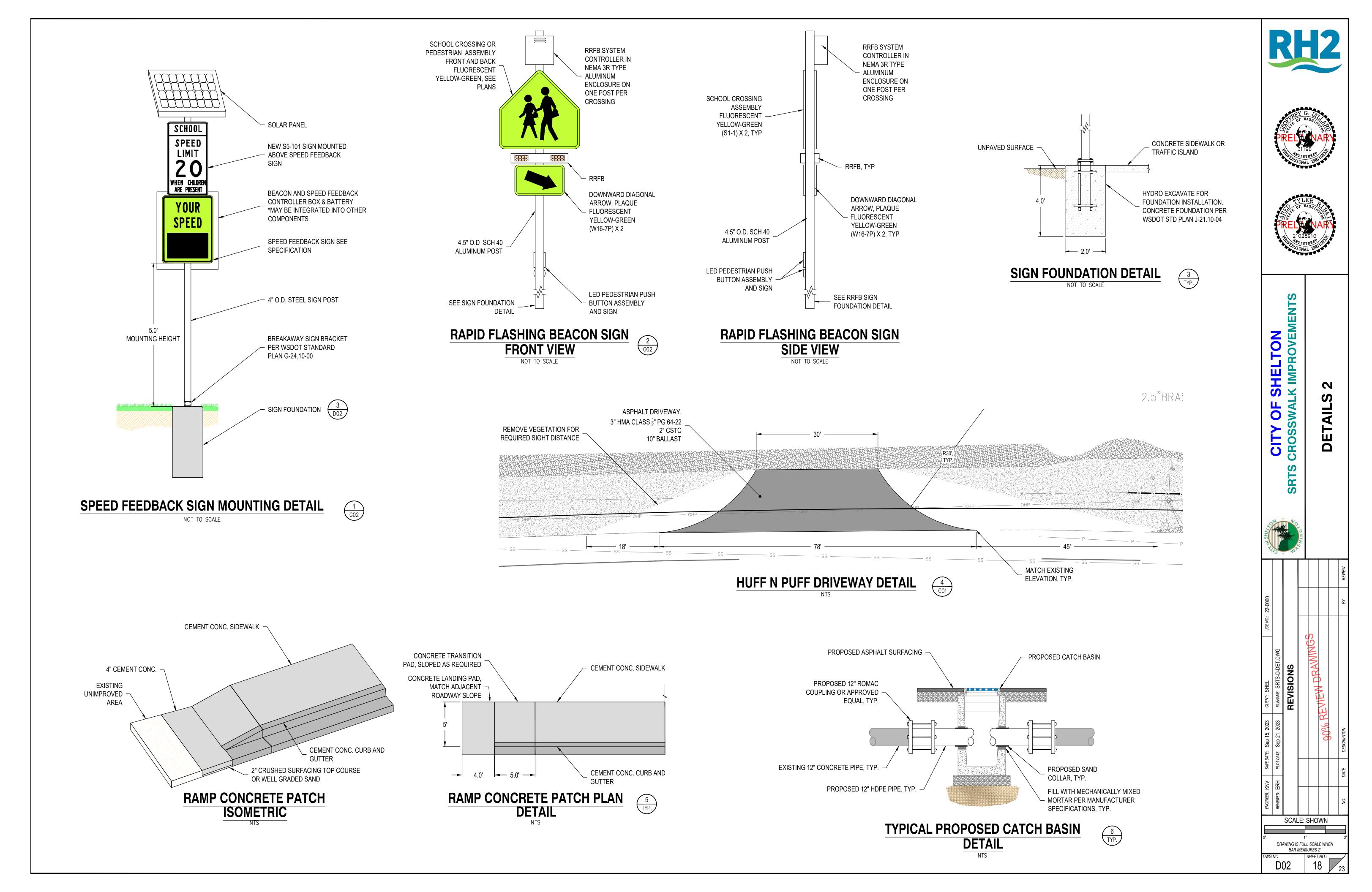


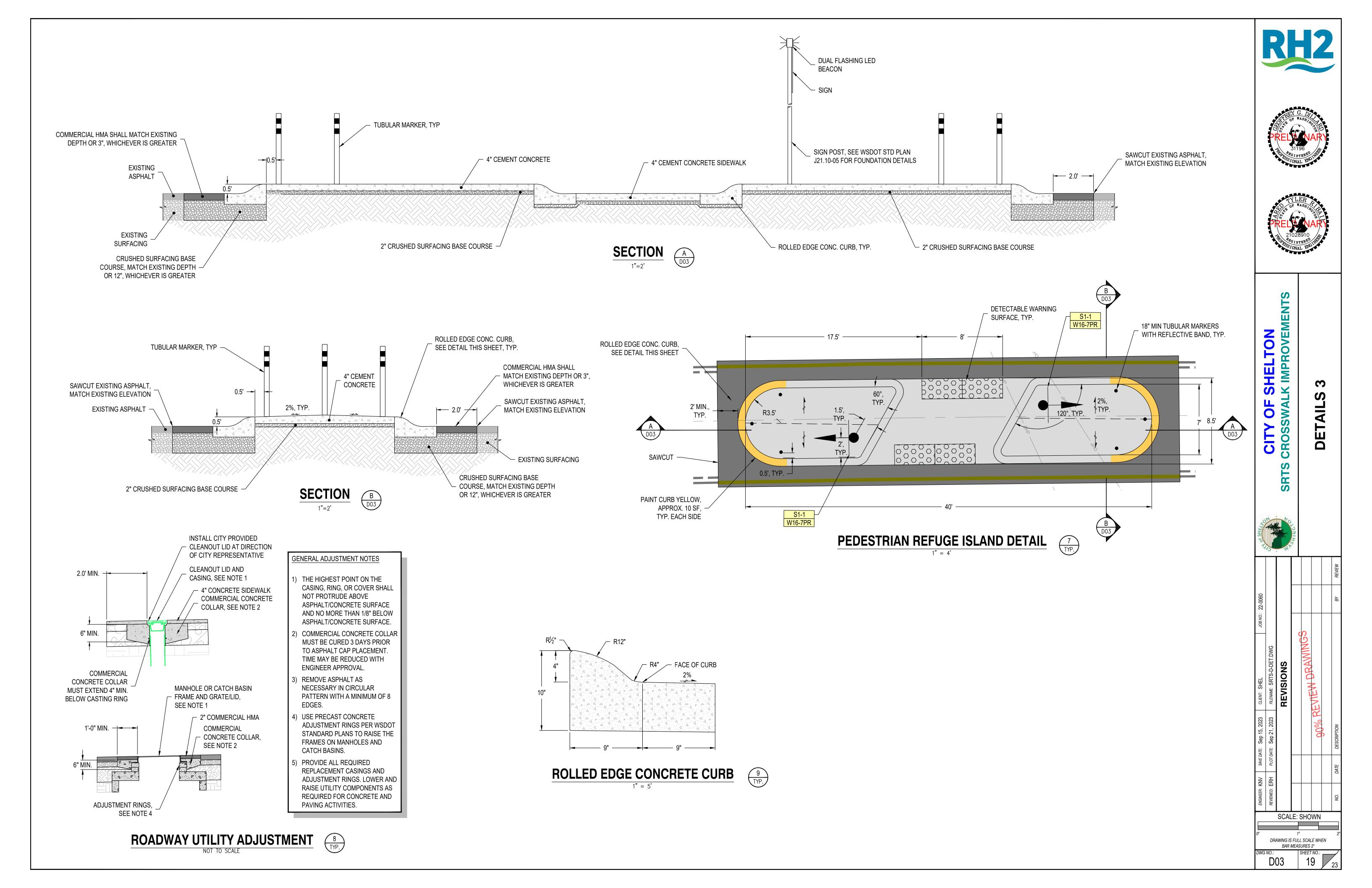
S

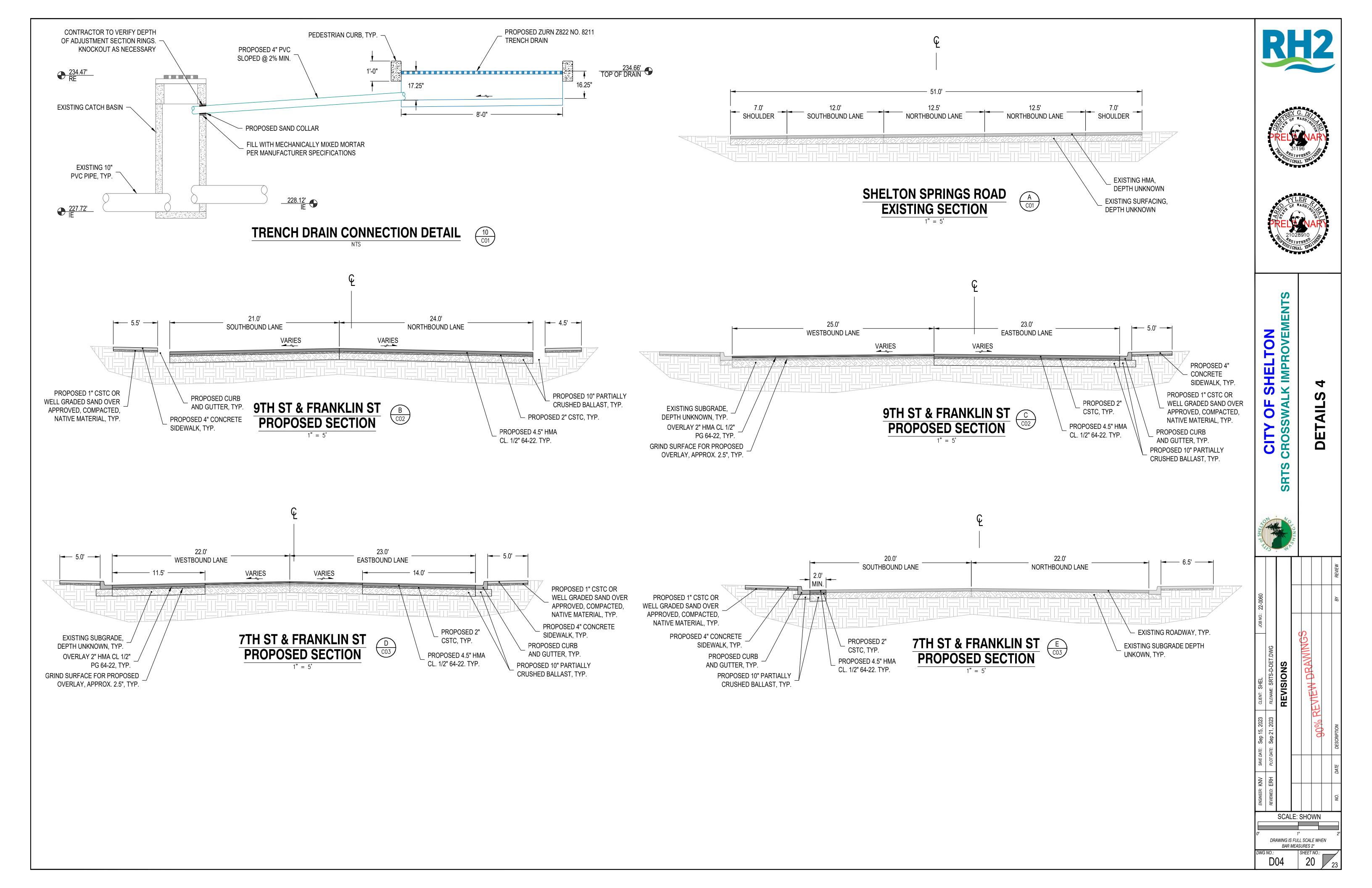


DRAWING IS FULL SCALE WHEN BAR MEASURES 2"









				BUFF	ER [ATAC	\			
			L	ONGITUDIN	NAL BUFFER	R SPACE = E	3			
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (FEET)	155	200	250	305	360	425	495	570	645	-
			BUFFE	R VEHICLE	ROLL AHE	AD DISTANO	CE = R			
TRANSPORT	ABLE ATTEN	UATOR:								
	OST VEHICLE CE WITH THE					L BE IN	30 F	EET MIN. TO) 100 FEET	MAX.
PROTECTIVE	VEHICLE:						NO S	PECIFIED DIS	STANCE REQU	JIRED

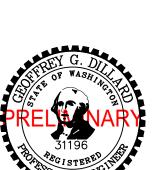
MAY BE A WORK VEHICLE STRATEGICALLY LOCATED TO SHIELD THE WORK AREA.

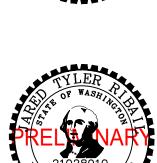
RURAL HIGHWAYS	60 / 65 MPH	800'
RURAL ROADS	45 / 55 MPH	500'
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350'
RURAL ROADS, URBAN ATERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' (2)
URBAN STREETS	25 MPH OR LESS	100' (2)

W20-7B (OPTIONAL IF 40 MPH OR LESS)

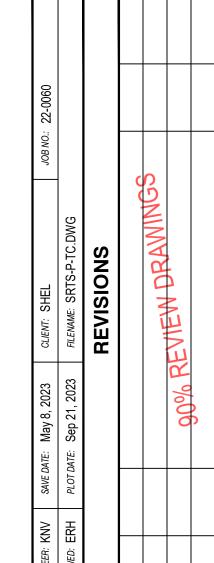
W20-7B











SCALE: SHOWN

DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

CHANNELIZATION DEVICE

SPACING (FEET)

TAPER

10 TO 20

10 TO 20

10 TO 20

50 / 65

35 / 45

25 / 30

TANGENT

60



	50' TO 100' - 6 DEVICE - R MIN WORK AREA	50' TO 100' 6 DEVICE MIN O O O O O O O O O O O O O	X X X
NONE LANE ROAD AHEAD AB* W20-4 BE PREPARED TO STOP W20-78 (OPTIONAL IF 40 MPH OR LESS)	INTERSECTING ROADWAY X X X X X X X X X X X X X	W20-7A BE PREPARED TO STOP W20-7B W20-7B	

1. ALL SIGNS ARE BLACK ON ORANGE.

NOTES:

DETAILS.

TEMPORARY SIGN LOCATION

2. EXTENDING THE CHANNELIZING DEVICE TAPER ACROSS SHOULDER IS RECOMMENDED.

3. NIGHT WORK REQUIRES ADDITIONAL ROADWAY LIGHTING AT FLAGGING

STATIONS. SEE THE STANDARD SPECIFICATIONS FOR ADDITIONAL

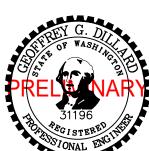
4. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.

CHANNELIZING DECIVES

LEGEND

PROTECTIVE VEHICLE















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	OF W	ASHING)
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	3119 REGIST SSIONAL	ERED L ENG	THE A	
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CLIENT: SHEL	FILENAME: SRTS-P-TC	REVISIONS		ARA MAN	ON REVIEW DIE		
Sa <i>ve Date:</i> May 8, 2023	РLОТ DATE: Sep 21, 2023				A %UO		1000
							1
ENGINEER: KNV	REVIEWED: ERH						0,1
		SCAL	.E: S	SHO	WN		
п			1"				
	DRA	awing is	FULL	SCAL	E WH	EN	

BAR MEASURES 2"

	MIN	IIMUM SHOULDER TAPER LENGTH = L/3 (FEET)	
50'	SHOULDER	POSTED SPEED (MPH)	

SHOULDER WIDTH		POSTED SPEED (MPH)								
(FEET)	25	30	35	40	45	50	55	60	65	75
8'	40	40	60	90	_	_	_	_	_	_
10'	40	60	90	90	_	_	_	_	_	_
			LICE A MINIMI	IM 3 DEVICES	TADED FOD	פשטווו חבם ובי	SC THAN 8'			

USE A MINIMUM 3	DEVICES	TAPFR	FOR	SHOULDER	LESS	THAN	8'.

			A 0 0	2' MIN.	
K	KI			WORK A	IREA 🔷
X —	X	L/3	B	R	50'

LEGEND	

SIGN SPACING = X(1)

ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

RURAL ROADS & URBAN ARTERIALS

RURAL ROADS, URBAN ATERIALS, RESIDENTIAL & BUSINESS DISTRICTS

URBAN STREETS

WORK

35 / 40 MPH

25 / 30 MPH

25 MPH OR LESS

200' (2)

100' (2)

TEMPORARY SIGN LOCATION

CHANNELIZING DEVICES

PROTECTIVE VEHICLE

NOTES:

1. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20'.

2. ALL SIGNS ARE BLACK ON ORANGE.

3. NO FLAGGERS OR SPOTTERS.

	LONGITUDINAL BUFFER SPACE = B									
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (FEET)	155	200	250	305	-	_	_	-	_	_
•			BUFFE	R VEHICLE	ROLL AHE	AD DISTAN	CE = R			•
RANSPORTA	BLE ATTEN	UATOR:	טטררנ	IN VEHICLE	. NOLL ALIE	TO DISTAIN				

MAY BE A WORK VEHICLE STRATEGICALLY LOCATED TO SHIELD THE WORK AREA.

CHANNELIZATION DEVICE

SPACING (FEET)

TAPER

30

NO SPECIFIED DISTANCE REQUIRED

35 / 40

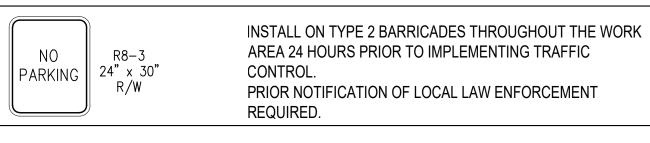
25 / 30

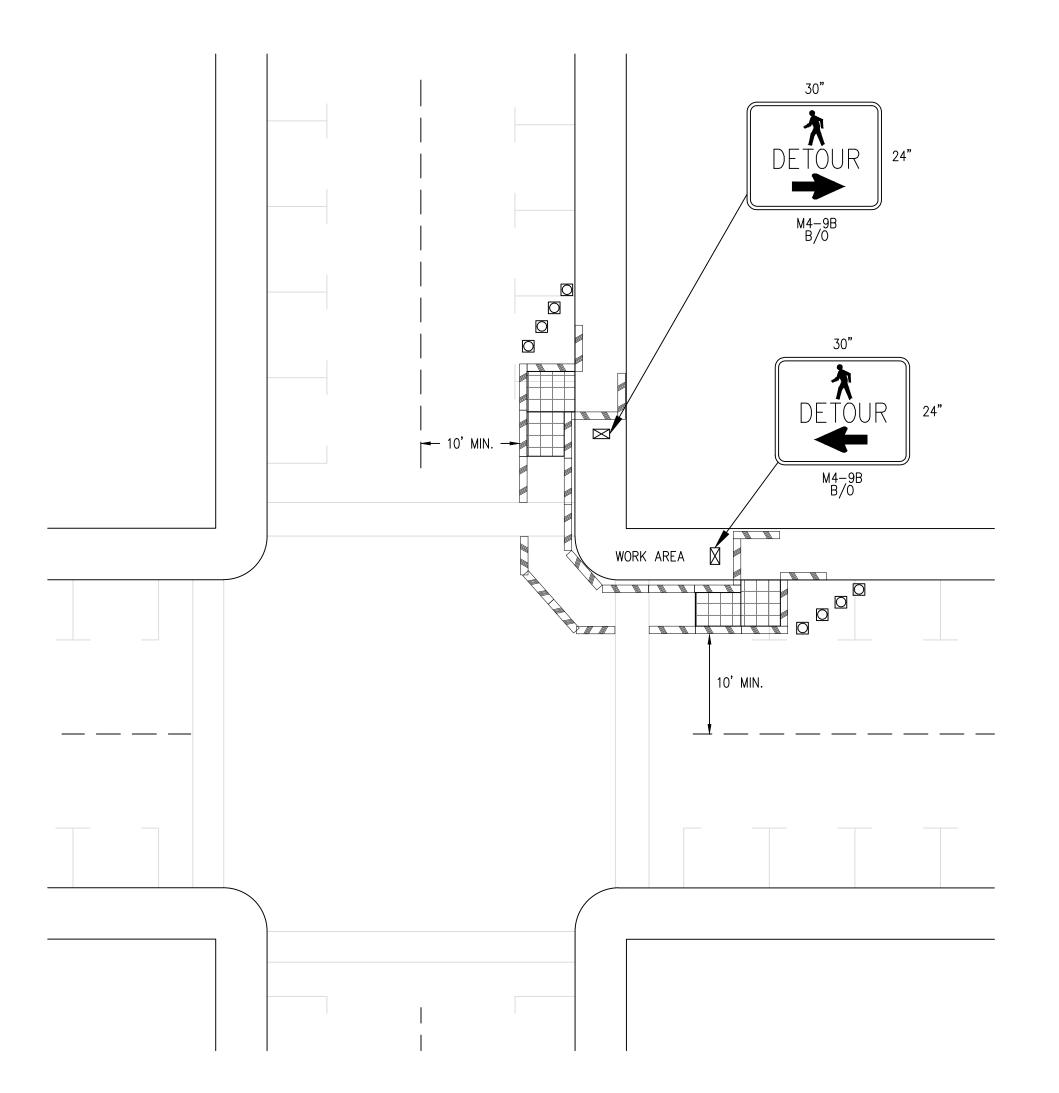
TANGENT

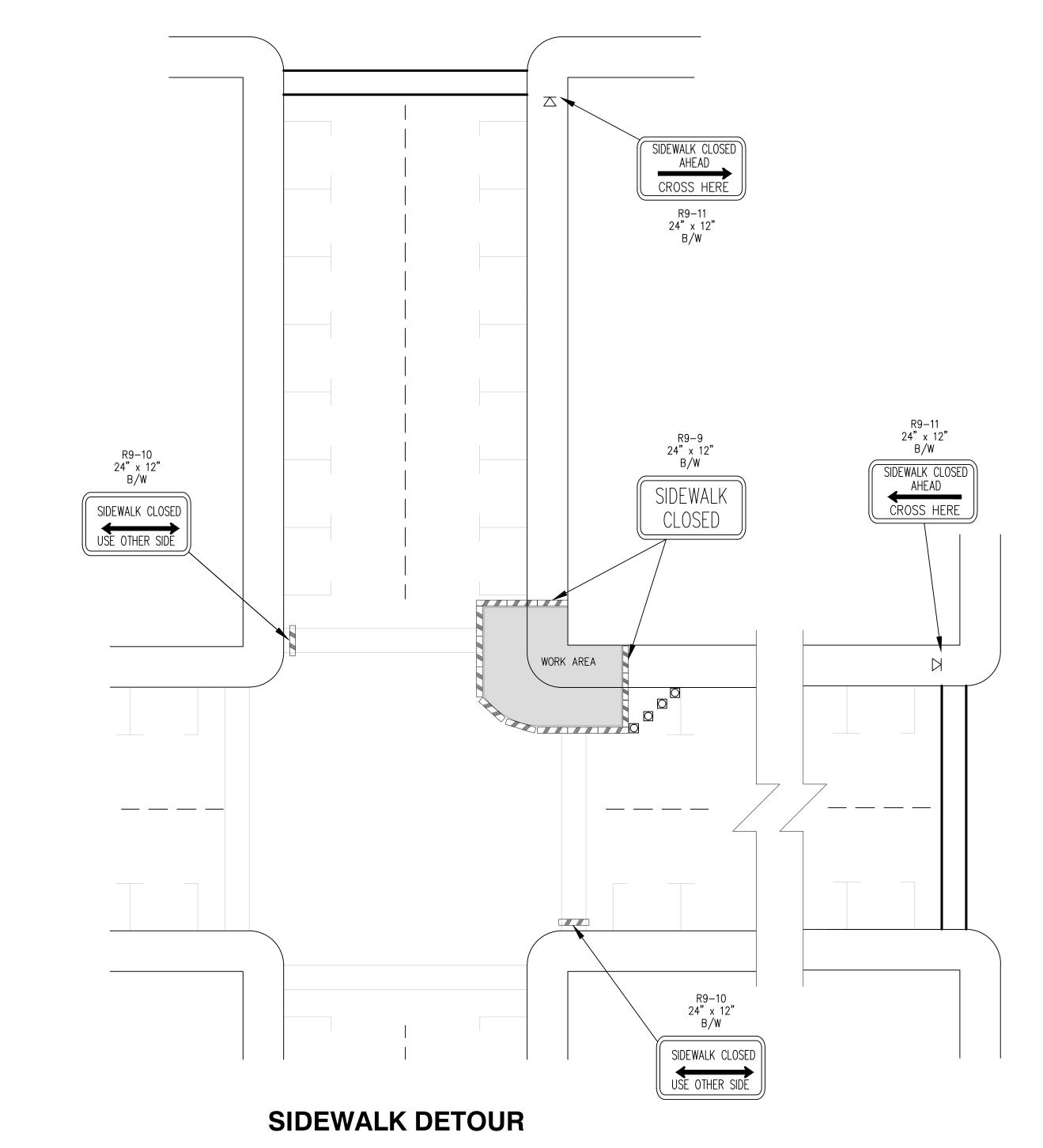
60

40

PROTECTIVE VEHICLE:







SIDEWALK DIVERSION

		NOTES

- TEMPORARY SIGN LOCATION
- CHANNELIZING DEVICES

LEGEND

- PEDESTRIAN CHANNELIZING DEVICES
- TEMPORARY PEDESTRIAN RAMP FOR SIDEWALKS

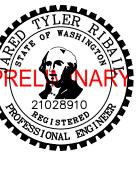
- 1. CONTROLS SHOWN ARE FOR PEDESTRIAN TRAFFIC ONLY.
- 2. A 60" PEDESTRIAN PATH WIDTH SHOULD BE MAINTAINED (48' IS THE MINIMUM).
- 3. CONTACT AND COORDINATE IMPACTED TRANSIT AGENCIES PRIOR TO IMPLEMENTING ANY CLOSURES.
- 4. SEE SHEET TC-52 FOR TEMPORARY PEDESTRIAN RAMP DETAILS.
- 5. ADA PEDESTRIAN FACILITIES MUST BE MAINTAINED, SEE STANDARD SPECIFICATION 1-10.2(1)B.
- 6. TEMPORARY PEDESTRIAN PUSH BUTTONS SHALL BE PLACED ON THE DIVERTED PATH WHEN EXISTING BUTTONS ARE NOT ACCESSIBLE TO PEDESTRIANS.

INTERSECTION PEDESTRIAN TRAFFIC CONTROL NOT TO SCALE









OF SHELTON SWALK IMPROVEMENTS CONTROL CITY OF SCROSSWAL

SCALE: SHOWN BAR MEASURES 2"

City of Shelton SRTS Project Engineer's Opinion of Probable Cost									
Item Number	Total Quantity	Unit	Item	City	Unit Cost		Engr. Est.		nar. Est.
			Tax Schedule - See specification section 1-07.2(1)						.g.:
			PREPARATION		_				
1	1		MOBILIZATION	11	\$	75,792.00		\$	75,792
2	1	L.S.	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	1	\$	20,000.00		\$	20,000
3	120	S.Y.	REMOVING CEMENT CONC. SIDEWALK	120	\$	30.00		\$	3,600
			SURFACING/PAVING						
4	220	TON	CRUSHED SURFACING TOP COURSE	220	¢.	100.00		\$	22,000
5	420		HMA CL. 1/2" PG 64-22	420	\$	150.00		\$	63,000
6	730		CRUSHED SURFACING BASE COURSE	730	\$	110.00		\$	80,300
	730	TON	CROSHED SORFACING BASE COORSE	730	φ	110.00		Φ	60,300
			GRADING						
7	640	C.Y.	ROADWAY EXCAVATION INCL. HAUL	640	\$	30.00		\$	19,200
-								<u> </u>	
			EROSION CONTROL AND PLANTING						
8	1	L.S.	EROSION CONTROL AND WATER POLLUTION CONTROL	1	\$	5,000.00		\$	5,000
_						, , , , , , , , , , , , , , , , , , , ,			
			TRAFFIC						
9	720	L.F.	CEMENT CONC. TRAFFIC CURB AND GUTTER	720	\$	75.00		\$	54,000
10	135	L.F.	PLASTIC STOP LINE	135	\$	15.00		\$	2,025
11	710	S.F.	PLASTIC CROSSWALK LINE	710	\$	7.00		\$	4,970
12	1	L.S.	PROJECT TEMPORARY TRAFFIC CONTROL	1	\$	50,000.00		\$	50,000
13	1	L.S.	RECTANGULAR RAPID FLASHING BEACON SYSTEM	1	\$	80,000.00		\$	80,000
14	1	L.S.	RADAR SPEED DISPLAY SIGN	1	\$	10,000.00		\$	10,000
15	1	L.S.	PERMANENT SIGNING	1	\$	10,000.00		\$	10,000
			WATER LINES						
16	1	EA.	HYDRANT ASSEMBLY	11	\$	11,000.00		\$	11,000
			STORM SEWER						
17	2	EA.	CATCH BASIN TYPE 1	2	\$	6,500.00		\$	13,000
.,,		L/\.	S. (1011 B. (CIII V 1 11 E 1		Ψ	0,000.00		Ψ	10,000
			OTHER ITEMS						-
18	14	EA.	CEMENT CONC. CURB RAMP TYPE PERPENDICULAR	14	\$	5,100.00		\$	71,400
19	560		CEMENT CONC. SIDEWALK	560	\$	150.00		\$	84,000
20	40	S.Y.	CEMENT CONC. DRIVEWAY ENTRANCE TYPE 1	40	\$	200.00		\$	8,000
21	1	L.S.	CEMENT CONC. MEDIAN REFUGE ISLAND	1	\$	20,000.00		\$	20,000
22	4	EA.	MAILBOX SUPPORT TYPE 1	4	\$	800.00		\$	3,200
23	145	L.F.	CHAIN LINK FENCE TYPE 3	145	\$	50.00		\$	7,250
24	1	EA.	ADJUST MONUMENT	1	\$	1,000.00		\$	1,000
25	1	EA.	ADJUST WATER VALVE BOX	1	\$	1,000.00		\$	1,000
26	1	EA.	ADJUST MANHOLE	1	\$	1,500.00		\$	1,500
27	1	EA.	ADJUST CLEANOUT	1	\$	1,000.00		\$	1,000
28	5	EA.	ADJUST CATCH BASIN	5	\$	1,500.00		\$	7,500
29	1	L.S.	ADA FEATURES SURVEY	1	\$	5,000.00		\$	5,000
30	5000	DOL	UNKNOWN UTILITY REPAIR	5000	\$	1.00		\$	5,000
31	1	L.S.	RECORD DRAWINGS	1	\$	2,000.00		\$	2,000
32	1	L.S.	ROADWAY SURVEYING	1	\$	5,000.00		\$	5,000
33	5000	DOL	MINOR CHANGE	5000	\$	1.00		\$	5,000

| Construction Total | 751,737 | Design Engineering | 101,952 | Construction Engineering | 35,500 | Project Total | 889,189 |